



SAN FERNANDO CITY COUNCIL
REGULAR MEETING NOTICE AND AGENDA
NOVEMBER 20, 2017 – 6:00 PM

COUNCIL CHAMBERS
117 MACNEIL STREET
SAN FERNANDO, CA 91340

CALL TO ORDER/ROLL CALL

Mayor Sylvia Ballin
Vice Mayor Antonio Lopez
Councilmember Jaime Soto
Councilmember Joel Fajardo
Councilmember Robert C. Gonzales

PLEDGE OF ALLEGIANCE

Led by Kenneth Jones Public Works Management Analyst

APPROVAL OF AGENDA

PRESENTATIONS

- a) RECOGNITION OF POLICE RESERVE OFFICERS AND CITY VOLUNTEERS
Presented by Management Staff

DECORUM AND ORDER

The City Council, elected by the public, must be free to discuss issues confronting the City in an orderly environment. Public members attending City Council meetings shall observe the same rules of order and decorum applicable to the City Council ([SF Procedural Manual](#)). Any person making impertinent derogatory or slanderous remarks or who becomes boisterous while addressing the City Council or while attending the City Council meeting, may be removed from the room if the Presiding Officer so directs the sergeant-at-arms and such person may be barred from further audience before the City Council.

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PUBLIC STATEMENTS – WRITTEN/ORAL

There will be a three (3) minute limitation per each member of the audience who wishes to make comments relating to City Business. Anyone wishing to speak, please fill out the blue form located at the Council Chambers entrance and submit it to the City Clerk. When addressing the City Council please speak into the microphone and voluntarily state your name and address.

CITY COUNCIL - LIAISON UPDATES**CONSENT CALENDAR**

Items on the Consent Calendar are considered routine and may be disposed of by a single motion to adopt staff recommendation. If the City Council wishes to discuss any item, it should first be removed from the Consent Calendar.

1) REQUEST TO APPROVE THE MINUTES OF NOVEMBER 6, 2017 – REGULAR MEETING**2) CONSIDERATION TO ADOPT A RESOLUTION APPROVING THE WARRANT REGISTER**

Recommend that the City Council adopt Resolution No. 17-112 approving the Warrant Register.

3) CONSIDERATION TO ADOPT A RESOLUTION AMENDING THE SALARY SCHEDULE TO INCLUDE THE NEGOTIATED MINIMUM WAGE INCREASES FOR CERTAIN PART-TIME CLASSIFICATIONS

Recommend that the City Council:

- a. Adopt Resolution No. 7823 amending the Salary Schedule to include the negotiated minimum wage increases for certain part-time classifications;
- b. Approve the implementation of the negotiated wages for all applicable part-time employees including the unrepresented confidential part-time employees; and
- c. Authorize the City Manager to make non-substantive corrections and execute all related documents.

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4) CONSIDERATION TO ADOPT A RESOLUTION APPOINTING THE CITY'S REPRESENTATIVES TO THE INDEPENDENT CITIES RISK MANAGEMENT AUTHORITY (ICRMA) GOVERNING BOARD

Recommend that the City Council adopt Resolution No. 7825 appointing City Manager Alexander P. Meyerhoff as the Primary Representative, re-affirming Finance Director Nick Kimball as the Alternate Representative, and re-affirming Personnel Manager Michael Okafor as the Substitute Alternate representative on the Independent Cities Risk Management Authority Governing Board.

PUBLIC HEARING**5) CONSIDERATION TO APPROVE ANNUAL GENERAL PLAN HOUSING ELEMENT PROGRESS REPORTS FOR CALENDAR YEARS 2015 AND 2016**

Recommend that the City Council:

- a. Open the Public Hearing and receive any comments from the public regarding the General Plan Housing Element Annual Progress Reports for the Calendar Years 2015 and 2016, and thereafter close the Public Hearing; and
- b. Consider and approve the General Plan Housing Element Annual Progress Reports for the Calendar Years 2015 and 2016; and
- c. Authorize staff to forward the 2015 and 2016 Housing Element Annual Progress Reports to the California Department of Housing and Community Development and the Governor's Office of Planning and Research.

6) CONSIDERATION TO ADOPT RESOLUTIONS CERTIFYING ENVIRONMENTAL IMPACT REPORT SCH NO. 2015121088 AND APPROVING THE AMENDMENTS TO THE CITY'S GENERAL PLAN TEXT AND MAP, AND CONSIDERATION TO ADOPT AN ORDINANCE APPROVING THE SAN FERNANDO CORRIDORS SPECIFIC PLAN SP-5 AND RELATED AMENDMENTS TO THE CITY'S ZONING ORDINANCE TEXT AND MAP

Recommend that the City Council:

- a. Open the Public Hearing and receive City staff presentation;
- b. Pose questions to City staff;
- c. Receive public testimony;

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- d. Pose further questions to City staff based on receipt of public testimony;
- e. Close the Public Hearing and commence deliberations on the proposed Corridors Specific Plan SP-5 and related EIR and General Plan and Zoning Ordinance text changes and maps;
- f. Adopt Resolution No. 7826:
 - i. Certifying the Final Environmental Impact Report SCH No. 2015121088, pursuant to the California Environmental Quality Act, finding that the proposed San Fernando Corridors Specific Plan SP-5 and related amendments to the City's General Plan and Zoning Ordinance text and maps will not have an effect on the environment that cannot be mitigated to less-than-significant impact levels, and such finding shall be contingent upon the final adoption of the amendments to the General Plan text and map, and final adoption of the San Fernando Corridors Specific Plan SP-5 and amendments to the Zoning Ordinance text and map; and
 - ii. Approving the amendments to the City's General Plan text and map, contingent upon the final adoption of the San Fernando Corridors Specific Plan SP-5 and amendments to the Zoning Ordinance text and map; and
- g. Introduce for first reading, in title only, and waive further reading of Ordinance No. 1671, "An Ordinance of the City Council of the City of San Fernando, California, Approving the San Fernando Corridors Specific Plan Sp-5 and Repealing in its Entirety the Former Corridors Specific Plan Sp-4".

ADMINISTRATIVE REPORTS**7) GENERAL DISCUSSION OF DEVELOPMENT ISSUES INCLUDING, BUT NOT LIMITED TO, PROJECT LABOR AGREEMENTS, DOWN PAYMENT ASSISTANCE PROGRAMS, AND OTHER TOPICS OF COMMUNITY CONCERN OR INTEREST**

This item is placed on the agenda by Councilmember Joel Fajardo.

8) CONSIDERATION TO ADOPT AN ORDINANCE REGARDING THE REGULATION OF BEHESTED PAYMENTS

This item is placed on the agenda by Councilmember Joel Fajardo.

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DEPARTMENT HEADS - COMMISSION UPDATES**GENERAL COUNCIL COMMENTS****STAFF COMMUNICATION****ADJOURNMENT**

I hereby certify under penalty of perjury under the laws of the State of California that the foregoing agenda was posted on the City Hall bulletin board not less than 72 hours prior to the meeting.

Elena G. Chávez, CMC

City Clerk

Signed and Posted: November 17, 2017 (10:00 a.m.)

Agendas and complete Agenda Packets (including staff reports and exhibits related to each item) are posted on the City's Internet website (www.sfcity.org). These are also available for public reviewing prior to a meeting in the City Clerk Department. Any public writings distributed by the City Council to at least a majority of the Councilmembers regarding any item on this regular meeting agenda will also be made available at the City Clerk Department at City Hall located at 117 Macneil Street, San Fernando, CA, 91340 during normal business hours. In addition, the City may also post such documents on the City's website at www.sfcity.org. In accordance with the Americans with Disabilities Act of 1990, if you require a disability-related modification/accommodation to attend or participate in this meeting, including auxiliary aids or services please call the City Clerk Department at (818) 898-1204 at least 48 hours prior to the meeting.

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Regular Meeting

San Fernando City Council

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**SAN FERNANDO CITY COUNCIL
MINUTES**

**NOVEMBER 6, 2017 – 6:00 P.M.
REGULAR MEETING**

City Hall Council Chambers
117 Macneil Street
San Fernando, CA 91340

CALL TO ORDER/ROLL CALL

Mayor Sylvia Ballin called the meeting to order at 6:05 p.m.

Present:

Council: Mayor Sylvia Ballin, Vice Mayor Antonio Lopez, and Councilmembers Jaime Soto (arrived at 6:07 p.m.), Joel Fajardo, and Robert C. Gonzales

Staff: City Manager Alexander P. Meyerhoff, Assistant City Attorney Richard Padilla and City Clerk Elena G. Chávez

PLEDGE OF ALLEGIANCE

Led by Finance Director Nick Kimball

APPROVAL OF AGENDA

Councilmember Fajardo requested to delete Item No. 10 and to add a walk-on item.

City Attorney Olivarez explained that the walk-on item is to consider appointment of a Planning and Preservation Commissioner which was brought to staff's attention after the agenda was posted. Councilmember Fajardo's representative resigned and there is a need for City Council to take action because there are important votes that the Planning and Preservation Commission will need to take at their meeting tomorrow.

Motion by Councilmember Fajardo, seconded by Vice Mayor Lopez, to approve the agenda with the two specified changes. By consensus, the motion carried.

PRESENTATIONS

a) INTRODUCTION OF THREE NEW POLICE OFFICERS

SAN FERNANDO CITY COUNCIL**MINUTES – November 6, 2017****Page 2****ADDED WALK-ON ITEM**

Councilmember Fajardo reported that his appointed Planning and Preservation Commissioner Kevin Beaulieu recently resigned and he provided additional background information regarding his request to appoint Aida Montes.

Motion by Councilmember Fajardo, seconded by Councilmember Gonzales, to appoint Aida Montes to the Planning and Preservation Commission as his representative. By consensus, the motion carried.

DECORUM AND ORDER

City Attorney Olivarez provided a brief summary of the rules.

PUBLIC STATEMENTS – WRITTEN/ORAL

Marisol Diaz talked about the upcoming food drive at the City parks and invited everyone to a diabetes expo on November 18.

Dee Akemon said the homeless problem is becoming worse and asked for an update as to what the City is doing about the issue (Police Chief Anthony Vairo gave a brief update).

City Clerk Chávez read an email from resident Julie Cuellar who again expressed her concern regarding speeding issues in the City and reported that her son was injured by a hit and run speeding driver near her home.

CITY COUNCIL - LIAISON UPDATES

Councilmember Gonzales gave updates regarding the Library Commission, Independent Cities, and Metro Service Council activities.

Councilmember Fajardo gave updates regarding the San Fernando Valley Council of Governments.

Vice Mayor Lopez gave updates regarding the weekly meetings of the Ad Hoc on cannabis committee and he reported they've made a lot of progress.

CONSENT CALENDAR

City Manager Meyerhoff asked to pull Item No. 4 for further discussion.

Motion by Councilmember Fajardo, seconded by Councilmember Gonzales, to approve the remaining Consent Calendar Items:

SAN FERNANDO CITY COUNCIL**MINUTES – November 6, 2017****Page 3**

- 1) REQUEST TO APPROVE THE MINUTES OF:
 - a) APRIL 3, 2017 – REGULAR MEETING
 - b) OCTOBER 16, 2017 – SPECIAL MEETING
 - c) OCTOBER 16, 2017 – REGULAR MEETING
- 2) CONSIDERATION TO ADOPT A RESOLUTION APPROVING THE WARRANT REGISTER
- 3) CONSIDERATION TO APPROVE AN AMENDMENT TO THE ADMINISTRATIVE CONTRACT WITH LIEBERT CASSIDY WHITMORE TO CONTINUE TO PROVIDE LABOR NEGOTIATOR SERVICES
- 5) CONSIDERATION TO AWARD A CONTRACT FOR THE ANNUAL RESURFACING PROJECT, CIP NO. 0560, PLAN NO. P-725
- 6) CONSIDERATION TO APPROVE THE MAYOR'S PROPOSED APPOINTMENT OF THE CITY MANAGER TO THE CITY'S VACANT POSITION ON THE OVERSIGHT BOARD TO THE SUCCESSOR AGENCY TO THE SAN FERNANDO REDEVELOPMENT AGENCY

By consensus, the motion carried

Items Pulled for Further Discussion

- 4) CONSIDERATION TO ADOPT A RESOLUTION AMENDING THE CITY PERSONNEL RULES TO INCLUDE CHANGES TO THE VETERANS' EMPLOYMENT PREFERENCE POLICY

City Manager Meyerhoff gave background information and responded to questions from Councilmembers.

Motion by Councilmember Gonzales, seconded by Councilmember Soto, to:

- a. Adopt Resolution No. 7824 amending Section 9, Rule VI of the City Personnel Rules to include changes to the existing veterans' employment preference policy; and
- b. Authorize the City Manager to implement the new policy, and make non-substantive edits as necessary.

By consensus, the motion carried.

SAN FERNANDO CITY COUNCIL**MINUTES – November 6, 2017****Page 4****ADMINISTRATIVE REPORTS**

- 7) CONSIDERATION TO APPROVE USE OF CITY LETTERHEAD FOR A LETTER INVITING LOCAL SCHOOL PRINCIPALS TO A BRUNCH EVENT IN JANUARY 2018

Education Commissioner David Govea made a brief presentation.

Motion by Councilmember Fajardo, seconded by Councilmember Gonzales, to allow the Education Commission to use City letterhead for a letter inviting local school principals to a brunch event in January 2018. By consensus, the motion carried.

- 8) CONSIDERATION TO APPROVE THE USE OF CITY LETTERHEAD FOR A THANK YOU LETTER TO SENATOR JOHN MCCAIN

A brief discussion ensued amongst Councilmembers.

Motion by Mayor Ballin, seconded by Councilmember Fajardo, to allow Mayor Ballin to use City letterhead for the purpose of sending a thank you letter to Senator John McCain. By consensus, the motion carried.

- 9) CONSIDERATION OF THE SAN FERNANDO BEAUTIFICATION PROJECT

A brief discussion ensued amongst Councilmembers.

Motion by Councilmember Gonzales, seconded by Councilmember Fajardo, to:

- a. Approve use of the City Seal for the San Fernando Beautification Project event promotional print material pursuant to City Council Resolution No. 6904; and
- b. Authorize staff time to help organize the event.

By consensus, the motion carried.

- 10) CONSIDERATION TO APPOINT ALTERNATE BOARD REPRESENTATIVES TO THE SAN FERNANDO VALLEY COUNCIL OF GOVERNMENTS

Item removed under Approval of Agenda.

DEPARTMENT HEADS - COMMISSION UPDATES

Personnel Manager Michael Okafor reported that the County of Los Angeles is providing Hepatitis A vaccinations; he also gave updates regarding various recruitment positions and replied to questions from Councilmembers.

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Both Recreation Supervisor Maribel Perez and Cultural Arts Supervisor Virginia Diediker gave updates regarding upcoming events in the Recreation and Community Services Department.

GENERAL COUNCIL COMMENTS

Councilmember Gonzales thanked staff for their hard work and for the successful 5K Run Event.

Councilmember Fajardo said he's excited that the City is hiring new Department Heads and it's uplifting to see we're going in the right direction. He is very supportive of the veteran's measure approved today and said that he's glad to be back from his European vacation.

Vice Mayor Lopez said he was glad to see Police Officer new hires.

Mayor Ballin thanked Recreation and Community Services employees Maribel Perez and Virginia Diediker for their wonderful work and the events they've put together in the community.

Councilmember Soto also thanked Recreation and Community Services employees for their work, City staff and the City Manager for everything they're doing.

STAFF COMMUNICATION

City Manager Meyerhoff gave updates regarding various meetings and activities that he has participated with staff and several organizations.

ADJOURNMENT (7:23 P.M.)

Councilmember Soto requested to adjourn the meeting in memory of resident George Walter. Councilmember Fajardo also asked to adjourn the meeting in memory of the victims that were killed in the Texas church shooting.

Motion by Councilmember Soto, seconded by Councilmember Gonzales, to adjourn the meeting in memory of those requested. By consensus, the motion carried.

I do hereby certify that the foregoing is a true and correct copy of the minutes of November 6, 2017, meeting as approved by the San Fernando City Council.

Elena G. Chávez, CMC
City Clerk

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Alexander P. Meyerhoff, City Manager
By: Nick Kimball, Finance Director

Date: November 20, 2017

Subject: Consideration to Adopt a Resolution Approving the Warrant Register

RECOMMENDATION:

It is recommended that the City Council adopt Resolution No. 17-112 (Attachment "A") approving the Warrant Register.

BACKGROUND:

For each City Council meeting the Finance Department prepares a Warrant Register for Council approval. The Register includes all recommended payments for the City. Checks, other than handwritten checks, generally are not released until after the Council approves the Register. The exceptions are for early releases to avoid penalties and interest, excessive delays and in all other circumstances favorable to the City to do so. Handwritten checks are those payments required to be issued between Council meetings such as insurance premiums and tax deposits. Staff reviews requests for expenditures for budgetary approval and then prepares a Warrant Register for Council approval and or ratification. Items such as payroll withholding tax deposits do not require budget approval.

The Finance Director hereby certifies that all requests for expenditures have been signed by the department head, or designee, receiving the merchandise or services thereby stating that the items or services have been received and that the resulting expenditure is appropriate. The Finance Director hereby certifies that each warrant has been reviewed for completeness and that sufficient funds are available for payment of the warrant register.

ATTACHMENT:

A. Resolution No. 17-112

ATTACHMENT "A"**RESOLUTION NO. 17-112****RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SAN FERNANDO ALLOWING AND APPROVING FOR PAYMENT DEMANDS PRESENTED ON DEMAND/ WARRANT REGISTER NO. 17-112****THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY RESOLVE, FIND, DETERMINE AND ORDER AS FOLLOWS:**

1. That the demands (EXHIBIT "A") as presented, having been duly audited, for completeness, are hereby allowed and approved for payment in the amounts as shown to designated payees and charged to the appropriate funds as indicated.
2. That the City Clerk shall certify to the adoption of this Resolution and deliver it to the City Treasurer.

PASSED, APPROVED, AND ADOPTED this 20th day of November, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) ss
CITY OF SAN FERNANDO)

I HEREBY CERTIFY that the foregoing Resolution was approved and adopted at a regular meeting of the City Council held on the 20th day of November, 2017, by the following vote to wit:

AYES:

NOES:

ABSENT:

Elena G. Chávez, City Clerk

EXHIBIT "A"

rchlist		Voucher List				Page: 1
11/15/2017	3:27:35PM	CITY OF SAN FERNANDO				
Bank code :		bank3				
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208028	11/20/2017	892718 ABELAR, CONNIE	2000040.003		SENIOR TRIP REFUND 004-2383	140.00 140.00
208029	11/20/2017	892717 ABELAR, MARIA	2000041.003		SENIOR TRIP REFUND 004-2383	140.00 140.00
208030	11/20/2017	892716 ACUNA, IRENE	2000042.003		SENIOR TRIP REFUND 004-2383	130.00 130.00
208031	11/20/2017	100066 ADS ENVIRONMENTAL SERVICES,INC	22101.22-1017 22206.52-1017	11638 11550	WASTEWATER FLOW MONITORING TH 072-360-0000-4260 OCT'17-ON CALL MAINTENANCE SERV 072-360-0000-4260	1,555.00 903.00 2,458.00
208032	11/20/2017	892493 AG PRINTING & TROPHIES	101917		TABLE CLOTHES 001-420-0000-4300	90.75 90.75
208033	11/20/2017	891690 AGUIRRE, ELVIA N.	102817		FACE PAINTER-DIA DE LOS MUERTOS 001-424-0000-4260	150.00 150.00
208034	11/20/2017	889043 ALADIN JUMPERS	102717		FACE PAINTING SERVICES 001-424-0000-4260	325.00 325.00
208035	11/20/2017	100143 ALONSO, SERGIO	OCT 2017		MMAPI INSTRUCTOR 109-424-3678-4260 108-424-3694-4260	1,000.00 320.00 1,320.00
208036	11/20/2017	892719 ALONZO, RAMON	2000043.003		SENIOR TRIP REFUND 004-2383	60.00

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Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount	
208036	11/20/2017	892719	892719 ALONZO, RAMON	(Continued)		Total :	60.00
208037	11/20/2017	892729	AMERICAN INT'L ENG & MFG.	56-0480-02	WATER ACCT REFUND-864 ARROYO 070-2010	180.99 Total :	180.99
208038	11/20/2017	100175	AMERICAN WATER WORKS ASSOC.	7001425792	MEMBERSHIP RENEWAL-00034415 070-381-0000-4380	420.00 Total :	420.00
208039	11/20/2017	892285	APARICIO, MARGARITA	2000044.003	SENIOR TRIP REFUND 004-2383	130.00 Total :	130.00
208040	11/20/2017	102500	APCO INTERNATIONAL	475283	2018 MEMBERSHIP DUES 001-222-0000-4380	120.00 Total :	120.00
208041	11/20/2017	890411	ARC DOCUMENT SOLUTIONS, LLC	9405239	SRTS 1 & 2 PLANS FOR RESEARCH 012-311-0552-4600 012-311-0553-4600	160.90 160.90 Total :	321.80
208042	11/20/2017	100222	ARROYO BUILDING MATERIALS, INC	199180 199195 11685 11685	LOCAL HARDWARE PURCHASES 070-383-0000-4310 LOCAL HARDWARE PURCHASES 070-383-0000-4310	179.78 -29.96 Total :	149.82
208043	11/20/2017	892713	ASCENCIO, ALEJANDRO	102817	NATIONAL ANTHEM PRESENTATION 004-2359	200.00 Total :	200.00
208044	11/20/2017	892722	BANOS, GLORIA	2000045.003	SENIOR TRIP REFUND 004-2383	280.00 Total :	280.00
208045	11/20/2017	890546	BARAJAS, CRYSTAL	OCT 2017	MMAP MENTOR INSTRUCTOR		
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208054	11/20/2017	892380 CARDENAS, ENEDINA	(Continued)		004-2383	100.00
					Total :	100.00
208055	11/20/2017	891635 CASTELLANOS, ANA	2000047.003		SENIOR TRIP REFUND 004-2383	140.00
					Total :	140.00
208056	11/20/2017	892721 CASTELLANOS, RICHARD	2000048.003		SENIOR TRIP REFUND 004-2383	140.00
					Total :	140.00
208057	11/20/2017	892720 CASTILLO, ESTELA	2000049.003		SENIOR TRIP REFUND 004-2383	40.00
					Total :	40.00
208058	11/20/2017	892076 CEJ ENGINEERS, INC	142.02.16-04	11501	2016-17 ANNUAL STREET RESURFACIN 012-311-0560-4600	28,510.00
					Total :	28,510.00
208059	11/20/2017	891767 CHAVEZ, ELIZABETH	09/30/17-11/03/17		ZUMBA INSTRUCTOR 017-420-1337-4260	60.00
					Total :	60.00
208060	11/20/2017	892305 CISNEROS, GRACE	2000039.003		SENIOR TRIP REFUND 004-2383	130.00
					Total :	130.00
208061	11/20/2017	100731 CITY OF LOS ANGELES	74WP170000142	11640	O&M PORTION OF ASSSC-NOV 2017 072-365-0629-4600	196,525.00
			74WP170000142	11641	ASSSC CAPITAL PORTION-NOV 2017 072-360-0629-4260	147,704.00
					Total :	344,229.00
208062	11/20/2017	101957 CITY OF LOS ANGELES	38SF180000004		FIRE SERVICES-NOV 2017 001-500-0000-4260	230,441.17
					Total :	230,441.17
						Page: 5

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					Total :	10,522.16
208064	11/20/2017	100805 COOPER HARDWARE INC.	108176		STREET LIGHT PAINT 001-370-0301-4300	5.68
					Total :	5.68
208065	11/20/2017	887666 COUNTY OF LOS ANGELES	2037512		SF REGIONAL POOL REC RENTAL-EDU 001-115-0000-4450	585.70
					Total :	585.70
208066	11/20/2017	891637 CPRS DISTRICT 14	0100		RGSTR- REGION 4 MINI CONFERENCE 001-420-0000-4360	65.00
					Total :	65.00
208067	11/20/2017	892306 CRUZ, GILDA	2000050.003		SENIOR TRIP REFUND 004-2383	270.00
					Total :	270.00
208068	11/20/2017	891533 DEXMEDIA	6100347871775		DOMAIN REGISTRATION & E-MAIL 001-190-0000-4220	56.73
					Total :	56.73
208069	11/20/2017	891425 DIAZ, MARISOL	REIMB		PURCHASED ITEMS FOR SENIOR GRC 004-2383	27.30
			REIMB.		017-420-1322-4300	31.89
					MILEAGE REIMB 001-420-0000-4390	51.15
					Total :	110.34
208070	11/20/2017	887518 DURHAM, ALVIN	OCT 2017		COMMISSIONER'S STIPEND 001-150-0000-4111	50.00
					Total :	50.00
208071	11/20/2017	892611 ELLNER, JERRY	101617	11704	HEARING OFFICER 001-106-0000-4260	4,500.00
						Page: 6

vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 11
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208095	11/20/2017	891777 IRRIGATION EXPRESS	15099408-00	11659	MISC. IRRIGATION SUPPLIES FOR REF 043-390-0000-4300	141.55
					Total :	141.55
208096	11/20/2017	101688 J & R AUDIO	3566	11630	STAGE & SOUND SYS. RENTAL FOR 20 001-424-0000-4260	1,500.00
					Total :	1,500.00
208097	11/20/2017	887952 J. Z. LAWNMOWER SHOP	18920	11690	SMALL EQUIPMENT REPAIR 001-346-0000-4300	23.38
					Total :	23.38
208098	11/20/2017	889680 JIMENEZ LOPEZ, JUAN MANUEL	OCT 2017		MMAPI INSTRUCTOR 109-424-3678-4260 108-424-3694-4260	1,080.00 540.00
					Total :	1,620.00
208099	11/20/2017	892614 JWA URBAN CONSULTANTS, INC	SF #2017-8	11598	PROFESSIONAL SERVICES COMMUNIT 001-150-0000-4270	14,218.75
					Total :	14,218.75
208100	11/20/2017	102387 K.R. NIDA CORPORATION	2001370	11695	VEHICLE 2-WAY RADIO 016-310-3661-4500	1,098.01
					Total :	1,098.01
208101	11/20/2017	891738 KNIGHT COMMUNICATIONS INC	2010897	11588	IT MANAGEMENT SERVICES-NOV 2017 001-135-0000-4270	10,000.00
					Total :	10,000.00
208102	11/20/2017	892709 L.A. MAPPING SERVICE	07541		RADIUS MAP & LABELS FOR TOD 001-150-3673-4270	2,500.00
					Total :	2,500.00
208103	11/20/2017	101872 LEAGUE OF CALIFORNIA CITIES	57174		ADVERTISING-DIR OF PW/CITY ENGINEER 001-106-0000-4230	250.00
					Total :	250.00
208104	11/20/2017	101920 LIEBERT CASSIDY WHITMORE	1448313		LEGAL SERVICES	
						Page: 11

vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 12
Bank code : bank3						
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208104	11/20/2017	101920 LIEBERT CASSIDY WHITMORE	(Continued)			
			1448314		001-112-0000-4270 LEGAL SERVICES	140.00
			1448315		001-112-0000-4270 LEGAL SERVICES	112.00
			1448316		001-112-0000-4270 LEGAL SERVICES	1,666.00
			1448317		001-112-0000-4270 LEGAL SERVICES	1,586.00
					001-112-0000-4270	560.00
					Total :	4,064.00
208105	11/20/2017	102012 LOS ANGELES COUNTY	2508-002-270		2017-2018 PROPERTY TAXES-WELL3 070-381-0000-4450	138.68
			2508-005-270		2017-2018 PROPERTY TAXES-WELL 2A 070-381-0000-4450	154.32
			2508-005-271		2017-2018 PROPERTY TAXES-WELL 2A 070-384-0000-4450	102.38
			2509-014-270		2017-2018 PROPERTY TAX-RESVR 2&E 070-381-0000-4450	2,667.59
			2509-015-270		2017-2018 PROPERTY TAXES-RESVR 1 070-381-0000-4450	870.01
			2517-023-270		2017-2018 PROPERTY TAX-2005 FOUR 043-390-0000-4450	3,099.54
			8920-851-365		2017-2018 PROPERTY TAX-WATER 070-381-0000-4450	1,915.08
					Total :	8,947.60
208106	11/20/2017	102063 MACKAY METERS, INC.	1048259	11705	PARTS FOR EXISTING CITY PARKING M 029-335-0000-4320	3,623.75
					Total :	3,623.75
208107	11/20/2017	892714 MARTINEZ, ROSARIO	2000055.003		SENIOR TRIP REFUND 004-2383	140.00
					Total :	140.00
208108	11/20/2017	888254 MCCALLA COMPANY	159308		DISPOSABLE GLOVES	
						Page: 12

vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 13
Bank code : bank3						
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208108	11/20/2017	888254 MCCALLA COMPANY	(Continued)		001-222-0000-4300	596.31
					Total :	596.31
208109	11/20/2017	888242 MCI COMM SERVICE	7DK54968		MTA PHONE LINE 007-440-0441-4220	33.57
					Total :	33.57
208110	11/20/2017	891054 MEJIA, YVONNE G	OCT 2017		COMMISSIONER'S STIPEND 001-150-0000-4111	50.00
					Total :	50.00
208111	11/20/2017	892343 MELGOZA, RAUL	2000056.003		SENIOR TRIP REFUND 004-2383	60.00
					Total :	60.00
208112	11/20/2017	892725 MERCURY INSURANCE COMPANY	07132017		DAMAGE CLAIM REIMB. #CAPA-006018 006-190-0000-4800	1,451.00
					Total :	1,451.00
208113	11/20/2017	892140 MICHAEL BAKER	995535	11551	CDBG ADMINISTRATIVE & LABOR COM 001-150-0000-4270	1,800.00
					Total :	1,800.00
208114	11/20/2017	102201 MIERZYNSKI, IRMGARD	SEPT-OCT		LINE DANCER INSTRUCTOR 017-420-1339-4260	147.00
					Total :	147.00
208115	11/20/2017	102226 MISSION LINEN SUPPLY	506040127		LAUNDRY 001-225-0000-4350	85.40
			506066095		LAUNDRY 001-225-0000-4350	102.90
			506084718		LAUNDRY 001-225-0000-4350	110.35
			506112380		LAUNDRY 001-225-0000-4350	102.90
					Total :	401.55
						Page: 13

vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 14
Bank code : bank3						
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208116	11/20/2017	892379 MONTOYA, MARTINA	2000057.003		SENIOR TRIP REFUND 004-2383	130.00
					Total :	130.00
208117	11/20/2017	892535 MORAN, YOVANNI	09/30/17-11/03/17		YOGA (SUB) INSTRUCTOR 017-420-1337-4260	100.00
					Total :	100.00
208118	11/20/2017	890543 MUNISERVICES, LLC	INV06-000941		CAFR REPORT 001-130-0000-4270	1,000.00
					Total :	1,000.00
208119	11/20/2017	891355 NAREZ, FABIAN	OCT 2017		MMAP MENTOR INSTRUCTOR 109-424-3678-4260	272.00
					Total :	272.00
208120	11/20/2017	890843 NGOV, CHHIV	10/23/17-12/02/17		YOGA FOR SENIORS INSTRUCTOR 017-420-1322-4260	330.00
					Total :	330.00
208121	11/20/2017	102403 NOW IMAGE PRINTING	2017214		GARAGE SALE PERMITS 001-152-0000-4300	144.10
			2017216		UTILITY SERVICE ORDER FORMS 070-382-0000-4300	88.55
					072-360-0000-4300	88.55
					Total :	321.20
208122	11/20/2017	102432 OFFICE DEPOT	2122495783		OFFICE SUPPLIES 001-222-0000-4300	295.64
			9701387505002		OFFICE SUPPLIES 070-384-0000-4300	5.49
			971091938001		OFFICE SUPPLIES 001-222-0000-4300	391.89
			973137194001		OFFICE SUPPLIES 001-105-0000-4300	33.42
			975386283001		OFFICE SUPPLIES 001-106-0000-4300	419.38
						Page: 14

vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 15
Bank code :		bank3				
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208122	11/20/2017	102432 OFFICE DEPOT	(Continued)			
			976057829001		001-222-0000-4300 OFFICE SUPPLIES	316.47
			976346103001		001-222-0000-4300 TONER AND COPY PAPER	223.45
			976615751001		001-222-0000-4300 OFFICE SUPPLIES	401.52
					001-130-0000-4300	137.94
					070-382-0000-4300	41.86
					072-360-0000-4300	41.86
					Total :	2,308.92
208123	11/20/2017	892572 OLIVAREZ MADRUGA	1010		LEGAL SERVICES-JUNE 2017	
					001-2000	32,616.60
					072-2000	240.00
					070-2000	400.00
			1303		LEGAL SERVICES-JULY 2017	
					001-110-0000-4270	17,704.78
					072-360-0000-4270	880.00
					070-110-0000-4270	20.00
			1587		LEGAL SERVICES-AUG 2017	
					001-110-0000-4270	18,765.32
					072-360-0000-4270	620.00
					Total :	71,246.70
208124	11/20/2017	891419 ONE SOURCE INDUSTRIES	20150		INK-ID PRINTER	
					001-222-0000-4300	188.15
					Total :	188.15
208125	11/20/2017	890095 O'REILLY AUTOMOTIVE STORES INC	4605-263587	11666	VEHICLE MAINTENANCE & REPAIRS	
			4605-263692	11666	041-1215	54.89
					VEHICLE MAINTENANCE & REPAIRS	
					041-1215	117.62
					Total :	172.51
208126	11/20/2017	890403 OROZCO, CARMEN	2000058.003		SENIOR TRIP REFUND	
					004-2383	130.00

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vchlist 11/15/2017 3:27:35PM		Voucher List CITY OF SAN FERNANDO				Page: 16
Bank code :		bank3				
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208126	11/20/2017	890403 890403 OROZCO, CARMEN	(Continued)			Total : 130.00
208127	11/20/2017	890004 PACIFIC TELEMANAGEMENT SERVICE	952249		PD PAYPHONE - DEC	
					001-190-0000-4220	62.64
					Total :	62.64
208128	11/20/2017	889236 PADILLA, MARIA	REIMB.		MILEAGE REIMB-BUSINESS WRITING F	
					001-310-0000-4390	12.42
					Total :	12.42
208129	11/20/2017	102738 QUINTERO ESCAMILLA, VIOLETA	OCT 2017		SENIOR MUSIC INSTRUCTOR	
					017-420-1323-4260	300.00
					Total :	300.00
208130	11/20/2017	102779 RAMIREZ, THOMAS	OCT 2017		KARATE INSTRUCTOR	
					017-420-1326-4260	540.00
					Total :	540.00
208131	11/20/2017	102781 RAMOS, RICHARD	OCT 2017		MMAP INSTRUCTOR	
					109-424-3678-4260	720.00
					108-424-3694-4260	540.00
					Total :	1,260.00
208132	11/20/2017	102803 RED WING SHOE STORE	7-233	11600	ANNUAL SAFETY SHOE PURCHASE FC	
					070-384-0000-4310	4.66
					Total :	4.66
208133	11/20/2017	892368 REYES, MIGUEL ANGEL	10/23/17-12/02/17		LIFT&ENERGY TRAINING INSTRUCTOF	
					017-420-1322-4260	180.00
					Total :	180.00
208134	11/20/2017	887872 ROSENBERG, IRWIN	TRAVEL		PER DIEM-SBSLI POST TRAINING IN LC	
					001-222-0000-4370	105.00
					Total :	105.00
208135	11/20/2017	892036 ROTHSCILD, DEBORAH	10/23/17-11/29/17		STRETCH TO THE MUSIC INSTRUCTOF	
					017-420-1322-4260	600.00

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vchlist		Voucher List				Page: 17	
11/15/2017 3:27:35PM		CITY OF SAN FERNANDO					
Bank code :		bank3					
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount	
208135	11/20/2017	892036 892036 ROTHSCILD, DEBORAH	(Continued)			Total :	600.00
208136	11/20/2017	103010 SAM'S CLUB DIRECT, #0402814188546	2298		5K SPONSOR BASKETS		
			2300		017-420-1395-4300		94.33
					SENIOR CLUB EVENT REFRESHMENT		
					004-2380		97.55
					Total :		191.88
208137	11/20/2017	891253 SAN FERNANDO SMOG TEST ONLY	6232		SMOG TEST-WA8095		
			6238		070-383-0000-4400		50.00
			6239		SMOG TEST-CE6477		
			6240		041-320-0152-4400		50.00
			6242		SMOG TEST-CE5643		
			6252		041-320-0152-4400		50.00
					SMOG TEST-PW3241		
					029-335-0000-4400		50.00
					SMOG TEST-ME8615		
					041-320-0320-4400		50.00
					SMOG TEST-WA9503		
					070-382-0000-4400		50.00
					Total :		300.00
208138	11/20/2017	103057 SAN FERNANDO VALLEY SUN	10005		LEGAL PUBLICATION-TOD PUBLIC HEA		
					001-150-3673-4270		328.13
					Total :		328.13
208139	11/20/2017	892416 SANCHEZ, KARLA	09/30/17-11/03/17		ZUMBA INSTRUCTOR		
					017-420-1337-4260		160.00
					Total :		160.00
208140	11/20/2017	891653 SARGENT TOWN PLANNING	17020	11143	SAN FERNANDO TRANSIT ORIENTED I		
					113-150-3673-4270		18,327.83
					Total :		18,327.83
208141	11/20/2017	892726 SARITA'S BEAUTY COSMETICS	33-2065-02		WATER ACCT REFUND-1130 SAN FERN		
					070-2010		27.65
					Total :		27.65
						Page: 17	

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vchlist		Voucher List					Page: 18
11/15/2017 3:27:35PM		CITY OF SAN FERNANDO					
Bank code : bank3							
Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount	
208142	11/20/2017	891064 SIEMENS INDUSTRY INC	5620011888	11624	ON CALL TRAFFIC SIGNAL MAINTENAN	4,373.66	
			5620017806	11624	ON CALL TRAFFIC SIGNAL MAINTENAN	310.00	
					Total :	4,683.66	
208143	11/20/2017	892728 SILVA, RICHARD	52-1960-05		WATER ACCT REFUND-1924 CHIVERS	48.77	
					070-2010	48.77	
					Total :	48.77	
208144	11/20/2017	103170 SIRCHIE FINGER PRINT	0322208-IN		EVIDENCE COLLECTION SUPPLIES	114.74	
					001-222-0000-4300	114.74	
					Total :	114.74	
208145	11/20/2017	103184 SMART & FINAL	30681		SUPPLIES FOR ARTS & CRAFTS	15.92	
			40705		001-424-0000-4300	32.95	
			56435		SUPPLIES FOR ARTS & CRAFTS	78.41	
					001-424-0000-4300	72.56	
			58311		ENP SUPPLIES	94.19	
					004-2346	72.56	
					001-422-0000-4300	94.19	
					SUPPLIES-ENP FALL CELEBRATION	294.03	
					004-2346	294.03	
					Total :	294.03	
208146	11/20/2017	892367 SOLIS, MARGARITA	10/24-10/27		L P SENIOR PETTY CASH REIMB.	61.90	
					004-2380	61.90	
					Total :	61.90	
208147	11/20/2017	892723 SOUTH COAST PIERING INC	BS1711710		BUILDING PERMIT REFUND	209.54	
					001-3320-0000	209.54	
					Total :	209.54	
208148	11/20/2017	103202 SOUTHERN CALIFORNIA EDISON CO.	2-02-682-6982		ELECTRIC-910 FIRST	5,144.79	
			2-02-682-7675		043-390-0000-4210	8,073.10	
					VARIOUS LOCATIONS		
					043-390-0000-4210		

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Voucher List
CITY OF SAN FERNANDO

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Bank code : bank3

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
208167	11/20/2017	890970 WEX BANK	(Continued)		007-313-3630-4402	633.50
					029-335-0000-4402	188.65
					070-381-0000-4402	22.26
					070-382-0000-4402	171.46
					070-383-0000-4402	630.60
					070-384-0000-4402	236.99
					072-360-0000-4402	712.59
					Total :	11,349.32
208168	11/20/2017	889138 WIEDER, CAROL	110817		INTERPRETATION SERVICES CC-MEET	
					001-101-0000-4270	250.00
					Total :	250.00
208169	11/20/2017	891531 WILLDAN ENGINEERING	003-24549		ON-CALL CITY ENGINEERING SERVICE	
				11703	001-310-0000-4260	4,095.00
				11703	010-320-3697-4600	975.00
				11703	029-335-0000-4600	130.00
			003-24711		ON-CALL CITY ENGINEERING SERVICE	
				11703	001-310-0000-4260	7,410.00
				11703	010-320-3697-4600	975.00
			003-24916		ON-CALL CITY ENGINEERING SERVICE	
				11703	001-310-0000-4260	6,305.00
				11703	001-310-0159-4260	130.00
				11703	010-320-3697-4600	715.00
				11703	029-335-0000-4600	195.00
				11703	022-311-0560-4270	1,755.00
					Total :	22,685.00
208170	11/20/2017	889491 WILLDAN FINANCIAL SERVICES	010-36124		ANNUAL DISTRICT ADMINISTRATION	
				11644	027-344-0000-4260	2,500.00
					Total :	2,500.00
208171	11/20/2017	103752 ZUMAR INDUSTRIES, INC.	0172411		SPEED LIMIT SIGNS	
					001-310-0000-4310	483.67
					Total :	483.67

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Voucher List
CITY OF SAN FERNANDO

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Bank code : bank3

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
144		Vouchers for bank code : bank3			Bank total :	892,609.19
144		Vouchers in this report			Total vouchers :	892,609.19

Voucher Registers are not final until approved by Council.

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HANDWRITTEN CHECKS

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Voucher List

CITY OF SAN FERNANDO

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Bank code : bank3

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
207684	10/2/2017	102519 P.E.R.S.	DEMAND		HEALTH INSURANCE - OCTOBER 2017 001-1160	138,655.45
					Total :	138,655.45
207685	10/5/2017	103648 CITY OF SAN FERNANDO	PR 10-6-17		REIMB FOR PAYROLL W/E 9-29-17 001-1003	309,980.84
					007-1003	117.13
					008-1003	1,321.72
					010-1003	635.57
					018-1003	61,190.33
					027-1003	4,355.38
					029-1003	1,846.19
					041-1003	8,498.13
					043-1003	13,712.05
					070-1003	32,210.69
					072-1003	22,901.29
					119-1003	1,364.35
					120-1003	856.95
					Total :	458,990.62
207686	10/6/2017	890907 DELTA DENTAL OF CALIFORNIA	OCTOBER 2017		DENTAL INSURANCE BENEFITS-OCT 2 001-1160	11,247.70
					Total :	11,247.70
207687	10/6/2017	891230 DELTA DENTAL INSURANCE COMPANY	OCTOBER 2017		DENTAL INSURANCE BENEFITS-OCT 2 001-1160	234.96
					Total :	234.96
207688	10/6/2017	887627 STANDARD INSURANCE	OCTOBER 2017		LIFE/AD&D INSURANCE-OCT 2017 001-1160	3,132.59
					Total :	3,132.59
207689	10/6/2017	103596 CALIFORNIA VISION SERVICE PLAN	OCTOBER 2017		VISION INSURANCE-OCT 2017 001-1160	2,353.23
					Total :	2,353.23

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Voucher List

CITY OF SAN FERNANDO

Page: 2

Bank code : bank3

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
207797	10/18/2017	892550 GOVEA, DAVID	SEPT 2017		COMMISSIONER'S STIPEND 001-115-0000-4111	50.00
					Total :	50.00
207798	10/20/2017	103648 CITY OF SAN FERNANDO	PR 10-20-17		REIMB FOR PAYROLL W/E 10-13-17 001-1003	319,533.99
					007-1003	117.13
					008-1003	1,321.74
					010-1003	635.56
					017-1003	28.73
					018-1003	61,147.75
					027-1003	4,392.96
					029-1003	1,846.22
					041-1003	8,531.18
					043-1003	13,105.93
					070-1003	30,942.07
					072-1003	23,013.88
					119-1003	1,364.35
					120-1003	447.26
					Total :	466,428.75
8 Vouchers for bank code :		bank3			Bank total :	1,081,093.30
8 Vouchers in this report					Total vouchers :	1,081,093.30

Voucher Registers are not final until approved by Council.

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Alex P. Meyerhoff, City Manager
By: Michael E. Okafor, Personnel Manager

Date: November 20, 2017

Subject: Consideration to Adopt a Resolution Amending the Salary Schedule to Include the Negotiated Minimum Wage Increases for Certain Part-time Classifications

RECOMMENDATION:

It is recommended that the City Council:

- a. Adopt Resolution No. 7823 (Attachment "A") amending the Salary Schedule to include the negotiated minimum wage increases for certain part-time classifications;
- b. Approve the implementation of the negotiated wages for all applicable part-time employees including the unrepresented confidential part-time employees; and
- c. Authorize the City Manager to make non-substantive corrections and execute all related documents.

BACKGROUND:

1. In April 2010, the City and the San Fernando Part-time Employees' Bargaining Unit (SFPEBU) executed a three-year Memorandum of Understanding (MOU) for the term of July 1, 2009 through June 30, 2012 (Contract No. 1636).
2. In August 2012, the City and SFPEBU executed a Side Letter Agreement (Contract No. 1636[a]) extending the terms of the existing MOU through June 30, 2014.
3. In March 2016, the City and SFPEBU agreed to meet and begin negotiations for a successor MOU.
4. On November 21, 2016, the City Council approved the newly negotiated MOU between the City and SFPEBU for the term of July 1, 2016 through June 30, 2020 (Contract No. 1838). The approved MOU includes an agreement by the City to comply with stipulations of the State Senate Bill (SB) 3 that was signed into law on April 4, 2016 implementing certain

Consideration to Adopt a Resolution Amending the Salary Schedule to Include the Negotiated Minimum Wage Increases for Certain Part-time ClassificationsPage 2 of 2

minimum wage increases on January 1st of each year, from January 1, 2017 through January 1, 2022.

5. On January 17, 2017, the City Council approved an amendment to the Salary Schedule to include minimum wage increases from \$10.00 to \$10.50 per hour for all applicable part-time job classifications.

ANALYSIS:

Pursuant to the negotiated MOU between the City and SFPEBU for the term of July 1, 2016 through June 30, 2020 (Contract No. 1838), and pursuant to SB 3, effective January 1, 2018, the minimum wage in California shall increase from \$10.50 to \$11.00 per hour.

The existing Salary Schedule (Resolution No. 7796) that was approved on June 19, 2017 needs to be amended to reflect the negotiated minimum wage increases for certain part-time job classifications, and maintain existing separation between classifications impacted by the new minimum wage.

Eight (8) part-time job classifications will be impacted by this, and the new rates are reflected in the proposed Resolution No. 7823 in compliance with the mandatory State law, as well as SFPEBU MOU stipulations.

BUDGET IMPACT:

The total additional cost of the State mandated minimum wage increase for Fiscal Year (FY) 2017-2018 is approximately \$6,000. Sufficient funds are included in the FY 2017-2018 Adopted Budget to cover the cost of this increase.

CONCLUSION:

Approval of the amended Salary Schedule is necessary to make it consistent with the negotiated MOU provisions, as well as the State Minimum Wage Law.

ATTACHMENT:

- A. Resolution No.7823

ATTACHMENT "A"

RESOLUTION NO. 7823

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF
SAN FERNANDO, CALIFORNIA, AMENDING PORTIONS OF
SECTIONS 1, 2 AND 3 OF RESOLUTION NO. 7796, ADOPTED
JUNE 19, 2017**

**THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY
RESOLVE, FIND, DETERMINE, AND ORDER AS FOLLOWS:**

SECTION 1: That that portion of Section 1, Schedule "H" of Resolution No. 7796, adopted June 19, 2017, as amended, be further amended by deleting "Schedule H for Part-time Hourly Employees" on pages 7 and 8, respectively, and replacing it with the following, effective January 1, 2018:

SCHEDULE H					
FOR					
PART-TIME HOURLY EMPLOYEES					
SALARY RANGE NUMBER	STEP A	STEP B	STEP C	STEP D	STEP E
44	11.00	11.47	11.95	12.46	13.00
45	11.17	11.67	12.21	12.76	13.34
46	11.30	11.80	12.32	12.88	13.46
47	11.53	12.03	12.56	13.10	13.68
48	11.69	12.22	12.76	13.34	14.01
49	11.74	12.26	12.81	13.40	14.04
50	11.82	12.32	12.88	13.46	14.09
51	11.93	12.43	12.98	13.56	14.17
52	12.07	12.60	13.14	13.72	14.33
53	12.13	12.65	13.19	13.77	14.38
54	12.23	12.75	13.30	13.88	14.48
55	12.33	12.86	13.40	13.98	14.51
56	12.45	12.96	13.51	14.09	14.55
57	12.56	13.07	13.61	14.19	14.65
58	12.66	13.17	13.72	14.29	14.76
59	12.76	13.28	13.82	14.40	14.86
60	12.87	13.38	13.93	14.50	14.97
61	12.97	13.49	14.03	14.61	15.07
62	13.08	13.59	14.14	14.71	15.24
63	13.18	13.70	14.24	14.82	15.49
64	13.29	13.80	14.35	14.92	15.64

Schedule H for Part-time Employees (Continued)

SALARY RANGE NUMBER	STEP A	STEP B	STEP C	STEP D	STEP E
65	13.39	13.91	14.45	15.05	15.79
66	13.50	14.01	14.56	15.20	15.95
67	13.60	14.12	14.66	15.34	16.12
68	13.81	14.22	14.77	15.50	16.28
69	14.02	14.33	14.90	15.65	16.43
70	14.13	14.53	15.05	15.80	16.59
71	14.48	15.18	15.90	16.65	17.45
72	14.55	15.22	15.85	16.69	17.49
73	14.60	15.27	16.00	16.74	17.53
74	14.63	15.32	16.04	16.80	17.60
75	14.73	15.43	16.15	16.90	17.70

SECTION 2: That that portion of subsection (B) of Section 2 of Resolution No. 7796, adopted June 19, 2017, as amended, be further amended by deleting the following, effective January 1, 2018:

<u>CLASSIFICATION</u>	<u>SALARY RANGE NUMBER/ SCHEDULE</u>	<u>STEP A</u>	<u>STEP B</u>	<u>STEP C</u>	<u>STEP D</u>	<u>STEP E</u>
Crossing Guard	44H	10.50	10.94	11.40	11.89	12.40
Day Camp Counselor	44H	10.50	10.94	11.40	11.89	12.40
Junior Cadet	45H	10.66	11.14	11.65	12.18	12.73
Pool Attendant/Cashier	44H	10.50	10.94	11.40	11.89	12.40
Recreation Leader I	44H	10.50	10.94	11.40	11.89	12.40
Recreation Leader II	47H	11.00	11.48	11.98	12.50	13.05
Recreation Leader III	71H	13.82	14.48	15.17	15.89	16.65
Senior Day Camp Counselor	52H	11.52	12.02	12.54	13.09	13.67

SECTION 3: That that portion of subsection (B) of Section 2 of Resolution No. 7796, adopted June 19, 2017, as amended, be further amended by adding the following, effective January 1, 2018:

<u>CLASSIFICATION</u>	<u>SALARY RANGE NUMBER/ SCHEDULE</u>	<u>STEP A</u>	<u>STEP B</u>	<u>STEP C</u>	<u>STEP D</u>	<u>STEP E</u>
Crossing Guard	44H	11.00	11.47	11.95	12.46	13.00
Day Camp Counselor	44H	11.00	11.47	11.95	12.46	13.00
Junior Cadet	45H	11.17	11.67	12.21	12.76	13.34
Pool Attendant/Cashier	44H	11.00	11.47	11.95	12.46	13.00
Recreation Leader I	44H	11.00	11.47	11.95	12.46	13.00
Recreation Leader II	47H	11.53	12.03	12.56	13.10	13.68
Recreation Leader III	71H	14.48	15.18	15.90	16.65	17.45
Senior Day Camp Counselor	52H	12.07	12.60	13.14	13.72	14.33

SECTION 4: That all portions of subsection (B) of Section 3 of Resolution No. 7796, adopted June 19, 2017, as amended, be further amended by deleting all the portions therein, and adding provisions on salaries and benefits as approved in the Part-time Employees' Bargaining Unit (SFPEBU) Memorandum of Understanding (MOU) Contract No. 1838, adopted November 21, 2016.

SECTION 5: That the salary and benefit provisions as approved in Contract No. 1838, and adopted November 21, 2016, be extended to all part-time employees, including unrepresented confidential part-time employees.

SECTION 6: Except as amended herein, all other provisions of Resolution No. 7796, adopted June 19, 2017, remains unchanged and in full force and effect.

SECTION 7: The City Clerk shall certify to the adoption of this Resolution and shall cause this Resolution and her certification to be filed in the office of the City Clerk.

PASSED, APPROVED, AND ADOPTED this 20th day of November, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) ss
CITY OF SAN FERNANDO)

I HEREBY CERTIFY that the foregoing Resolution was approved and adopted at a regular meeting of the City Council held on the 20th day of November, 2017, by the following vote to wit:

AYES:

NOES:

ABSENT:

Elena G. Chávez, City Clerk

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Alexander P. Meyerhoff, City Manager
By: Nick Kimball, Finance Director

Date: November 20, 2017

Subject: Consideration to Adopt a Resolution Appointing the City's Representatives to the Independent Cities Risk Management Authority (ICRMA) Governing Board

RECOMMENDATION:

It is recommended that the City Council adopt Resolution No. 7825 (Attachment "A") appointing City Manager Alexander P. Meyerhoff as the Primary Representative, re-affirming Finance Director Nick Kimball as the Alternate Representative, and re-affirming Personnel Manager Michael Okafor as the Substitute Alternate representative on the Independent Cities Risk Management Authority (ICRMA) Governing Board.

BACKGROUND:

1. ICRMA is a joint powers authority that provides risk management programs for approximately 21 member cities in the area of general liability, workers' compensation, property, earthquake and other related pooled insurance coverages.
2. The City of San Fernando has been a member of ICRMA since July 1, 1986, and is required to have a primary and alternate representative appointed to the Governing Board to attend and vote during Board meetings. ICRMA also allows cities to have a substitute alternate representative who can attend and vote during board meetings, if the other members cannot attend.
3. Currently, Mayor Sylvia Ballin is appointed as the Primary Representative, Finance Director Nick Kimball is appointed as the Alternate Representative, and Personnel Manager Michael Okafor is appointed as the Substitute Alternate Representative.
4. Governing Board meetings are currently held on the second Wednesday of even-numbered months. The meetings are during the day and typically last approximately three to four hours.

Consideration to Adopt a Resolution Appointing the City's Representatives to the Independent Cities Risk Management Authority (ICRMA) Governing BoardPage 2 of 2

ANALYSIS:

Insurance and risk management programs tend to be specialized, technical and have a potentially significant impact on the City's finances. Therefore, it is in the City's best interest to appoint Governing Board members that have risk management experience and are available to attend meetings regularly.

Due to the many demands on Mayor Ballin's time, including her participation on the Metropolitan Water District (MWD) and Independent Cities Financing Authority (ICFA) Boards, it is recommended that City Manager Meyerhoff replace Mayor Ballin as the City's Primary Representative. City Manager Meyerhoff has experience serving as a board member on other insurance risk pools and his appointment will ensure consistent representation of the City's interests on the Board.

Adoption of a new Resolution is required by ICRMA when board members are changed or added. Therefore, a new Resolution is required to appoint a new representative.

BUDGET IMPACT:

There is no budget impact associate with adoption of this Resolution; however, it is advantageous to appoint City Manager Meyerhoff as a representative to replace Mayor Ballin on the Governing Board due to the potential financial impact of risk management and insurance programs.

CONCLUSION:**CONCLUSION:**

Approval of the Governing Board Members will ensure the City will continue to be properly represented at ICRMA Governing Board meetings.

ATTACHMENT:

A. Resolution No. 7825

ATTACHMENT "A"**RESOLUTION NO. 7825****A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SAN FERNANDO, CALIFORNIA APPOINTING A DELEGATE, ALTERNATE, AND SUBSTITUTE ALTERNATE TO THE GOVERNING BOARD OF THE INDEPENDENT CITIES RISK MANAGEMENT AUTHORITY (ICRMA)**

WHEREAS, the City of San Fernando ("City") is a member of the Independent Cities Risk Management Authority ("ICRMA"), a joint powers authority created pursuant to the provisions of the California Government Code;

WHEREAS, ICRMA provides a Liability Program, Workers' Compensation Program, Property Program, and other coverage programs for its members;

WHEREAS, the Joint Powers Agreement provides that the Governing Board of the Authority shall be comprised of one representative from each member;

WHEREAS, the City Council of each member may select and change any of its representatives by filing a resolution with ICRMA; an

WHEREAS, the City council may appoint legislative member(s) or staff member(s) to serve on the Governing Board;

WHEREAS staff members shall have one of the following positions, or equivalent: City Attorney, Assistant City Attorney, Financial Officer, City Administrator/Manager, Assistant/Deputy City Administrator/Manager, Assistant to City Manager, Risk Manager, Human Resources Director/Manager, or Administrative Services Director; and

WHEREAS, City desires to designate its representative(s) to the ICRMA Governing Board.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY RESOLVE, FIND, DETERMINE AND ORDER AS FOLLOWS:

SECTION 1. That Alexander P. Meyerhoff (a staff person) is hereby appointed to serve as a delegate on the ICRMA Governing Board.

SECTION 2. That Nick Kimball (a staff person) is hereby appointed as the alternate, to serve on the ICRMA Governing Board in the absence of the primary member noted in Section 1 above.

SECTION 3. That Michael Okafor (a staff person) is hereby appointed as the substitute alternate, to serve on the ICRMA Governing Board in the absence of the primary and alternate members noted in Sections 1 and 2 above.

SECTION 4. That the individuals designated by this City Council as the City's delegate, alternate and substitute alternate to the ICRMA Governing Board are hereby confirmed and designated as the City's delegates for all purposes of representing the City's interests and exercising the authority of the City with respect to all matters delegated to the Governing Board and signing all amendments as are contemplated to be approved by the Governing Board.

RESOLVED, that a certified copy of this Resolution shall be provided to the Executive Director of ICRMA.

PASSED, APPROVED, AND ADOPTED this 20th day of November, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) ss
CITY OF SAN FERNANDO)

I HEREBY CERTIFY that the foregoing Resolution was approved and adopted at a regular meeting of the City Council held on the 20th day of November, 2017, by the following vote to wit:

AYES:

NOES:

ABSENT:

Elena G. Chávez, City Clerk

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Alexander Meyerhoff, City Manager
By: Jack Wong, Interim Director of Community Development

Date: November 20, 2017

Subject: Consideration to Approve Annual General Plan Housing Element Progress Reports for Calendar Years 2015 and 2016

RECOMMENDATION:

It is recommended that the City Council:

- a. Open the Public Hearing and receive any comments from the public regarding the General Plan Housing Element Annual Progress Reports for the Calendar Years 2015 and 2016, and thereafter close the Public Hearing; and
- b. Consider and approve the General Plan Housing Element Annual Progress Reports for the Calendar Years 2015 and 2016; and
- c. Authorize staff to forward the 2015 and 2016 Housing Element Annual Progress Reports to the California Department of Housing and Community Development (HCD) and the Governor's Office of Planning and Research (OPR).

BACKGROUND:

1. The purpose of this item is for the City Council to consider the status of the General Plan Housing Element Annual Progress Reports (APR) for calendar years 2015 and 2016, and the progress of its implementation, which is to be reported to the California Department of Housing Community Development (HCD) and the Governor's Office of Planning and Research (OPR). Government Code Section 65400 establishes the requirement that each city and county prepare an annual report on the status of its Housing Element, and the actions taken towards completion of the programs and status of the local government's compliance with the deadlines in its housing element.
2. The current Housing Element 2013-2021 (5th Cycle), was adopted by the City on January 21, 2014 and certified by HCD on March 18, 2014 as being in full compliance with State housing element law. The Housing Element establishes the City's strategy for meeting community housing needs for the period 2013-2021 and is one of seven integral and interrelated elements of the General Plan.

Consideration to Approve Annual General Plan Housing Element Progress Reports for Calendar Years 2015 and 2016

Page 2 of 3

ANALYSIS:

The 2015 and 2016 Annual Reports reflect the City's progress during the 2nd and 3rd year of the Regional Housing Need Allocation (RHNA) projection period (January 1, 2014 - October 1, 2021)¹ for the 5th Cycle Housing Element. The RHNA allocates the amount of housing growth each jurisdiction must plan for in their housing element by providing "adequate sites" through zoning. So long as a jurisdiction provides sufficient sites and does not impose undue constraints to development, the City is not penalized for falling short of its RHNA target. However, pursuant to Government Code Section 65863, jurisdictions are required to maintain an adequate sites inventory throughout the planning period. For example, to the extent that high density sites identified as accommodating the lower income RHNA are developed with fewer units, the jurisdiction will need to make up any shortfall on other sites, including rezoning as necessary.

As presented in Table 1 below, the City was allocated a total RHNA of 217 units for the planning period. During calendar years 2014-2016, a total of 83 residential building permits were issued. The City has made progress in addressing its lower income housing needs both through the provision of deed restricted affordable units, as well as through the provision of accessory dwelling units. The City will need to continue to provide sites for a mix of single-family, multi-family and mixed use housing, supported by a variety of programs to enhance affordability, to accommodate its outstanding RHNA throughout the planning period.

The City continues to actively implement the policies of the General Plan including the goals, policies and programs of the Housing Element. The APR represents the progress the City has made towards implementing the General Plan Housing Element during the Calendar Years 2015 and 2016 reporting period.

Table 1: Regional Housing Needs Allocation Progress: 2014-2016					
Income Level	2014-2021 RHNA Allocation	Building Permits			Remaining RHNA Target by Income Level
		2014	2015	2016	
Very Low	87	28	0	0	27
Low		4	5	5	18
Moderate	35	0	0	0	35
Above Moderate	95	27	0	9	59
Total	217	59	5	14	139

¹ While the RHNA projection period for the 5th Cycle Housing Element begins January 1, 2014, the Housing Element planning period begins slightly earlier on October 15, 2013. Measurement of RHNA progress for the 2013-2021 Housing Element thus begins in calendar year 2014.

Consideration to Approve Annual General Plan Housing Element Progress Reports for Calendar Years 2015 and 2016Page 3 of 3

Legal Notice Of Public Hearing.

This matter was set for Public Hearing in accordance with the requirements of the Government Code Section 65905 and the requirements of Section 106-72 of the City's Municipal Code. Legal Notice of the Public Hearing for the General Plan Housing Element Annual Progress Report was posted at City Hall, on November 15, 2017, and published in a newspaper of general circulation (*The San Fernando Valley Sun*) on November 9, 2017.

BUDGET IMPACT:

The City hired a consultant to prepare the Housing Element at a cost of \$2,160, which was included in the Fiscal Year 2017-2018 Adopted Budget. No additional fiscal impacts are associated with the submittal of the General Plan Housing Element Annual Progress Reports for calendar years 2015 and 2016 to HCD and OPR.

CONCLUSION:

The City continues to diligently administer and implement the goals, objectives, and strategies to increase the diversity of housing options in the City. It is recommended that the City Council approve the attached Housing Element Annual Progress Reports for the calendar years 2015 and 2016 and authorize staff to forward the Housing Element Annual Progress Reports to the California Department of Housing and Community Development and the Governor's Office of Planning and Research.

ATTACHMENTS:

- A. Annual Housing Element Progress Report 2015
- B. Annual Housing Element Progress Report 2016

ANNUAL ELEMENT PROGRESS REPORT
Housing Element Implementation
(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO
Reporting Period 01/01/2015 - 12/31/2015

Pursuant to GC 65400 local governments must provide by April 1 of each year the annual report for the previous calendar year to the legislative body, the Office of Planning and Research (OPR), and the Department of Housing and Community Development (HCD). By checking the “Final” button and clicking the “Submit” button, you have submitted the housing portion of your annual report to HCD only. Once finalized, the report will no longer be available for editing.

The report must be printed and submitted along with your general plan report directly to OPR at the address listed below:

Governor’s Office of Planning and Research
P.O. Box 3044
Sacramento, CA 95812-3044

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

Table A

Annual Building Activity Report Summary - New Construction Very Low-, Low-, and Mixed-Income Multifamily Projects

Housing Development Information								Housing with Financial Assistance and/or Deed Restrictions		Housing without Financial Assistance or Deed Restrictions	
1	2	3	4				5	5a	6	7	8
Project Identifier (may be APN No., project name or address)	Unit Category	Tenure R=Renter O=Owner	Affordability by Household Incomes				Total Units per Project	Est. # Infill Units*	Assistance Programs for Each Development	Deed Restricted Units	Note below the number of units determined to be affordable without financial or deed restrictions and attach an explanation how the jurisdiction determined the units were affordable. Refer to instructions.
			Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income			See Instructions	See Instructions	
Second units	SU	Renter	0	5	0	0	5	5			Rent survey of guesthouses in San Fernando and immediately surrounding areas evidence average rents of \$825, within the level affordability for one person, low income households.
(9) Total of Moderate and Above Moderate from Table A3						0	0				
(10) Total by Income Table A/A3			0	5	0	0					
* Note: These fields are voluntary (11) Total Extremely Low-Income Units*			0								

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

Table A2

Annual Building Activity Report Summary - Units Rehabilitated, Preserved and Acquired pursuant to GC Section 65583.1(c)(1)

Please note: Units may only be credited to the table below when a jurisdiction has included a program in its housing element to rehabilitate, preserve or acquire units to accommodate a portion of its RHNA which meet the specific criteria as outlined in GC Section 65583.1(c)(1)

Activity Type	Affordability by Household Incomes				(4) The Description should adequately document how each unit complies with subsection (c)(7) of Government Code Section 65583.1
	Extremely Low-Income*	Very Low-Income	Low-Income	TOTAL UNITS	
(1) Rehabilitation Activity	0	0	0	0	
(2) Preservation of Units At-Risk	0	0	0	0	
(3) Acquisition of Units	0	0	0	0	
(5) Total Units by Income	0	0	0	0	

* Note: This field is voluntary

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

Table A3
Annual building Activity Report Summary for Above Moderate-Income Units
(not including those units reported on Table A)

	1. Single Family	2. 2 - 4 Units	3. 5+ Units	4. Second Unit	5. Mobile Homes	6. Total	7. Number of infill units*
No. of Units Permitted for Moderate	0	0	0	0	0	0	0
No. of Units Permitted for Above Moderate	0	0	0	0	0	0	0

* Note: This field is voluntary

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

Table B

Regional Housing Needs Allocation Progress

Permitted Units Issued by Affordability

Enter Calendar Year starting with the first year of the RHNA allocation period. See Example.												Total Units to Date (all years)	Total Remaining RHNA by Income Level
Income Level		RHNA Allocation by Income Level	Year 1	Year 2	Year 3	Year 4	Year 5	Year 6	Year 7	Year 8	Year 9		
Very Low	Deed Restricted	55	0	28	0	0	0	0	0	0	0	28	27
	Non-Restricted		0	0	0	0	0	0	0	0	0		
Low	Deed Restricted	32	0	4	0	0	0	0	0	0	0	9	23
	Non-Restricted		0	0	5	0	0	0	0	0	0		
Moderate		35	0	0	0	0	0	0	0	0	0	0	35
Above Moderate		95	0	27	0	0	0	0	0	0	-	27	68
Total RHNA by COG. Enter allocation number:		217	0	59	5	0	0	0	0	0	0	64	153
Total Units ▶▶▶													
Remaining Need for RHNA Period ▶▶▶▶▶													

Note: units serving extremely low-income households are included in the very low-income permitted units totals.

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

Table C

Program Implementation Status

Program Description (By Housing Element Program Names)		Housing Programs Progress Report - Government Code Section 65583. Describe progress of all programs including local efforts to remove governmental constraints to the maintenance, improvement, and development of housing as identified in the housing element.	
Name of Program	Objective	Timeframe in H.E.	Status of Program Implementation
6. Conservation of Existing and Future Affordable Units	Monitor the status of the existing and future affordable rental housing stock in San Fernando. Work with property owners, interest groups and the State and Federal governments to conserve its affordable housing stock.	31-Dec-21	City continues to monitor existing deed restricted affordable units on an annual basis.
7. Monitor Residential Capacity	Monitor the consumption of residential acreage to ensure an adequate inventory is available to meet the City's RHNA obligations. Develop and implement a formal ongoing (project by project) evaluation procedure pursuant to Government Code Section 65863.	31-Dec-21	City continues to monitor new residential and commercial construction in areas zoned for residential land uses on an annual basis and compares development sites to land use inventory incorporated in City Housing Element as well as mix of housing types based on income level affordability as noted in RHNA Allocation numbers for current reporting period.
8. Removal of Governmental Constraints	Monitor changes in State and Federal laws and revised City policies, programs, and regulations as necessary and appropriate.	31-Dec-21	City has adopted new zoning regulations ensuring compliance with State mandated housing requirements including, but not limited to, reasonable accommodation requests, density bonus provisions, supportive and transitional housing provisions and accommodation of homeless shelters.

9. Housing Choice Voucher Assistance Program	Support HaCOLA's administration of the program to assist an average of 35 extremely low and very low income households annually. Encourage landlords to register units; prepare bilingual HaCOLA program contact information.	31-Dec-21	A total of 81 households are receiving Section 8 rental assistance, including 39 standard housing vouchers; 37 project based vouchers; 3 continuum of care vouchers; and 2 VASH (veterans) vouchers. City continues to work with Affordable Housing developers in the City to refer prospective tenants to HaCOLA to apply for and/or be placed on the Section 8 Voucher waiting list. City also works with property owners recommending inclusion of their property on HaCOLA's list of registered units.
10. Fair Housing	Contract with the FHCSFV or another fair housing service provider; disseminate brochures; coordinate fair housing education with community events.	31-Dec-21	City provides a list of housing resources at the public counter to property owners and renters including a brochure for the Housing Rights Center with offices in the San Fernando Valley.
11. Homeownership Programs	Promote County homebuyer programs. Prepare and distribute bi-lingual program flyer. Advertise County programs at the public counter and on the City's website.	31-Dec-21	City continues to promote County Homeownership Programs including distribution of printed County website information at the City's public counter.
1. Residential Rehabilitation Loan Program	Assist 20 households during the planning period, subject to available funding.	31-Dec-21	No monies were available during 2015 to fund the program due to dissolution of the city redevelopment agency. However, as the Housing Funds balance begins to grow from residential loan repayments/refinancing, the city may be able to re-initiate the Rehabilitation Loan Program in the future. As of June 30, 2015, the cash balance was \$92,000, with the first \$250,000 earmarked as a loan for an affordable housing project.
2. Neighborhood Preservation and Revitalization Program (CAPP)	Implement CAPP within designated focus areas.	31-Dec-21	While the City no longer implements the CAPP program in designated focus areas, code enforcement is provided on a citywide basis.
3. Housing Inspection Program	Complete inspections of approximately 200 apartment units annually. Inspect ownership units upon resale.	31-Dec-21	While the inspection fee in the Ordinance has not been implemented, Building & Safety conducts inspections on a complaint basis, and on apartment buildings with numerous repeat violations. In 2015, 102 multi-family units were inspected. Code Enforcement also inspects 1-2 unit properties.
4. Lead Based Paint Awareness	Remediate lead cases. Coordinate with LA County and Pacoima Beautiful on educational programs and identification of funding sources.	31-Dec-21	City continues to provide brochures to prospective building permit applicants on compliance with applicable lead based paint procedures. Also, all construction documents are stamped with lead based paint abatement procedures.
5. Facilitate Affordable and Special Needs Housing Development	Coordinate with affordable housing developers, offer regulatory concessions and incentives, and identify new funding sources to facilitate production of at least 195 new affordable and special needs housing units.	31-Dec-21	No additional deed restricted affordable housing was built during 2015.

ANNUAL ELEMENT PROGRESS REPORT
Housing Element Implementation
(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2015 - 12/31/2015

General Comments:

ANNUAL ELEMENT PROGRESS REPORT
Housing Element Implementation
(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO
Reporting Period 01/01/2016 - 12/31/2016

Pursuant to GC 65400 local governments must provide by April 1 of each year the annual report for the previous calendar year to the legislative body, the Office of Planning and Research (OPR), and the Department of Housing and Community Development (HCD). By checking the “Final” button and clicking the “Submit” button, you have submitted the housing portion of your annual report to HCD only. Once finalized, the report will no longer be available for editing.

The report must be printed and submitted along with your general plan report directly to OPR at the address listed below:

Governor’s Office of Planning and Research
P.O. Box 3044
Sacramento, CA 95812-3044

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2016 - 12/31/2016

Table A

Annual Building Activity Report Summary - New Construction Very Low-, Low-, and Mixed-Income Multifamily Projects

Housing Development Information						Housing with Financial Assistance and/or Deed Restrictions		Housing without Financial Assistance or Deed Restrictions			
1	2	3	4				5	5a	6	7	8
Project Identifier (may be APN No., project name or address)	Unit Category	Tenure R=Renter O=Owner	Affordability by Household Incomes				Total Units per Project	Est. # Infill Units*	Assistance Programs for Each Development	Deed Restricted Units	Note below the number of units determined to be affordable without financial or deed restrictions and attach an explanation how the jurisdiction determined the units were affordable. Refer to instructions.
			Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income			See Instructions	See Instructions	
Second units	SU	Renter	0	5	0	0	5	5			Rent survey of guesthouses in San Fernando and immediately surrounding areas evidence average rents of \$825, within the level of affordability for one person, low income households.
(9) Total of Moderate and Above Moderate from Table A3						0	9				
(10) Total by Income Table A/A3			0	5	0	9					
* Note: These fields are voluntary (11) Total Extremely Low-Income Units*			0								

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2016 - 12/31/2016

Table A2

Annual Building Activity Report Summary - Units Rehabilitated, Preserved and Acquired pursuant to GC Section 65583.1(c)(1)

Please note: Units may only be credited to the table below when a jurisdiction has included a program in its housing element to rehabilitate, preserve or acquire units to accommodate a portion of its RHNA which meet the specific criteria as outlined in GC Section 65583.1(c)(1)

Activity Type	Affordability by Household Incomes				(4) The Description should adequately document how each unit complies with subsection (c)(7) of Government Code Section 65583.1
	Extremely Low-Income*	Very Low-Income	Low-Income	TOTAL UNITS	
(1) Rehabilitation Activity	0	0	0	0	
(2) Preservation of Units At-Risk	0	0	0	0	
(3) Acquisition of Units	0	0	0	0	
(5) Total Units by Income	0	0	0	0	

* Note: This field is voluntary

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2016 - 12/31/2016

Table A3
Annual building Activity Report Summary for Above Moderate-Income Units
(not including those units reported on Table A)

	1. Single Family	2. 2 - 4 Units	3. 5+ Units	4. Second Unit	5. Mobile Homes	6. Total	7. Number of infill units*
No. of Units Permitted for Moderate	0	0	0	0	0	0	0
No. of Units Permitted for Above Moderate	1	8	0	0	0	9	9

* Note: This field is voluntary

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2016 - 12/31/2016

Table B

Regional Housing Needs Allocation Progress

Permitted Units Issued by Affordability

Enter Calendar Year starting with the first year of the RHNA allocation period. See Example.												Total Units to Date (all years)	Total Remaining RHNA by Income Level
Income Level		RHNA Allocation by Income Level	Year 1	Year 2	Year 3	Year 4	Year 5	Year 6	Year 7	Year 8	Year 9		
Very Low	Deed Restricted	55	0	28	0	0	0	0	0	0	0	28	27
	Non-Restricted		0	0	0	0	0	0	0	0	0		
Low	Deed Restricted	32	0	4	0	0	0	0	0	0	0	14	18
	Non-Restricted		0	0	5	5	0	0	0	0	0		
Moderate		35	0	0	0	0	0	0	0	0	0	0	35
Above Moderate		95	0	27	0	9	0	0	0	0	-	36	59
Total RHNA by COG. Enter allocation number:		217	0	59	5	14	0	0	0	0	0	78	139
Total Units ▶ ▶ ▶													
Remaining Need for RHNA Period ▶ ▶ ▶ ▶ ▶													

Note: units serving extremely low-income households are included in the very low-income permitted units totals.

ANNUAL ELEMENT PROGRESS REPORT

Housing Element Implementation

(CCR Title 25 §6202)

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Reporting Period 01/01/2016 - 12/31/2016

Table C

Program Implementation Status

Program Description (By Housing Element Program Names)		Housing Programs Progress Report - Government Code Section 65583. Describe progress of all programs including local efforts to remove governmental constraints to the maintenance, improvement, and development of housing as identified in the housing element.	
Name of Program	Objective	Timeframe in H.E.	Status of Program Implementation
1. Residential Rehabilitation Loan Program	Assist 20 households during the planning period, subject to available funding.	31-Dec-21	No monies were available during 2016 to fund the program due to dissolution of the city redevelopment agency. However, as the Housing Funds balance begins to grow from residential loan repayments/refinancing, the city may be able to re-initiate the Rehabilitation Loan Program in the future. As of June 30, 2016, the cash balance was \$176,000, with the first \$250,000 earmarked as a loan for an affordable housing project.
2. Neighborhood Preservation and Revitalization Program (CAPP)	Implement CAPP within designated focus areas.	31-Dec-21	While the City no longer implements the CAPP program in designated focus areas, code enforcement is provided on a citywide basis.
3. Housing Inspection Program	Complete inspections of approximately 200 apartment units annually. Inspect ownership units upon resale.	31-Dec-21	While the inspection fee in the Ordinance has not been implemented, Building & Safety conducts inspections on a complaint basis, and on apartment buildings with numerous repeat violations. In 2016, 119 multi-family units were inspected. Code Enforcement also inspects 1-2 unit properties.
4. Lead Based Paint Awareness	Remediate lead cases. Coordinate with LA County and Pacoima Beautiful on educational programs and identification of	31-Dec-21	City continues to provide brochures to prospective building permit applicants on compliance with applicable lead based paint abatement procedures.

	funding sources.		
5. Facilitate Affordable and Special Needs Housing Development	Coordinate with affordable housing developers, offer regulatory concessions and incentives, and identify new funding sources to facilitate production of at least 195 new affordable and special needs housing units.	31-Dec-21	No additional deed restricted affordable housing was built during 2016.
6. Conservation of Existing and Future Affordable Units	Monitor the status of the existing and future affordable rental stock in San Fernando. Work with property owners, interest groups and the State and Federal governments to conserve its affordable housing stock.	31-Dec-21	City continues to monitor existing deed restricted affordable units on an annual basis.
7. Monitor Residential Capacity	Monitor the consumption of residential acreage to ensure an adequate inventory is available to meet the City's RHNA obligations. Develop and implement a formal ongoing (project by project) evaluation procedure pursuant to Government Code Section 65863.	31-Dec-21	City continues to monitor new residential and commercial construction in areas zoned for residential land uses on an annual basis and compares development sites to land use inventory incorporated in City Housing Element as well as mix of housing types based on income level affordability as noted in the RHNA Allocation numbers for the current reporting period.
8. Removal of Governmental Constraints	Monitor changes in State and Federal laws and revised City policies, programs, and regulations as necessary and appropriate.	31-Dec-21	City has adopted new zoning regulations ensuring compliance with State mandated housing requirements including, but not limited to, reasonable accommodation requests, density bonus provisions, supportive and transitional housing provisions and accommodation of homeless shelters.
9. Housing Choice Voucher Assistance Program	Support HaCOLA's administration of the program to assist an average of 35 extremely low and very low income households annually. Encourage landlords to register units; prepare bilingual HaCOLA program contact information.	31-Dec-21	A total of 81 households are receiving Section 8 rental assistance, including 39 standard housing vouchers; 37 project based vouchers; 3 continuum of care vouchers; and 2 VASH (veterans) vouchers. City continues to work with Affordable Housing developers in the City to refer prospective tenants to HaCOLA to apply for and/or be placed on the Section 8 Voucher waiting list. City also works with property owners recommending inclusion of their property on HaCOLA's list of registered units.
10. Fair Housing	Contract with the FHCSFV or another fair housing service provider; disseminate brochures; coordinate fair housing education with community events.	31-Dec-21	City provides a list of housing resources at the public counter to property owners and renters including a brochure for the Housing Rights Center with offices in the San Fernando Valley.
11. Homeownership Programs	Promote County homebuyer programs. Prepare and distribute bi-lingual program flyer. Advertise County programs at the public counter and on the City's website.	31-Dec-21	City continues to promote County Homeownership Programs including distribution of printed County website information at the City's public counter.

ANNUAL ELEMENT PROGRESS REPORT
Housing Element Implementation
(CCR Title 25 §6202)

Jurisdiction SAN FERNANDO

Reporting Period 01/01/2016 - 12/31/2016

General Comments:

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Alexander P. Meyerhoff, City Manager
By: Jack Wong, Interim Community Development Director

Date: November 20, 2017

Subject: Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

RECOMMENDATION:

It is recommended that the City Council:

- a. Open the Public Hearing and receive City staff presentation;
- b. Pose questions to City staff;
- c. Receive public testimony;
- d. Pose further questions to City staff based on receipt of public testimony;
- e. Close the Public Hearing and commence deliberations on the proposed Corridors Specific Plan SP-5 and related EIR and General Plan and Zoning Ordinance text changes and maps;
- f. Adopt Resolution No. 7826 (Attachment "A"):
 - i. Certifying the Final Environmental Impact Report SCH No. 2015121088 (Exhibit "C" to Attachment "A"), pursuant to the California Environmental Quality Act, finding that the proposed San Fernando Corridors Specific Plan SP-5 (Exhibit "D" to Attachment "A") and related amendments to the City's General Plan and Zoning Ordinance text and maps will not have an effect on the environment that cannot be mitigated to less-than-significant impact levels, and such finding shall be contingent upon the final adoption of the amendments to the General Plan text and map, and final adoption of the San Fernando Corridors Specific Plan SP-5 and amendments to the Zoning Ordinance text and map; and

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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- ii. Approving the amendments to the City's General Plan text and map, contingent upon the final adoption of the San Fernando Corridors Specific Plan SP-5 (Exhibit "D" to Attachment "A") and amendments to the Zoning Ordinance text and map; and
- g. Introduce for first reading, in title only, and waive further reading of Ordinance No. 1621 (Attachment "B"), "An Ordinance of the City Council of the City of San Fernando, California, Approving the San Fernando Corridors Specific Plan Sp-5 and Repealing in its Entirety the Former Corridors Specific Plan Sp-4".

BACKGROUND:

1. On February 28, 2013, Metro awarded a three-year \$282,392 planning grant to the City of San Fernando for the completion of a comprehensive update to the existing SP-4 San Fernando Corridors Specific Plan and to make the necessary amendments to the General Plan text, Zoning Ordinance text, and related maps and an Environmental Impact Report (EIR). The City is to contribute an additional match of \$13,306 (\$6,306 in-kind, such as staff-time and \$7,000 funds). Metro has extended the deadline to March 31, 2018.
2. On May 19, 2014, the City Council awarded a contract to the firm of Sargent Town Planning (Consultant) to complete the project from a general solicitation to qualified consultants.
3. Throughout this process, several public meetings, and workshops have been conducted with residents, merchants, and local stakeholders. Sargent Town Planning, the consultant who is preparing the specific plan amendment, conducted several individual and small group meetings with local stakeholders in September 2014 and October 2014 and again in September 2015. The proposed specific plan amendment was also discussed by the following:
 - a. The Development Advisory Committee (DAC) conducted four public meetings on September 30, 2014, November 12, 2014, January 14, 2015, and June 17, 2015.
 - b. The City and Sargent Town Planning conducted one environmental scoping meeting on January 7, 2016 and four community workshops on November 19, 2014, January 21, 2015, August 28, 2017, and September 15, 2017.
 - c. The Planning and Preservation Commission also conducted discussions at two of its regularly scheduled meetings on August 4, 2015, October 6, 2015.
4. There have been two major phases during the development of the project. The first phase can be characterized as the initial data collection and development of concepts and

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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strategies. The first phase term is between June 2014 and May 2015. During this initial phase, the Consultant conducted extensive data collection, community participation, which resulted in the development of nascent concepts, strategies, and early forms of place making. These ideas were vetted over the next three months, between June 2015 and August 2015, with City staff and the Planning & Preservation Commission. The term of the second phase was from October 2015 to present day, wherein the Consultant initiated revisions to the Specific Plan in recognition of community input and prepared the Draft Environmental Impact Report.

5. Community input and comments were seriously considered and substantially shaped the final product, in terms of suggested uses, locations, building intensities, urban aesthetics, and place making strategies.
6. On November 7, 2017, the Planning and Preservation Commission conducted a duly noticed Public Hearing and following the Public Hearing, after consideration of the entire public record and extensive deliberation, adopted Resolution No. 2017-008 (Attachment "C"), recommending that the City Council certify the Environmental Impact Report SCH No. 2015121088, for the adoption of the San Fernando Corridors Specific Plan SP-5, and related amendments to the General Plan text and map, and Zoning Ordinance text and map, the staff report, and minutes (Attachment "D").
7. On November 10, 2017, a Notice of Public Hearing was published in the *Los Angeles Daily News* (Attachment "E") and was also physically posted at City Hall. The notice for the Public Hearing was also mailed to all owners of property within 500 feet of the Specific Plan Area.

ANALYSIS:

Environmental Review (California Environmental Quality Act (CEQA)):

Pursuant to CEQA, the City is the designated Lead Agency overseeing the environmental review for the proposed SP-5 Plan. As the Lead Agency, the City directed the preparation of an Environmental Impact Report, which identified potential impacts associated with future development allowed by the project. On December 22, 2015, the City circulated a Notice of Preparation (NOP) for review and comment, through January 20, 2016, by the public, responsible agencies, and reviewing agencies. On January 7, 2016, a Scoping Meeting was conducted to receive public input and comment and on August 10, 2017, the Draft Environmental Impact Report (EIR) was released, initiating a 45-day public review period. A Notice of Completion (NOC) of the Draft EIR was also provided on August 10, 2017 to the Governor's Office of Planning and Research State Clearinghouse for environmental review documents, along with copies for review by state agencies. A Notice of Availability (NOA) of the

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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Draft EIR for review and copies of the Draft EIR were also sent to County Clerk on August 10, 2017 and to responsible agencies, agencies that had commented on the NOP, and all other interested parties that had requested notice and copies of the Draft EIR. The City and Consultant discussed the Draft EIR at two community meetings on August 28, 2017 and September 15, 2017.

The significant impacts identified were noise during construction, the unearthing of subsurface cultural resources during construction, and decreases in intersection performance due to automobile traffic. All these impacts would occur with or without approval of the project and can be mitigated with the appropriate mitigation measures as follows:

- **Noise during construction.** The Project will allow future development within the Specific Plan Area. Construction within an existing community, particularly where potential development sites are close to existing residences, has the potential to generate construction noise levels that exceed community standards. As such, the Project could result in potential significant noise impacts during construction. To address the potential construction noise:
 - Prior to the issuance of any demolition, grading or building permits with the Specific Plan area, specifications shall be prepared that identify requirements regarding attenuation of noise from construction vehicles and activities, including notification of surrounding owners and tenants of the proposed construction schedule and activities, posting of project hours and contractor contact information at all construction entrances, submittal of a material haul route plan to the City, staging of noisy equipment away from sensitive uses, and implementation of noise attenuation measures to the extent feasible.
- **Unearthing of subsurface cultural resources during construction.** Due to the history of the area, the potential exists for cultural resources, and specifically Tribal Cultural resources to be disturbed, during initial construction activities. As a result, future development associated with the Project that involved site excavation could have an impact on subsurface cultural artifacts within the Specific Plan Area. As the significance of any subsurface artifacts is currently unknown, this impact is considered potentially significant. To address the potential subsurface cultural resource impacts:
 - The project proponent of any proposed development within the Specific Plan area that involves excavation, or similar ground disturbing activity, shall retain a certified Native American Monitor, procured by the Fernandeño Tataviam Band of Mission Indians ("the Tribe"), for the duration of construction-related ground disturbance activities. The Monitor shall complete monitoring logs on a daily basis that document ground

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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disturbing activities, locations, soil, and any cultural materials identified. On-site Native American monitoring shall end when the project site grading and excavation activities are completed, or when the Tribe's Tribal Historic and Cultural Preservation Officer (THCPO) has indicated, in writing, that the project will no longer need to obtain Native American monitoring services.

- **Future development associated with the Project could increase vehicle traffic within the Specific Plan Area.** Traffic analysis of assumed future traffic identified intersections where the level of performance could be reduced to an unacceptable level. As such, the Project could have a significant impact due to increase vehicular traffic. To address potential traffic impacts, the City shall:
 - Create an eastbound right-turn lane on First Street. This improvement may require the removal of one parking space between the commercial driveways on the south side of First Street west of Maclay Avenue. Removing the one parking space would allow for a right turn of 150 feet in length. If additional right turn storage is required, then additional parking spaces on the south side of First Street may need to be removed. This improvement will also fit within the existing curbs, not requiring any street widening.
 - Implement a signal synchronization program, coordinating traffic signal systems within the Downtown District of the Specific Plan area, specifically along Maclay Avenue, Hubbard Street, Truman Street, and San Fernando Road.

If the aforementioned mitigation measures are implemented, potential impacts would be reduced to a less than significant level.

The Final Environmental Impact Report SCH No. 2015121088 was prepared, pursuant to California Environmental Quality Act (CEQA) and analyzes the potential impacts of the proposed specific plan. On November 7, 2017, the Planning and Preservation Commission, prior to taking action on the proposed specific plan and related amendments to the General Plan and Zoning Ordinance and appurtenant maps, certified that the Final EIR was prepared in accordance with the requirements of CEQA, and found that the identified environmental effects were either insignificant or can be adequately mitigated to acceptable insignificant levels.

Legal Notice.

Notice of Public Hearing was mailed to all designated property owners within a 500-foot radius of the proposed Specific Plan Area. Notices were also posted at City Hall and on the City's website, and noticed in a newspaper of general circulation (Attachment "G")

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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The Specific Plan Document And Relationship To General Plan And Zoning Ordinance.

The Specific Plan provides a framework in which future development and investments can be nurtured and expanded. To effectuate these changes, State law requires that the City's General Plan, Specific Plan and Zoning Ordinance and maps are internally consistent (i.e. no internal contradictions). Therefore, as part of the overall process, the Consultant has prepared the appurtenant amendments to the General Plan and Zoning Ordinance and related maps. These documents are also attached to this staff report for your consideration.

Public Comments.

San Fernando is a special community of caring residents who are attracted to the City's rich history and quaint "small town" atmosphere. At the same time, the residents realize that Metro is implementing its East San Fernando Valley Transit Corridor project, with property acquisition starting as early as 2019. The leading or most favored alternative by Metro and communities along the route expansion is Light Rail Transit within the existing railroad right-of-way. For San Fernando, the implementation of the Metro project will offer definite benefits and challenges. One of the objectives of the San Fernando Corridors Specific Plan SP-5 is to address these "benefits and challenges" as early as possible to ensure a smooth transition for the residents and businesses as the community enters the next decade. The development of the San Fernando Corridors Specific Plan SP-5 was shaped by the residents' and businesses' comments.

City staff and the Consultant received public comments concerning the proposed San Fernando Corridors Specific Plan SP-5 at various venues and forums (i.e., public workshops, phone calls, personal meetings, and written correspondence). Each of the written comments received at the August 28, 2017 and September 15, 2017 workshops are addressed in the EIR. By in large, the main areas of concern focused upon residential development, building intensity, and businesses and jobs.

- **Residential Development.** Most of the comments received concerning residential development were negative. The residents perceived that the construction of additional residential units will lead to further traffic congestion, circulation and parking issues as well as exacerbating the existing aged infrastructure system (i.e., water and sewer). In response to this sentiment, the Consultant altered and refined the respective areas of the Specific Plan so that all new residential construction will require approval of a Conditional Use Permit (CUP), which affords the community an additional level of discretionary review by the Planning and Preservation Commission. Furthermore, the Consultant is recommending that the eligible area for residential development be limited to only specific sites and at reduced densities and building heights. By preserving the ability to add new residential units to the market place, the Consultant is recommending a strategy that minimizes the potential impact of residential while at the same time, still address that segment of the

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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population expressing the need for newer housing, in terms of diversity, price points, and which promotes expansion of homeownership opportunities at all segments of the market.

- **Building Intensity.** Closely related to the first point, many residents raised the concern for the loss of San Fernando's rich culture, history, and quaintness; values that many hold very dear and a strong desire to maintain. Towards that goal, the Consultant crafted design guidelines that will facilitate the harmony between the old and the new. Contextual relationships between buildings and within urban environments are proposed. As a result, the building densities, heights, and massing are reduced and architectural standards and design guidelines emphasized.
- **Businesses and Jobs.** Underlying many of the public forums was the fear that the proposed specific plan will hinder job growth and business expansion. It was explained that one of the purposes of the proposed specific plan is to foster economic growth, business formation, and economic development. In fact, the specific plan expands the list of permitted uses across all districts of the specific plan, streamlines the approval process for many types of businesses, and converts a number of CUP uses into principally permitted uses. In many respects, the specific plan contains policies and strategies that promote economic prosperity, job creation, and enhanced growth of existing businesses as well as the attraction of new businesses.

Staff received two letters from concerned residents (Attachment "F"). The first letter, dated October 18, 2017 and received on October 25, 2017, was made a part of the November 7, 2017 Planning and Preservation Commission staff report. The second correspondence is an email dated November 6, 2017 was read verbatim into the record of the November 7, 2017 Planning and Preservation Commission meeting. The comments expressed by both letters are considered and reflected in the amended specific plan provisions. The focus of the comments dealt with their perception of potential issues related to residential development, location, intensity and height of development and traffic congestion and parking issues. The development of the specific plan has resulted in reduced development intensity (building density and height, and FAR) as well as a reduction in the number of properties where residential projects can be built. Moreover, the specific plan requires all residential projects to be processed through a discretionary review process that requires a notification of all property owners within 500 feet of a proposed residential project and a public hearing before the Planning and Preservation Commission. The reduction in development intensity is similarly applied to commercial and industrial projects in varying degrees. The references to lowering building heights on Maclay Avenue between 4th and 8th Streets were not a part of the original scope of work and are not addressed by the specific plan. However, the comments for the need of additional public open space, parks and/or plazas is supported by the specific plan and is identified as an action item in the specific plan. In summation, the development of the specific plan has been influenced by

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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the public comments that were received in the numerous public forums, workshops, and meetings with the community stakeholders, residents, and businesses.

The San Fernando Corridors Specific Plan SP-5 is a "living" document that is meant to be updated pursuant to changing circumstances. The original San Fernando Corridors Specific Plan was adopted by Ordinance No. 1562 in January, 2015 as SP-4. Since then, the voters of Los Angeles County have approved Measure M, which provides the necessary funding for one of the planned improvements identified in the East San Fernando Valley Transportation Plan that traverses San Fernando's downtown area towards the Sylmar/San Fernando Metrolink Station. The current proposed San Fernando Corridors Specific Plan (SP-5) establishes policies and strategies that recognizes Metro's public transportation project that is projected to start in 2019 and to update the current SP-4 policies and strategies. As such, SP-5 will replace SP-4, in its entirety.

The proposed San Fernando Corridors Specific Plan (SP-5) establishes planning principles, land use policies, development standards, and design guidelines for public improvements and private development within the specific plan area. Some of the more notable changes from the SP-4 Plan are identified below, and a more detailed comparison of the changes are presented in in a matrix (Attachment "G").

- Making all residential projects allowable pursuant to receiving a Conditional Use Permit;
- Reducing the density, heights and number of floors and overall intensity for residential projects;
- Limiting the eligible areas for residential projects within the Downtown District;
- Allowing commercial and retail uses in expanded locational areas; and
- Streamlining the approval process for retail, restaurants, and commercial uses.

At the Planning and Preservation Commission meeting on November 7, 2017, and following consideration of the entire record and Public Hearing testimony and extensive deliberation, the Commissioners directed staff to include the following amendments to the San Fernando Corridors Specific Plan SP-5:

- Add Market Hall/Food Hall use: Added Market Hall/Food Hall as a permitted by-right use to Table 4.1 (Land Use and Permit Requirements) under the "Retail, Service, Entertainment, Lodging and Office Uses" (page 45) within the Downtown, Mixed-Use Corridor, and Auto Commercial Districts.
- Permit Bowling Alley use in Downtown District: Added Bowling Alley use as a permitted by-right use to Table 4.1 (Land Use and Permit Requirements) in the Downtown District and

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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also in the Auto Commercial District (page 45). Previously, bowling alleys were only allowed in the Mixed-Use Corridor District.

- Reverse Angled Parking should be allowed: Added text to San Fernando Road Street Standards (page 165) and First Street East of Hubbard Avenue (page 168) specifying that reverse angled parking is allowed, subject to review by the Public Works Director.
- Allow for Business Incubators: added Business Incubators as a permitted use in all districts with the exception of the Maclay and General Neighborhood Districts.

BUDGET IMPACT:

In 2013, the City of San Fernando received a \$282,392 grant award from Metro to complete the San Fernando Corridors Specific Plan SP-5. Following years of community engagement, the project is nearing completion. In recognition of the community's participation, the City allocated approximately an additional \$30,000, from budgeted funds and cost savings from unfilled staff positions, to fully assess and understand the desires of the residents and businesses in the hope of crafting a more meaningful plan for the community.

CONCLUSION:

The proposed San Fernando Corridors Specific Plan (SP-5) serves as a guide to implement the goals, policies, and objectives of the City's General Plan and promotes sustainable development, job creation, homeownership, economic development, livability and walkability within the downtown area and surrounding neighborhoods towards the Sylmar/San Fernando Metrolink Station. New improvements in the Specific Plan Area will be of high architectural quality and appropriate scale that will contribute a "sense of place" and facilitate the "branding" of this district.

Staff recommends that prior to approving the amendments to the General Plan, Zoning Ordinance and related maps, and proposed San Fernando Corridors Specific Plan (SP-5), the San Fernando City Council hereby finds that:

- The Final EIR SCH No. 2015121088 (EIR) was completed in compliance with California Environmental Quality Act, and

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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- The Final EIR was presented and approved at a duly noticed Public Hearing of the Planning and Preservation Commission at its November 7, 2017 meeting for review and consideration at its November 7, 2017 Public Hearing.
- The Final EIR was presented to the City Council for review and consideration, and the City Council has considered the information contained in the Final EIR prior to considering approval of the proposed amendments to the General Plan, Zoning Ordinance, related maps, and proposed San Fernando Corridors Specific Plan (SP-5).

Staff, therefore, recommends that the City Council:

1. Adopt Resolution No. 7826, which in sequential order: i) certifies the Final Environmental Impact Report SCH No. 2015121088, pursuant to the California Environmental Quality Act; and, ii) adopts the amendments to the General Plan text and map;
2. Approve aforementioned Resolution No. 7826, contingent upon the final adoption of the San Fernando Corridors Specific Plan SP-5 and amendments to the Zoning Ordinance text and map; and
3. Introduce for first reading, Ordinance No. 1621, recommending adoption of the San Fernando Corridors Specific Plan SP-5 and adoption of the proposed amendments to the City's Zoning Ordinance text and map; and
4. Schedule a Public Hearing date, at the regular City Council meeting of December 4, 2017, to conduct the second reading of Ordinance No. 1621.

ATTACHMENTS:

- A. Resolution No. 7826, Including the following Exhibits:
 - Exhibit A. Specific Plan Area
 - Exhibit B. Land Use Districts
 - Exhibit C. Environmental Impact Report SCH No. 2015121088
 - Exhibit D. San Fernando Corridors Specific Plan SP-5
 - Exhibit E. Land Use Element, Housing Element, and Circulation Element of the City of San Fernando General Plan
- B. Ordinance No. 1621
- C. Planning and Preservation Commission Resolution No. 2017-008
- D. Planning and Preservation Commission November 7, 2017 Staff Report and Draft Minutes (11/7/17)

Consideration to Adopt Resolutions Certifying Environmental Impact Report SCH No. 2015121088 and Approving the Amendments to the City's General Plan Text and Map, and Consideration to Adopt an Ordinance Approving the San Fernando Corridors Specific Plan SP-5 and Related Amendments to the City's Zoning Ordinance Text and Map

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- E. Public Notice of City Council Public Hearing
- F. Two Letters from Residents
- G. Comparison of Proposed Changes between SP-4 and SP-5

ATTACHMENT "A"**RESOLUTION NO. 7826**

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SAN FERNANDO AMENDING THE TEXT OF THE LAND USE ELEMENT, HOUSING ELEMENT, AND CIRCULATION ELEMENT OF THE SAN FERNANDO GENERAL PLAN TO ADD THE SAN FERNANDO CORRIDORS SPECIFIC PLAN LAND USE DESIGNATION AND A DESCRIPTION OF THE LAND USE DESIGNATIONS AS PROPOSED WITHIN THE DRAFT SAN FERNANDO CORRIDORS SPECIFIC PLAN (SP-5); AMENDING THE LAND USE ELEMENT MAP OF THE SAN FERNANDO GENERAL PLAN BY CHANGING THE LAND USE DESIGNATION OF THE PROPERTIES LOCATED WITHIN THE SAN FERNANDO CORRIDORS SPECIFIC PLAN AREA TO THE DRAFT SAN FERNANDO CORRIDORS SPECIFIC PLAN LAND USE DESIGNATIONS (SP-5); AND CERTIFYING THE ENVIRONMENTAL IMPACT REPORT SCH NO. 2015121088

WHEREAS, on February 28, 2013, Metro awarded a \$282,392 planning grant to the City of San Fernando for the completion of a comprehensive update to the existing SP-4 San Fernando Corridors Specific Plan, to make the necessary amendments to the General Plan text, Zoning Ordinance text, and related maps and to prepare an Environmental Impact Report (EIR) pursuant to CEQA. The City agreed to contribute a match of \$13,306 (\$6,306 in-kind, such as staff-time and \$7,000 funds);

WHEREAS, on May 19, 2014, the firm of Sargent Town Planning (Consultant) was selected to complete the project from a general solicitation to qualified consultants;

WHEREAS, throughout this process, multiple public meetings, and workshops were conducted with residents, merchants, and local stakeholders. The City conducted several individual and small group meetings with local stakeholders in September 2014 and October 2014 and again in September 2015;

WHEREAS, the Development Advisory Committee (DAC) conducted four public meetings on September 30, 2014, November 12, 2014, January 14, 2015, and June 17, 2015;

WHEREAS, the City conducted one environmental scoping meeting on January 7, 2016 and four community workshops on November 19, 2014, January 21, 2015, August 28, 2017, and September 15, 2017;

WHEREAS, the Planning and Preservation Commission also conducted discussions at two of its regularly scheduled meetings on August 4, 2015, October 6, 2015;

WHEREAS, Metro's deadline to complete the project was extended from the original date of June 30, 2016 to March 31, 2018;

WHEREAS, the City of San Fernando (c/o Community Development Department), hereinafter referred to as "Applicant," has submitted a proposal for approval of the San Fernando Corridors Specific Plan SP-5 in conjunction with amendments to the text of the Land Use Element, Housing Element, Circulation Element, and the Land Use Element Map of the San Fernando General Plan, and amendments of the City of San Fernando Zoning Ordinance and Zoning Map (hereinafter referred to as the "Project"), in order to implement development strategies for the revitalization of approximately 150 acres of land covered by the San Fernando Corridors Specific Plan (SP-5); whereas the project area encompasses the full lengths of Truman Street, San Fernando Road, and Celis Street within the City of San Fernando, from the eastern boundary with Pacoima to the western boundary with Sylmar. The project boundaries include the entire public rights-of-way as well as parcels located to the north and south of these roads. The San Fernando Corridors Specific Plan (SP-5) planning area also includes the First Street public right-of-way and properties located along the south side of First Street between Hubbard Avenue and Macneil Street; the properties bounded by First Street and Second Street between Hubbard Avenue and Macneil Street; the entire Maclay Avenue public right-of-way and all fronting properties between San Fernando Road to Eighth Street at the City's northern boundary with Sylmar; and the properties located along the north side of Pico Street between Kalisher Street and Chatsworth Drive (hereinafter referred to as the "Specific Plan Area"), as depicted in Exhibit "A";

WHEREAS, the San Fernando Corridors Specific Plan SP-5 ("Specific Plan") contains the goals, objectives, and policies to guide the community's vision for the revitalization of the properties located within the Specific Plan Area. The San Fernando Corridors Specific Plan SP-5 document includes a detailed statement of the community's vision for the future of the corridor areas as well as the revitalization strategy and urban design principles to be used in achieving that vision. The San Fernando Corridors Specific Plan SP-5 provides specific land use regulations, development standards and design guidelines that would apply to new development in the corridor areas, as well as a program of public improvements for the streets and sidewalks;

WHEREAS, the Project includes a request to amend the text of the Land Use Element, Housing Element, and Circulation Element of the San Fernando General Plan to include a description of the San Fernando Corridors Specific Plan SP-5 land use designation, including the Maclay District, Downtown District, Mixed-Use Corridor District, Auto Commercial District, Workplace Flex District, and General Neighborhood District, and the Truman/San Fernando District, which are located within the Specific Plan Area; and

WHEREAS, the Project includes a request to amend the Land Use Element Map to include the San Fernando Corridors Specific Plan SP-5 land use designation, as shown in Exhibit "B" to this Resolution.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY RESOLVE, FIND, DETERMINE AND ORDER AS FOLLOWS:

SECTION 1. The facts set forth in the recitals above are true and correct and incorporated herein by reference.

SECTION 2. On November 7, 2017, the Planning Commission held a duly noticed public hearing and adopted Resolution No. 2017-18, finding the Project to be in conformance with the General Plan and recommending that the City Council approve the San Fernando Corridors Specific Plan SP-5 in conjunction with the corresponding General Plan Text and Map Amendments, and the Zoning Ordinance and Zoning Map Amendment.

SECTION 3. A notice of Public Hearing for the adoption of the San Fernando Corridors Specific Plan SP-5, and the related General Plan Text and General Plan Map Amendments, and Municipal Code and Zoning Map Amendments, was given pursuant to San Fernando Municipal Code Section 78-69 and Section 106-72, and in compliance with Government Code Sections 65090 and 65091. Therefore, the City placed a public notice in the Los Angeles Daily News, a newspaper of general circulation, at least 10 days prior to the City Council meeting. In addition, the City mailed the notice of the hearing to affected owners of property within 500 feet of the Specific Plan Area.

SECTION 4. The City Council makes the following findings and determinations:

Pursuant to the California Environmental Quality Act ("CEQA"), the City as the Lead Agency overseeing the environmental review for the proposed specific plan has prepared an Environmental Impact Report (EIR) for the proposed project. The Project includes applications for Specific Plan approval, General Plan Text and Map Amendments, and Zoning Ordinance and Zoning Map Amendments. All these actions together constitute the Project evaluated pursuant to CEQA.

Based upon the findings of the Environmental Impact Report SCH No. 2015121088 (Exhibit "C" to this report) prepared for the Project, the City determined that the Project may have potential significant effects on the environment attributed to growth (i.e., noise, cultural and transportation impacts) associated with the proposed redevelopment of the Specific Plan Area. The Environmental Impact Report SCH No. 2015121088, pursuant to the California Environmental Quality Act (CEQA) provisions, includes mitigation measures that will provide for the reduction of identified environmental impacts to "less than significant" levels.

The City Council has reviewed the Environmental Impact Report SCH No. 2015121088 and all comments received regarding the environmental assessment and, based on the whole record before it, finds: (i) that the Environmental Impact Report SCH No. 2015121088 was prepared in compliance with CEQA; and (ii) that, based on the imposition of mitigation measures, there is no substantial evidence that the Project will have a significant effect on the environment. Based on these findings, the City Council hereby adopts and certifies the Environmental Impact Report SCH No. 2015121088 for this Project.

The City Council designates the custodian of records for the Environmental Impact Report SCH No. 2015121088 and all other materials which constitute the record of proceedings upon which the City Council's decision is based to be the Community Development Department of the City of San Fernando. Those documents are available for public review in the Community Development Department of the City of San Fernando located at 117 Macneil Street, San

Fernando, California 91340, telephone number: (818) 898-1227.

SECTION 5. Based on the evidence and all other applicable information presented, the City Council finds that the General Plan Text and Map Amendments are appropriate for the following reasons:

- A. Replacing the San Fernando Corridors Specific Plan SP-4 with the addition of the San Fernando Corridors Specific Plan SP-5 (Exhibit “D”) land use designations (Maclay, Downtown, Mixed-Use Corridor, Auto Commercial, Workplace Flex, and General Neighborhood Districts) and map are consistent with the General Plan Land Use Element's Goals and Objectives and will not adversely affect the properties adjacent to the Specific Plan Area. The San Fernando Corridors Specific Plan SP-5 allows the clustering of commercial, higher density residential, and mixed-use development in a manner that retains the small town character of San Fernando, attracts new commercial activities to the City, and provides for new public and private investment within the commercial corridors.

SECTION 6. The City Council hereby amends the Land Use Element, Housing Element, and Circulation Element of the City of San Fernando General Plan, as shown on Exhibit “E” to this Resolution, which is attached hereto and hereby incorporated by this reference.

SECTION 7. The City Council hereby approves the amendment to the Land Use Element Map of the City of San Fernando General Plan to change the land use designation of the Specific Plan Area, which is attached hereto and as shown on Exhibit "**FB**" to this Resolution.

PASSED, APPROVED, AND ADOPTED this 20th day of November, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) ss
CITY OF SAN FERNANDO)

I HEREBY CERTIFY that the foregoing Resolution was approved and adopted at a regular meeting of the City Council held on the 20th day of November, 2017, by the following vote to wit:

AYES:

NOES:

ABSENT:

Elena G. Chávez, City Clerk

Exhibit “A” Specific Plan Area Map (Michelle, this is the outline of the SP-5 area- not with colored land uses)

EXHIBIT “B” San Fernando Corridors Specific Plan SP-5 (See _____, _____)

Exhibit “C” General Plan Text (Land Use, Housing and Circulation Elements) and Land Use Map Amendment (With the colored land uses)

SAN FERNANDO CORRIDORS SPECIFIC PLAN (SP-5)
SPECIFIC PLAN AREA

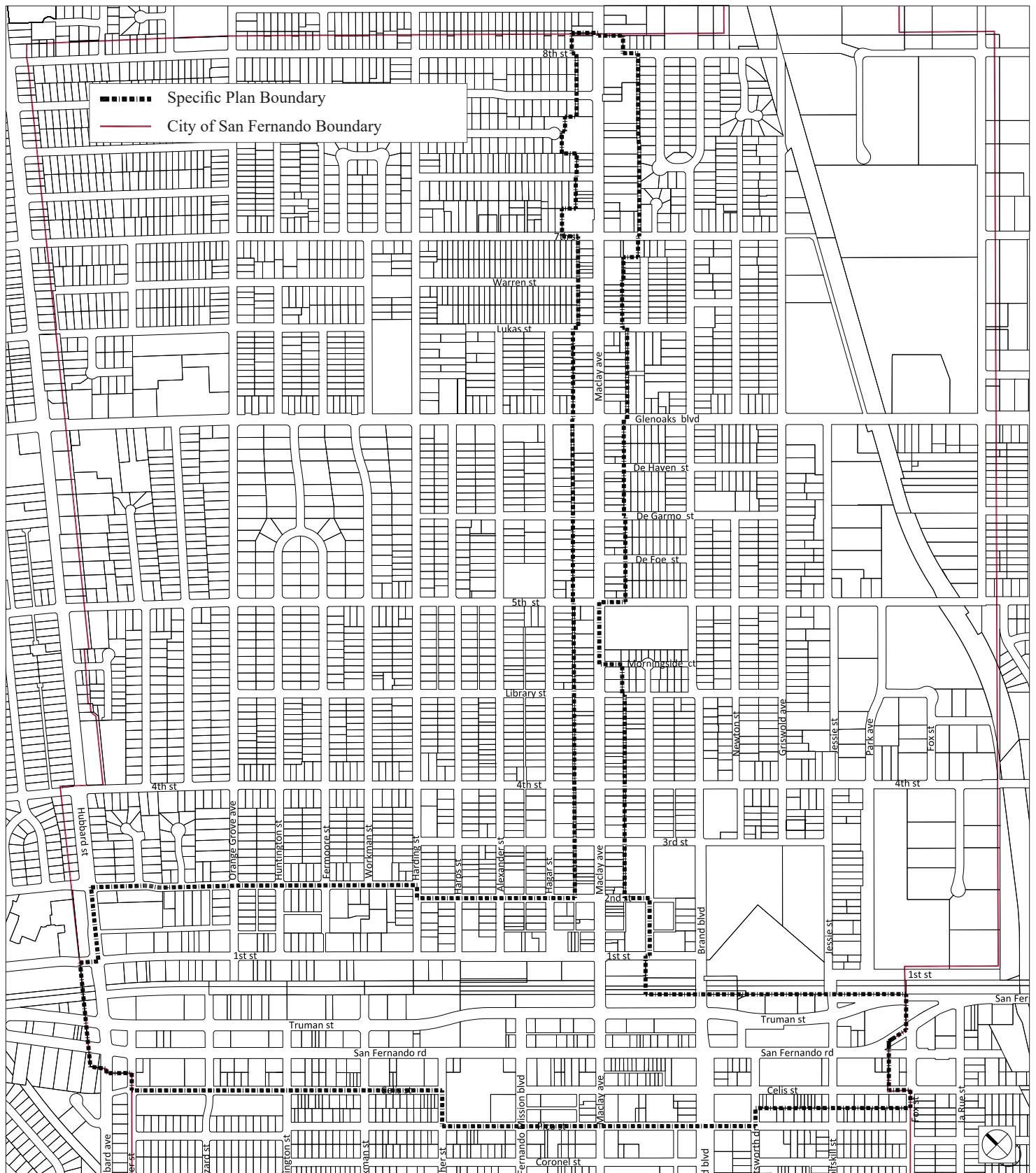


EXHIBIT "B"

SAN FERNANDO CORRIDORS SPECIFIC PLAN (SP-5) **LAND USE DISTRICTS**

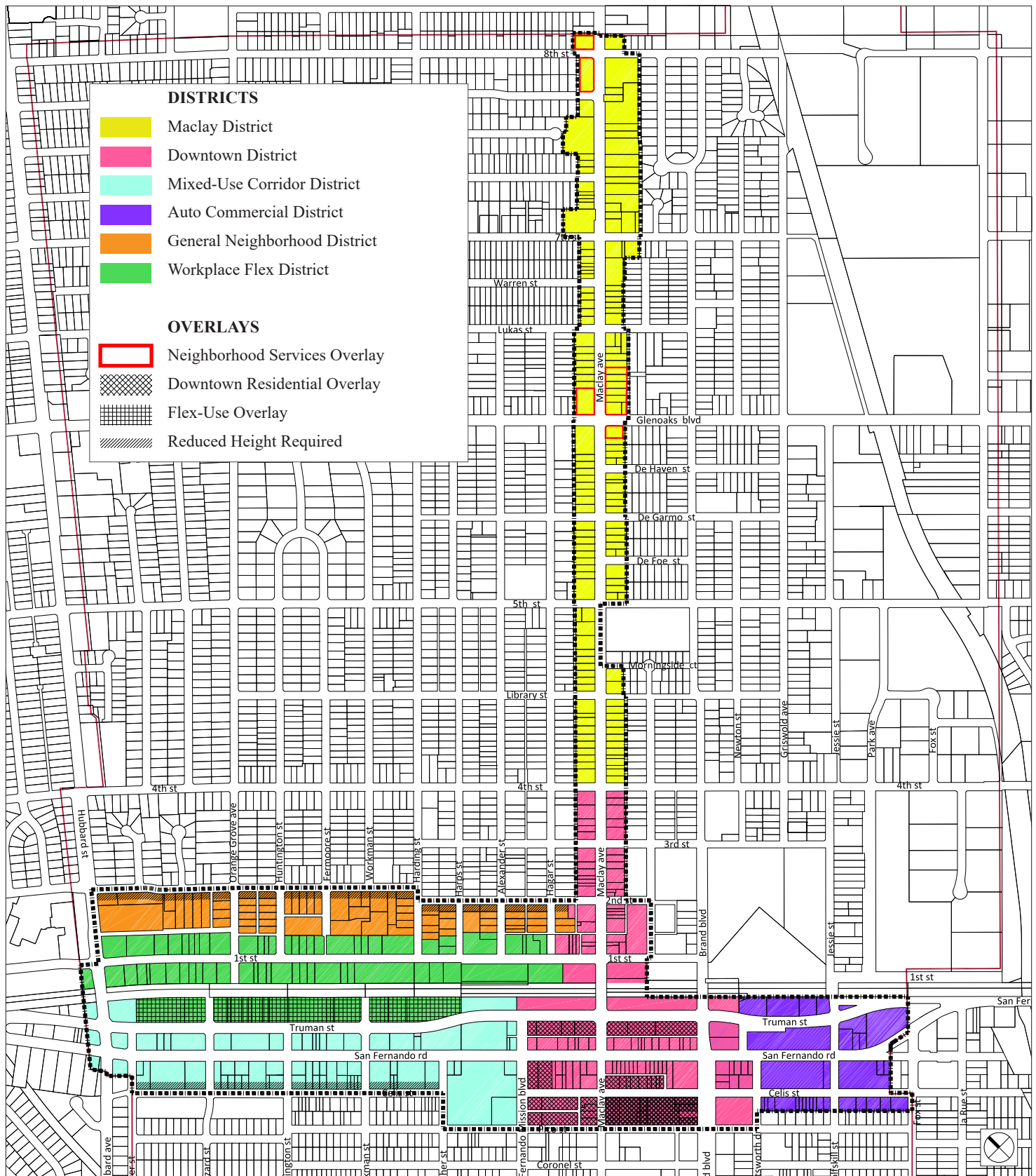


EXHIBIT "C"

FINAL**Prepared For:**

CITY OF SAN FERNANDO
Community Development Department
117 Macneil Street
San Fernando, California 91340

**San Fernando Corridors
Specific Plan Amendment**
ENVIRONMENTAL IMPACT REPORT
SCH No. 2015121088

NOTE:

**FULL COPY OF THIS EXHIBIT
(1,896 PAGES) IS PROVIDED
UNDER SEPARATE COVER**



910 Hampshire Road, Suite V
Westlake Village, CA 91361
(805) 367-5720 FAX (805) 367-5733

OCTOBER 2017

SAN FERNANDO CORRIDORS SPECIFIC PLAN (SP-5)



PUBLIC HEARING DRAFT
NOVEMBER 15, 2017



PROJECT PARTICIPANTS

CITY OF SAN FERNANDO

City Council

Mayor Sylvia Ballin
 Vice Mayor Antonio Lopez
 Councilmember Mayor Joel Fajardo
 Councilmember Robert C. Gonzalez
 Councilmember Jaime Soto

Planning and Preservation Commission

Chair Theale E. Haupt
 Vice-Chair Alvin Durham, Jr.
 Commissioner Kevin Beaulieu
 Commissioner Yvonne G. Mejia
 Commissioner David Bernal

Staff

Alexander Meyerhoff, City Manager
 _____ Public Works Director
 Jack Wong, Interim Community Development Director
 Michelle De Santiago, Community Development Department Secretary
 Francisco Villalva, Community Development, Building and Safety Supervisor
 Rick Olivarez, City Attorney
 Lloyd Pilchen, City Attorney
 Nick Kimball, Finance Director
 Manuel Fabian, Public Works, Civil Engineering Assistant II
 Patsy Orozco, Public Works, Civil Engineering Assistant II
 Tony Salazar, Public Works Superintendent
 Anthony Vario, Chief of Police

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 Troy Reinhalter, Intermediate Planner/Designer

MR+E – Economics Consulting

David Bergman, Principal

Nelson\Nygaard - Transportation Planning

Patrick Siegman, Principal

Meridian Consultants - Environmental Impact Report

Tony Locacciato, Partner
 Ned Baldwin, Project Manager

Gibson Transportation Consulting - Traffic Transportation

Pat Gibson, President
 Richard Gibson, Associate

The San Fernando

Corridors Specific Plan

Adopted by Ordinance #1562

January 2005

Prepared for

The City of San Fernando

Prepared by

**Freedman Tung & Bottomley Conley
Consulting Group**

Amended _____ by

Sargent Town Planning

in association with

Raimi+Associates

Nelson\Nygaard

MR+E

Meridian Consultants

Gibson Transportation Consulting

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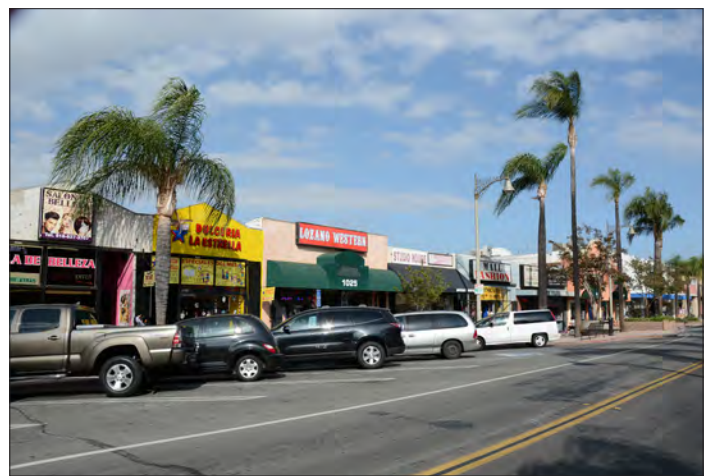
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CHAPTER ONE: ORIENTATION





Community Workshop comments.



The Community Workshop process.



Part of the history of San Fernando.

The City of San Fernando is moving forward with a community-based vision for economic development and revitalization of the Maclay Avenue, Truman Street, San Fernando Road, and First Street corridors. This document, the San Fernando Corridors Specific Plan, is the City's tool to help guide and realize this vision. The original San Fernando Corridors Specific Plan was adopted in January, 2015 as SP-4. Since then, the voters of Los Angeles County approved Measure M, which provides the necessary funding for the planned transit improvements of the East San Fernando Valley Transit Corridor project, which traverse San Fernando's downtown area, terminating at the Sylmar/San Fernando Metrolink Station. The current proposed San Fernando Corridors Specific Plan (SP-5) will establish strategies that recognize Metro's public transportation project, projected to start in 2019, and to update the current SP-4 policies and strategies. As such, this Specific Plan, SP-5, will replace SP-4, in its entirety.

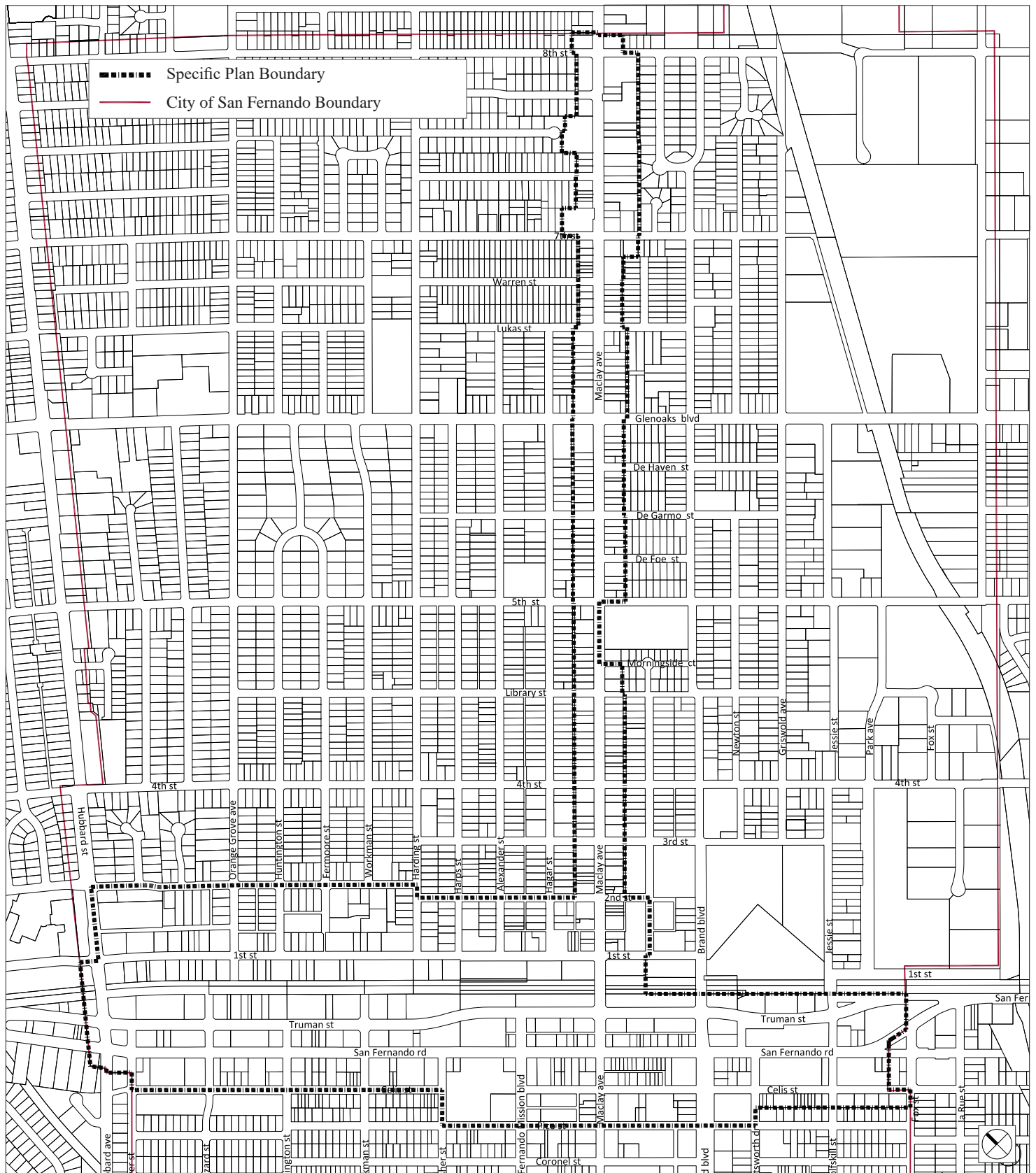
Funding for this specific plan amendment was provided by Metro in order to encourage cities along major transit corridors such as the Metrolink railway to make regulatory changes necessary to facilitate infill and transit oriented development projects that are consistent with regional transportation plans.

SPECIFIC PLAN AREA DEFINITION

The San Fernando Corridors Specific Plan encompasses the full lengths of Truman Street, San Fernando Road, and Celis Street within the city, from the eastern boundary with Pacoima to the western boundary with Sylmar (see Figure 1.1). The project boundaries include the entire public rights-of-way as well as parcels located to the north and south of these roads. It also includes the First Street public right-of-way and properties located along the south side of First Street between Hubbard Avenue and Macneil Street; the properties bounded by First Street and Second Street between Hubbard Avenue and Macneil Street; the entire Maclay Avenue public right-of-way and all fronting properties between San Fernando Road to Eighth Street at the City's northern boundary with Sylmar; and the properties located along the north side of Pico Street between Kalisher Street and Chatsworth Drive.

PLAN PURPOSE

The purpose of the San Fernando Corridors Specific Plan is to transform Truman Street, San Fernando Road, Maclay Avenue, and First Street into attractive, livable, and economically vibrant districts. These corridors are provide a framework for movement and activity in the community and will accommodate Metro's East San Fernando Valley Transit Corridor enhanced transit improvements – either Bus Rapid Transit (BRT) along

FIGURE 1.1 - SPECIFIC PLAN PROJECT LIMITS

Truman Street, Modern Streetcar (Tram) along San Fernando Road, or Light Rail within the existing railroad right-of-way – and provide pedestrian, bicycle, and vehicular access to the San Fernando/Sylmar Metrolink Station and the transit stops associated with Metro’s proposed transit initiatives. They also provide opportunities for investment. The corridors are where much of the community life is “on display” for residents and visitors alike. Because it is important to properly shape growth and change on the corridors, the specific plan combines a policy framework with design standards and guidelines, and includes concepts for capital improvements to encourage and focus activity and investment along them.

PLAN INTENT

Maclay Avenue, Truman Street and San Fernando Road are the primary arteries that transverse the City of San Fernando and connect it to its surroundings. As gateways to the city, these streets convey an initial impression of San Fernando to entering visitors. However, these corridors, have suffered for many decades from a lack of new private investment and consequently present a less than attractive image of the community to visitors, residents, and prospective investors. The City of San Fernando and its residents would like to reverse this trend of disinvestment, and transform these highly visible and undervalued portions of the city into vibrant and attractive places.

A key part of this strategy is leveraging San Fernando’s many assets, including its convenient location near three major highways (Interstate 5, Interstate 210, and State Highway 118); its convenient access to transit; its many significant historic resources, including a several sites listed or potentially eligible for listing on the National Register of Historic Places; its picturesque, pre-World War II residential neighborhoods; its distinctive “main-street” shopping district, the San Fernando Mall; its reviving Maclay Street, an outcome of the policies of the previous San Fernando Corridors Specific Plan (SP-4); the job-creating opportunities offered by the light industrial businesses along First Street; and most important of all, its identity as a unique small city that stands out from surrounding areas in the great metropolis that is Los Angeles.

In addition, supporting Metro’s new East San Fernando Valley Transit Corridor, as well as introducing residential and office uses within walking and biking distance of the Metrolink Station and Metro’s proposed BRT, Tram, or Light Rail stops, will help transform San Fernando into an active, vibrant, pedestrian-friendly, mixed-use place.



A Tram, also known as a Modern Streetcar, could potentially be introduced along San Fernando Road.



A Bus Rapid Transit (BRT) line could potentially be introduced along Truman Street.



A Lightrail could potentially be within the railroad right-of-way with stops at Maclay Avenue and at the Metrolink Station.

Finally, as discussed in the previous San Fernando Corridors Specific Plan (SP-4), one of the barriers to investment and revitalization along the corridors that was identified was the large amount of land devoted to retail and commercial uses. One approach to turning this trend around, as endorsed by the Urban Land Institute (ULI, "Ten Principles for Reinventing America's Suburban Strips, 2001), is to reduce the amount of retail-zoned land along the corridor or to permit additional uses within those zones. By reducing the amount of land currently over-zoned for retail and commercial uses, or by increasing the number of allowed uses, this approach frees-up marginal and underutilized land for strongly sought-after forms of new investment such as housing, office, research and development (R&D), and incubator space.

In conjunction with reducing the amount of retail and commercially-zoned land, the Urban Land Institute endorses a "clustering" of retail uses at major arterial and freeway intersections and in existing downtowns. Communities are rediscovering the function of walkable "main streets" as a component of revitalized downtowns and town centers. Cities are recognizing that a reduction in retail-zoned land along commercial corridors in combination with the clustering of retail uses focuses market attention on areas that may be efficiently amortized and serviced. Correspondingly, the re-zoned corridors and their arterial streets can be re-tooled to provide opportunity for pent-up housing demand, and help reverse the trend of marginalization and disinvestment plaguing commercial strip corridors. These trends show that communities have options to revitalize arterial street corridors from past eras that no longer serve the highest expectation of the community.

THE VISION: A City of Districts

The Truman, San Fernando, Maclay, and First Street corridors will provide the structure upon which the city can be organized as a series of clearly identifiable districts. Rather than continuing to embody the placeless character of commercial "strip" corridors, the corridors will embody the street type, development type, and aesthetic qualities envisioned for the city district in which they are situated. New investment in the corridors will lead to city-wide revitalization. Where the corridors meet the city's boundaries, distinctive gateways will introduce the qualities that set San Fernando apart from the adjacent communities. The corridors will be places for new investment in the form of housing, office, and commercial development.

In addition, either Truman Street or San Fernando Road could potentially accommodate Metro's new transit initiatives that include new transit stations/stops and Bus Rapid Transit (BRT) on Truman Street or Modern Streetcar (Tram) on San Fernando Road. Metro is also



The decline of the commercial strip.



The Truman Road corridor.



The distinctive gateway Maclay Avenue and Eight Street marks the northern entry into the City of San Fernando.



Maclay Avenue between First and Fourth Streets.

considering introducing Light Rail Transit (LRT) within the existing rail right-of-way, in lieu of the BRT or Tram alternatives. New streetscape improvements and gateway features will create the appropriate setting for new public and private investment, generating developer interest. The corridors will themselves become distinctive districts within the city. These distinct districts, as shown in Figure 1.2, are: the Downtown District, the Mixed-Use Corridor District, the Auto Commercial District, the Workplace Flex District, and the General Neighborhood District.

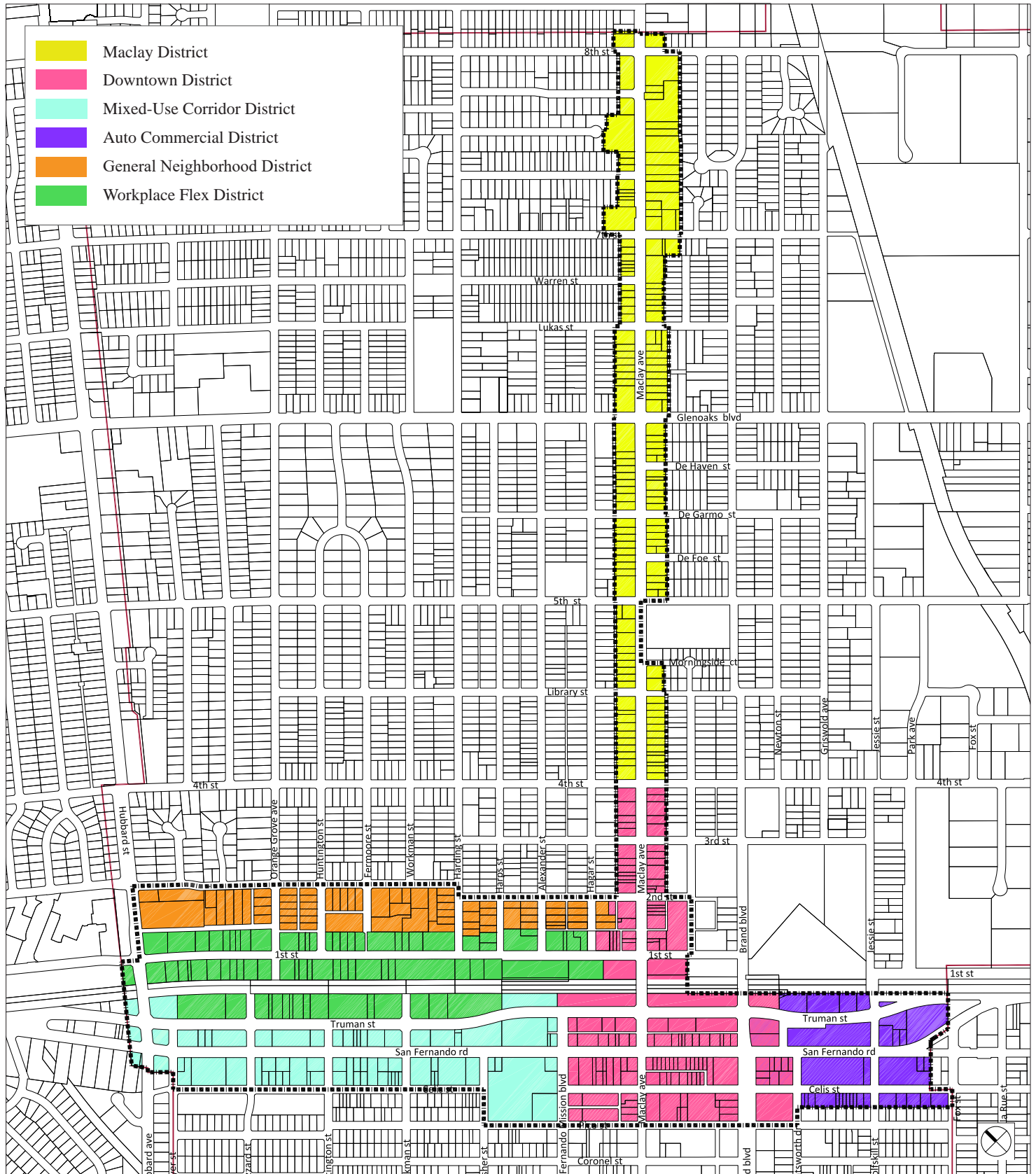
The Downtown District

The Downtown District is the city's functional center and contains most of the city's primary destinations – the shopping district along Maclay Avenue, the Civic Center, and the San Fernando Mall. With proposed improvements, this revitalized downtown will reflect its role as the most public place in the city and stand out as a highly recognizable and attractive community focus. Along Maclay Avenue between First Street and Fourth Street, a revitalized historic retail district serves as the vibrant and attractive civic heart of the downtown. The transformation of this area has occurred through new streetscape amenities and new investment in the form of retail shops, restaurants and cafes offering residents a charming area in which to spend their lunch hour, a place to gather after work, or to enjoy a meal with friends and family as the sun sets and the air cools in the evening hours. Continuing south on Maclay, the intersection of Maclay Avenue and Truman Street will tie the historic retail district with the vibrant destination of the San Fernando Mall, and anchor this vital intersection as a destination around which the downtown is structured. New investment at the intersection of Maclay Avenue and Truman Street will announce to visitors the center of the city. Continuing down Maclay Avenue and into the San Fernando Mall, development will embrace the street with ground-floor activity-generating uses including retail shops, restaurants, and services.

Mixed-Use Corridor District

To the west of the Downtown, the disparate development fronting and adjacent to Truman Street and San Fernando Road will be transformed into a cohesive, walkable urban fabric that connects Downtown to the Metrolink Station. New development will provide opportunities for new retail, offices, and residences within close proximity of the San Fernando Mall, the Sylmar/San Fernando Metrolink Station, and enhanced access to major public transit routes, including Metro's proposed BRT, Tram, or Light Rail initiatives and associated transit stations/stops. Well-designed buildings will provide a comfortable transition between existing residential development along and to the south of Celis Street, and the mixed-use areas to the north.

FIGURE 1.2 - SPECIFIC PLAN DISTRICTS



Auto Commercial District

The area to the east of the Downtown is devoted to the expansion of auto dealerships, increasing the city's position as a center for new car sales. This area will also allow office, retail, and service uses.

Streetscape improvements along Truman Street within the above three districts will support its role as a primary east-west thoroughfare that traverses the City, reshaping this corridor as a grand boulevard. West of San Fernando Mission Boulevard, large shade trees will provide an attractive environment for pedestrians, transit users, and automobiles alike. Streetscape improvements along San Fernando Road will slow traffic, provide convenient parking for shoppers, and give shade to shoppers and residents alike. At the district's boundary with Sylmar, a new gateway feature will announce one's arrival into San Fernando.

The Maclay District

Extending northward from the Downtown District, the Maclay District will enable new corridor-oriented home sites for the city's residents. Here, residential neighborhoods will reclaim their place as the rightful occupants of the city's primary north / south collector. Complementing new residential development along a corridor where Morningside Elementary School and Mission City Church set a high standard of neighborhood character, new shops and services will grace the corridor along with new residential buildings that are attractive and designed to support the Maclay District's neighborhood identity.

Within the Maclay District, locally-serving clusters of retail and services will provide residents with many of the conveniences needed to support their daily lives within a walkable and pedestrian-friendly environment. These retail nodes serve as local destination points for community members to meet and to gather. Residents will be able to walk to visit with friends and neighbors, or will meet one another at a café, bookstore, or other locally-serving establishment.

Throughout the district, new street trees will provide shade to the pedestrian environment while serving to buffer the sidewalks from traffic and parking lanes. New residential and residentially-compatible commercial development will be set back from the street with well-landscaped frontages providing a safe and comfortable atmosphere for strolling. At the northern end of the Maclay District, where Maclay Avenue intersects Eighth Street at the city's boundary, a new city gateway monument welcomes visitors and residents to San Fernando.

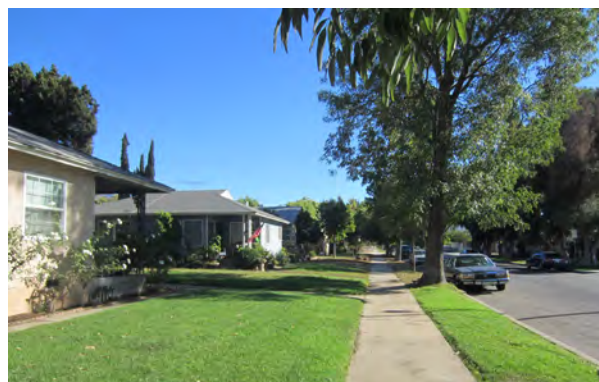
Workforce Flex District



An example of historic San Fernando architecture.



San Fernando is home to a variety of architectural styles.



Residential street between First Street and Second Street. Buildings are setback behind front yards.



A Mixed-Use Sub-District is envisioned along San Fernando Road.



New mixed uses are envisioned along San Fernando Road and Truman Street. This building is designed in the Monterey Style.



The Lopez Adobe is designed in the Monterey Style with Queen Anne Style balconies.

First Street and the north side of Truman Street between Meyer Street and Kalisher Street, commercial sales, office development, light industrial complexes, and warehouse and distribution development will fulfill the community's desire for a cohesive district to attract future investment. Businesses, and services will infill the district's undeveloped areas. Well-designed buildings will expand the power of the city's workplace incorporating new office, commercial and light-industrial development into the mix. Existing light industrial uses will continue to do business and new live-work uses will be allowed subject to a conditional use permit.

Street improvements, including new shade trees, wider sidewalks enabled by building setbacks, and a bicycle sharrow (travel lane shared by both cars and bikes), improve the physical experience of the street and strengthen the connection between the Sylmar/San Fernando Metrolink Station, the commercial uses along Maclay Avenue, and the civic uses to the east.

General Neighborhood District

The General Neighborhood District accommodates multi-family housing near Downtown and the Metrolink Station and to ensure that new housing along the south side of Second Street provides a suitable transition to the low-rise, single-family residential neighborhoods across the street to the north. New multi-family buildings facing the south side of Second Street will have two-story volumes at the front of the lot with massing that is in character with the single-family houses across the street. Higher massing – up to four stories – is allowed at the center and rear of the lot.

THE PLANNING PROCESS

The specific plan is both a record and a manifestation of the community's goals. Through participation in a series of public workshops and meetings, community members articulated a vision for the future of the community. This vision carries an expectation that these primary corridors should better represent the quality and character of San Fernando. They should be planned to provide an environment that is more comfortable for pedestrians than they presently do by employing roadway design that tames the current flow of traffic. Most of all, the residents of San Fernando wish to see new investment and activity in the corridors that convey the sense of uniqueness, pride and community spirit that differentiates San Fernando from other nearby communities.

In February 2013, the Los Angeles County Metropolitan Transportation Agency (Metro) Board awarded the City of San Fernando a grant to amend the Corridors Specific Plan to allow residential and commercial uses within walking distance of the Sylmar/San Fernando



A need for better pedestrian spaces was often cited by community workshop participants.



Community Workshop participants called for improved signage announcing the entry in San Fernando.

Metrolink Station, expand the Specific Plan boundary to include the areas west of Maclay Avenue and south of Second Street, generate an Environmental Impact Report (EIR) to assess the impacts of the Specific Plan update, and to amend the General Plan and Zoning Ordinance to ensure the proposed updated Specific Plan regulations, the Zoning Ordinance, and the City's General Plan are consistent with one another. The subsequent planning process focused on amending the Corridors Specific Plan to:

- Align land uses within the Specific Plan area with larger regional objectives to build housing and commercial uses adjacent to metro transit stations throughout the Los Angeles basin.
- Promote compact, pedestrian-friendly Transit Oriented Development (T.O.D.) around the Metrolink Station.
- Integrate the proposed East San Fernando Valley Transportation Corridor transit initiatives.
- Improve access to Downtown San Fernando and the Metrolink Station, and to better connect Downtown, the Civic Center, and nearby neighborhoods to the Metrolink Station.

To help guide the process, a Development Advisory Committee (DAC) comprised of City Councilmembers, Transportation and Safety and Planning and Preservation commissioners, property owners, business representatives, and residents was formed to:

- Identify the strengths, weaknesses, opportunities, and constraints of San Fernando and the Planning Area.
- Craft a vision for the Planning Area in terms of appropriate building types, heights, and land uses and street design strategies that generate a more pedestrian-friendly environment that also accommodates the transit initiatives proposed by Metro as part of the East San Fernando Valley Transit Corridor Study, while continuing to accommodate cars.
- Identify suitable areas for transit oriented infill development within the Planning Area.
- Provide input on the modifications to the Specific Plan.

On September 30, 2014, the DAC held the first meeting. The purpose of the meeting was for the City and the consultant to introduce the DAC to the project and get their initial input on the scope and approach to the project.

On November 12, 2014, the DAC held the second meeting. The consultant described some initial ideas for introducing transit oriented development around the Metrolink Station and how San Fernando Road, Truman Street, First Street, and Second Street, and the development along these corridors, could change over time.

On November 19, 2014, the City hosted the first of four community meetings with the purpose of introducing the project and the proposed project area, the historic setting, and the planning background; to describe what Transit Oriented Development is; to explain the relationship of the East San Fernando Transportation Corridor project; and to provide some initial ideas for how San Fernando Road, Truman Street, First Street, and Second Street could change over time. Twenty three people attended the first meeting that included a presentation by the consultant and followed up with a question and answer session.

On January 14, 2015, the DAC held a meeting to review the work to date in preparation for the second community meeting. The presentation included the preliminary zoning map, the key attributes of each proposed zone, and potential street and streetscape improvements for key streets within the Planning Area.

On, January 21, 2015, the City hosted the second community meeting to present a preliminary zoning map, to describe the key attributes and proposed modifications to each zone, and to show potential street and streetscape improvements for San Fernando Road, Truman Street, and First Street.

On June 17, 2015, a fourth DAC meeting was held. The consultant presented the latest iteration of the specific plan amendment, including the proposed zoning map and zone parameters, the proposed street designs, and the proposed development potential. Over 30 members of the public also attended this meeting.

During the 45-day EIR public review period, the third and fourth community workshops were held on August 28, 2017 and on September 15, 2017.

SPECIFIC PLAN'S RELATIONSHIP TO THE GENERAL PLAN

The San Fernando Corridors Specific Plan sets forth the planning principles, land use policies, development standards, and design guidelines for private development and public improvements within the specific plan area. In doing so, the specific plan implements the goals, objectives, and policies identified in the City of San Fernando General Plan, with particular emphasis in imple-



Community Workshop participation.



Community Workshop participation.

menting the goals and objectives set forth in the General Plan's Land Use, Housing, and Circulation Elements.

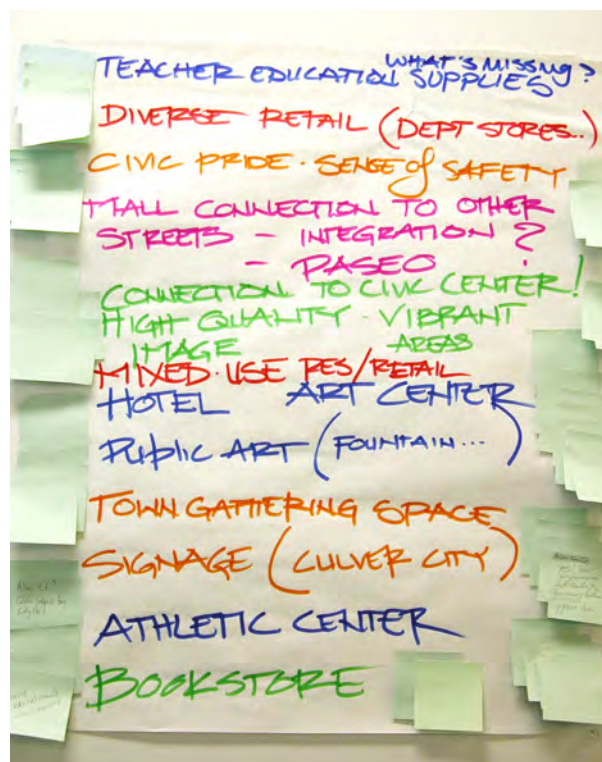
For example, as is discussed in detail throughout the specific plan, it provides for the clustering of commercial, higher density residential, and mixed-use development within the specific plan area in a manner that:

1. "retains the small town character of San Fernando, promotes the economic vitality of commercial areas, and maintains an identity that is distinct from surrounding communities," per San Fernando General Plan Land Use Goals I-III, Pg. IV-6;
2. "continue(s) to provide adequate sites to facilitate the development of a range of residential development types in San Fernando which will fulfill regional housing needs," per General Plan Housing Element Goal 3.0 and Policy 3.5; and,
3. "provide(s) an efficient street system which allows maximum safety and economy of movement," per Circulation Element Goal No. 1, Pg. V-5.

The specific plan thus provides for the systematic implementation of the San Fernando General Plan as it relates to the development of properties located in the specific plan area. The specific plan describes the types of uses and character envisioned within the specific plan area and the necessary public and private facilities, infrastructure improvements, services, and private property development standards that are designed to accommodate the planned new infill development. In doing so, the specific plan meets the State of California requirements for specific plans as set forth in *Government Code* Section 65450 (et. seq.).

SPECIFIC PLAN'S RELATIONSHIP TO THE ZONING CODE

The San Fernando Corridors Specific Plan replaces all zoning regulations previously applicable to the specific plan planning areas, which include the Downtown District, the Maclay District, and the Workplace Flex District. The specific plan supplants the zoning code as the regulatory tool within the specific plan districts and outlines the types of uses, development standards, and design guidelines that will guide future public and private development. In the event the adopted specific plan is silent as to a development standard or procedure, the provisions of the *San Fernando City Code* Chapter 106 (zoning) shall control.



Community Workshop comments.



Community Workshop participation.

DOCUMENT ORGANIZATION

Legal Application

The *San Fernando Corridors Specific Plan* is both an implementation and a policy document, in that it is intended both as a strategy for change and as regulatory policy to guide and govern future development along the corridors. It publicly states the community's goals, objectives and expectations for the corridors, and details the proposed land uses, capital and infrastructure improvements, standards for development and design, and implementation measures that will achieve these goals.

Under the legal authorization of Article 8 of the *California Government Code* (Sections 65450 - 65457), this specific plan, upon adoption, will become the primary means of regulating and directing land use planning and development within the corridors. The development standards and design guidelines in this specific plan will replace other policy governing the corridors, including those contained in the former *San Fernando Corridors Specific Plan (SP-4)* and the *San Fernando Zoning Ordinance*. However, any aspects of new development or redevelopment not covered in the specific plan must conform to the regulations of the *San Fernando Zoning Ordinance* (City Code Chapter 106) or other pertinent City regulations.

Document Chapters

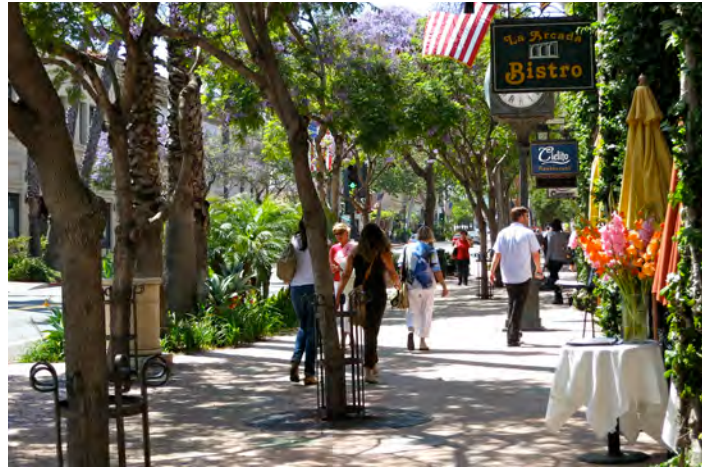
In addition to this Orientation Chapter, the specific plan contains the following elements:

- *Revitalization Strategy* – Chapter 2 presents the recommendations that will lead to the revitalization of the Maclay, Truman, San Fernando Road, and First Street corridors. The directions it contains are a direct result of the objectives established by the community, and provide the goals, objectives, and strategies that will achieve the goal of revitalization.
- *Land Use Framework and Urban Design Principles* – Chapter 3 contains the overall principles that structure the plan and its policies. It translates the community's vision for the corridors into a series of policies to direct change along the corridors.
- *Development Standards* – Chapter 4 contains the policies to be applied to properties in the specific plan area, organized by district. These policies consist of development standards, which are mandatory requirements directing use, intensity and development structure.

- *Design Guidelines* – Chapter 5 contains design guidelines, which are recommendations for articulating buildings, landscapes, and signage to conform to the character and design quality demanded by the community.
- *Capital Improvements* – Chapter 6 describes the capital improvements that are integral to the envisioned future of the San Fernando Corridors. These capital improvements, including streetscape improvements, architectural landmarks and gateway features, will set the stage for revitalization of the San Fernando Corridors.
- *Utilities and Infrastructure Plan* – Chapter 7 describes the impacts to the sewage, water, drainage, solid waste disposal, energy, and other essential facilities needed to support the land uses described in the plan. Based on these impacts, objectives and policies for improvements to the existing facilities are provided.
- *Implementation* – Chapter 8 lists the public actions that are a critical aspect of the community's vision for the corridors. It describes the key steps needed to implement the specific plan, such as capital improvements, streetscapes, gateways, catalyst projects and other programs that will spur revitalization efforts. It also contains a statement as to the financing measures that will be necessary to carry out this specific plan.
- *Appendices* – Included as appendices to this plan are the San Fernando Corridors Specific Plan Circulation Plan (Appendix A); the Existing Physical Conditions Analysis (Appendix B); the Opportunities & Constraints Analysis (Appendix C); the Land Use Survey (Appendix D); the Parking, Access, and Linkage Study (Appendix D); and the Community Assets Assessment (Appendix F).

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CHAPTER TWO: REVITALIZATION STRATEGIES





The corridors should be places within the City, not just traffic conduits that move you through the City.



The corridors should attract new investment, like the Library Plaza development.

The purpose of this chapter is to lay out the recommendations for the revitalization of the Maclay, Truman, San Fernando Road, and First Street corridors. The basis for these recommendations originated in a community vision for corridor revitalization developed through a public workshop process. The vision was then refined through collaboration with city staff, the Development Advisory Committee, the Planning and Preservation Commission, the City Council, and the consultant team. The resulting objectives and revitalization strategy have been expanded into a set of recommended actions to be taken by the City to achieve the ends of revitalization.

REVITALIZATION OBJECTIVES

As described in Appendix B (Existing Physical Conditions), the Maclay, Truman, San Fernando Road, and First Street corridors are centrally important “pieces of city” in San Fernando. They play a strong role in the community’s daily life, provide access through the city and to its neighborhoods, and are the home of most of the city’s commercial and service establishments as well as many civic and cultural facilities. However, their lack of reinvestment and unattractive appearance represent missed opportunities and lost potential for the community.

The overall goal of the *San Fernando Corridors Specific Plan* is to breathe new life into the corridors by removing obstacles to change, investment, and care. The following objectives and strategies are intended to transform the corridors from unfocused commercial roadways into places of community pride. Objectives within the goal are as follows:

1. **Establish the city’s corridors as the armature of the city.** Define the Maclay, Truman, San Fernando Road, and First Street corridors to be major spines of city character and activity. The corridors should have a more civic quality and structure that befits their roles as central spaces of the city’s neighborhoods. They should be recognizable not just as the major traffic conduits of the city, but also as active, livable and unique places in their own right.
2. **Remedy the feeling of “sprawl” on the corridors.** The corridors can contribute to the city’s distinctiveness from the rest of the surrounding San Fernando Valley by not emulating the “sprawl” character typical to the Valley’s commercial corridors.
3. **Attract new investment appropriate to the envisioned character of the corridors.** Enable the corridors to be attractive places for new businesses, residences, entertainment and civic centers, and workplaces desired by the community. Configure the patterns of uses, building scales, and activity to be compat-

ible and mutually reinforcing of value and livability. Assist existing businesses and establishments to play their part, and bring in new ones that the community feels are missing.

4. *Revitalize the identity and investment climate of the city as a whole.*
5. *Make walking, cycling, and driving along the corridors a more pleasant experience by improving the physical settings of corridor streets.*
6. *Use the corridors to enhance San Fernando's identity to visitors.* Since the corridors are the most visible places for visitors and residents, put the best of San Fernando's identity on display, in terms of its architecture, culture, and community. Make sure that entering the city is attractive and memorable.
7. *Promote compact, pedestrian-friendly Transit Oriented Development (T.O.D.) near the Metrolink Station and future public transit stops.*
8. *Integrate Metro's proposed East San Fernando Valley Transit Corridor transit initiatives to improve access to Downtown San Fernando, the Civic Center area, future Metro transit stops and the Metrolink Station.*
9. *Better connect Downtown, the Civic Center, and nearby neighborhoods to the Metrolink Station and future Metro public transit stations/stops.*
10. *Streamline the development review, entitlement and building permit issuance process.*
11. *Use City police powers, including Code enforcement, to support investment and require responsible property maintenance.*

STRATEGIES

The strategies that follow contain the specific actions intended to revitalize the San Fernando corridors. These are provided as action steps to be taken by the City. They range from the formation of districts, to land use, zoning and policy changes, to specific capital improvements and design principles for development along the corridors.

1. *Transform the corridors into a series of defined districts.* The monotony of the corridors should be separated into a series of distinct places. Each district should be identifiable as a physical place that is distinguished from other parts of the city by a unique orientation, a harmonious character, and a consistent aesthetic. Each district will have a mix of land uses that work together, building forms that are identifiable to that district, and an overall configuration of public spaces and facilities that support



Walking along the corridors should be a pleasant experience.



A lively mixed-use district in Ventura, CA



Buildings of various heights and lush landscape along Santa Barbara's State Street.

the district form. Each district will take advantage of each corridor's physical location to meet different community and regional needs. These corridor districts will be joined to San Fernando's overall city pattern and to its residential neighborhoods to create a cohesive town pattern that reflects the lifestyle of the community. The key districts to be created are:

- A. *The Downtown - The Center of the City:* The most public areas of the city – the City Center, its Civic Center and the San Fernando Mall - have the potential to create a Downtown core that is an exciting centerpiece for San Fernando. They are located at the crossroads of Maclay Avenue and the Truman/San Fernando corridors, and together roughly form the shape of an inverted "tee" area on a map. The core of the City, the Downtown District should contain only the most active types of retail, from small-scale storefronts and independent storefronts to restaurants and specialty services. Within the Downtown District, the Downtown Residential Overlay seeks to capitalize on City surface parking lots located in the Downtown adjacent to the Mall area in order to play a direct role in facilitating mixed use projects that incorporate one more of the following: public and private parking facilities, residential, retail, service commercial and entertainment uses.
- B. *The Mixed-Use Corridor - A Transit-Oriented Neighborhood:* The current disparate commercial land uses scattered along the parallel spines of Truman Street and San Fernando Road west of San Fernando Mission Boulevard can be transformed into a walkable, mixed-use district where mixed-use development – including residential and live-work components as well as health and professional service sectors – is located within walking distance of the San Fernando Mall to the east and the Metrolink Station to the west.
- C. *The Auto Commercial District - A Flexible Business District:* Located east of the San Fernando Mall, the Auto Commercial District will continue to accommodate San Fernando's auto dealerships, while also accommodating a wide array of retail, restaurant, and office uses.
- D. *The Maclay District -The Neighborhood Spine:* The reestablishment of a neighborhood spine along Maclay Avenue can serve two purposes. First, Maclay Avenue has a substantial number of underutilized commercial parcels that have the potential to be restructured to enable pent-up investment opportunities to expand the city's



Small scale storefronts, restaurants, and pedestrian activity - Berkeley, CA.



Proposed Mixed-Use Prototype.



Existing Mixed-Use Development on Maclay Avenue.



A Mission Style Restaurant - Albany, CA



Outdoor Dining - Los Angeles, CA



Multi-Family Residential in a corridor environment - Ventura, CA

supply of housing. Second, allowing attractive residential development along the corridor will improve the visual quality of Maclay Avenue by making one of the city's greatest assets, the character of its residential neighborhoods, visible on the thoroughfare.

E. The Workplace Flex District - The City's Workplace: The Workplace Flex District is established along First Street and along the north side of Truman Street between Kalisher Street and Hubbard Avenue to support the continued functioning and expansion of the City's light industrial, workshop, and large-scale commercial sectors. It also accommodates live-work uses, subject to a conditional use permit. The Workplace Flex District provides a framework for creating a more inviting pedestrian, bicycle, and vehicular connection along First Street between the Metrolink Station and Maclay Avenue's "main street," the Civic Center, and along Truman Street between the Metrolink Station and Downtown.

F. The General Neighborhood District - Neighborhood Compatible Multi-family Housing: The General Neighborhood District is established to ensure that multi-family housing along the south side of Second Street transitions between First Street's commercial and light industrial buildings, and the single-family residential neighborhoods to the north. New multi-family buildings face Second Street with two-story masses at the front of the lot that are in character with the single-family houses across the street. Higher masses - up to four stories - are allowed at the center and rear of the lot.

It is important that the streets within each sub-district serve the development type intended for each sub-district. As each sub-district is intended to serve as a location for a specific set of land uses and functions, the streetscapes within each sub-district should be designed to support these uses and functions.

2. Promote the right kind of investment in each district. The biggest problem the corridors (and by extension, the city as a whole) face can be summed up in a single statement: too much commercially-zoned land, and not enough land for housing. While the corridors have been zoned solely for commercial and industrial use for two generations, their development potential has never been fulfilled in that time. While there are good businesses along the corridors, many parcels are vacant, underutilized, or disinvested, dragging down the image of the corridors and the city, and continuing to impede new investment. Meanwhile, the city is nearly built out

and there are few opportunities for new housing - yet second and third-generation San Fernandans want to live in the community and older residents may want to remain in the community but in a different housing type. Land use controls can be used to address this problem by allowing a balance of uses that more closely match the needs of the community. Specific land use strategies to address this issue are stated below:

A. *Prune back the amount of land zoned for retail use.* Like many cities, San Fernando has designated its primary corridors for commercial use. This has led to an over-zoning of land for retail, a problem common to many cities and identified in publications by respected urban design organizations such as the Congress for the New Urbanism, in "Civilizing Downtown Highways", and the Urban Land Institute, in "Ten Principles for Reinventing America's Suburban Strips". Because its potential retail destinations are scattered across all three corridors, it is difficult for the City to capture the maximum value from each establishment.

- Cluster retail uses in the Downtown District, Mixed-Use Corridor District, and the Auto Commercial District. Encourage activity-generating ground-floor uses in the Downtown District, such as retail, restaurant, lodging, and entertainment. Keep a tight rein on retail in areas outside of these districts, especially along Maclay Avenue.
- Encourage a mix of new jobs producing businesses, good quality infill housing, and local-serving retail and restaurant uses along the west end of the Truman / San Fernando corridors, especially within comfortable walking distance of the Metrolink Station and Metro's existing and future transit stops.
- Build on the city's successful auto-sales area, located at the east end of the Truman / San Fernando corridors, to bring additional auto dealerships to this district's eastern end. Use the locational assets of this area to attract large-scale commercial and support services, as well as retail and professional uses.
- Limit retail along Maclay Avenue to defined "Neighborhood Serving Overlay Areas" - such as at its intersections with Eighth Street and Glenoaks Boulevard. These clusters should be designed as small, convenient, walkable centers for their immediate neighborhood, and retail uses in these areas



Mid-Density Multi-Family Residential - San Diego, CA



Multi-Family Residential in a corridor environment - Ventura, CA



A retail and civic district - Ventura, CA



Retail storefronts along Maclay Avenue in the City Center Sub-District



An active retail street - Santa Cruz, CA

will be limited to those that provide convenience goods.

B. *Increase residential opportunities within the specific plan area.* Throughout the community process, citizens expressed a need for housing that is unique to San Fernando and accommodates a variety of income levels, both affordable and market rate. Young working professionals who grew up in the area and choose to return, hoping to settle in the area where they were raised, find that there is limited housing stock available for them to move into.

- Change the focus of Maclay Avenue between Fourth and Eighth Streets, from service and auto-oriented commercial uses to predominantly (but not exclusively) residential ones. Allow housing at densities high enough 1) to provide housing units that can serve the local population, 2) to generate developments that are well-matched to the corridor environment and 3) sensitive to the scale, character and value of existing neighborhoods behind corridor-fronting properties. Spur developer interest in sites along the corridor by identifying opportunity sites along Maclay Avenue, promoting public/private partnerships in projects, and streamlining the overall investment process.
- Provide opportunities for mid-density residential development, with an eye on for-sale housing opportunities, in conjunction with compatible retail and office uses in the Mixed-Use Corridor Sub-District along San Fernando Road. Capitalize on the proximity to Downtown, the Metrolink Station, and Metro's existing and future transit stops in order to encourage residential development in this area.
- Establish the Downtown District as a desirable place to live for residents seeking an active, vibrant "round-the clock" living environment located near transit. Permit housing and office uses on upper floors within the Downtown Residential Overlay to help generate a lively, safe atmosphere throughout the day and evenings. Promote mixed-use development (i.e. residential units or office uses in combination with retail and restaurants) throughout appropriate areas within the Downtown.
- Ensure that multi-family buildings, especially north of the Union Pacific Railroad Tracks and along Celis Street, are designed to be compatible in form and scale with adjacent and nearby single family houses.

C. *Develop strategies to attract and support businesses in the downtown.* Overall, policies for the downtown will work to create a vibrant pedestrian scaled district that is everybody's destination for the vital storefront retail, restaurants and services it provides, as well as for the unique San Fernando character it embodies. Improvements for the area along Maclay Avenue between First Street and San Fernando Road face a daunting task, which is to overcome the physical "gap" created by the train tracks and the major arterial intersection of Truman and Maclay. It is important that this gap is healed not only for physical reasons like pedestrian connectivity, but for social ones as well. The Downtown should be a unified center for all of the residents of the city, and not divided into separate economic or social enclaves between the Civic Center and the San Fernando Mall.

- Encourage new retail along Maclay Avenue between First Street and San Fernando Road. Require new developments to create continuous street activity along Maclay and support an active link between the activity north of the railroad and the San Fernando Mall. Initiate the transformation of parking lot sites into activity-creating uses along Maclay Avenue between the railroad and First Street and on the south side of Truman Street to aid in establishing this link.
- With the redevelopment of these parking lots, ensure the replacement of spaces to support the continued success of business tenants. Consider additional strategies for accommodating parking as the Downtown intensifies, such as a shared parking program, and possible park-once parking structures to support future demand.
- Enhance parking areas to make them safer places. Where possible, construct mid-block "paseos" to connect parking to the retail activities throughout the Downtown, and improve lighting and signage to make a more comfortable experience for the customer.
- Proactively recruit the kinds of businesses that will contribute the most to the community and to the Downtown District. Use inducements such as low-interest loans and grants to entice new establishments to locate within the Downtown.
- Investigate business relocation options to bring valuable community retailers into the Downtown (particularly types of businesses that are seen as "missing" from the current



Paseo linking Main Street to parking lots- Claremont, CA



Strategies should support the ongoing success of merchants in the San Fernando Mall.



Downtown signage should be attractive and well integrated into facade design, as specified by the design guidelines.



The streetscape of Maclay Avenue should support residential development with landscaping that “buffers” homes from traffic.



Street trees provide shade for pedestrians.

mix of businesses), and to assist existing community business that are not compatible with the vision for Downtown in finding alternative sites within San Fernando. Consider allocating an annual budget for this purpose.

- Work with the San Fernando Chamber of Commerce, the San Fernando Mall Association, and the Northeast San Fernando Valley Chamber of Commerce to encourage “after 5:00” business hours throughout the Downtown. Promote “special event” evenings, in cooperation with civic events or entertainment, to initiate later operating hours on certain nights.
- Within the Downtown District, consider the implementation of a signage improvement program. Provide a small-scale but high-visibility “kick-start” by awarding grants to qualified businesses for signage improvement, in keeping with the high quality signage demanded by the design guidelines.
- Consider appointment of a part- or full-time Downtown coordinator to oversee and encourage future investment in the Downtown.
- Consider adopting a mural ordinance to allow the installation of murals on buildings within the Specific Plan area. Murals will nurture creative and artistic expression and contribute to downtown San Fernando’s distinctive sense of place.

3. **Employ capital improvements to “set the stage” for new investment.** The public spaces of each district -most notably streets and plazas - should be a clear indication of kind of place the city hopes to create. For example, where residential land use is prominent, the street should support this use, with plenty of shade trees to buffer homes from the street while new residential development should establish landscaped frontages where appropriate. The pedestrian environment should also be buffered from automobile traffic with street trees and on-street parking to ensure that residents feel comfortable walking along the corridor. Where corridor retail uses are developed, streets should maintain a welcoming and public character. They should be designed to attract pedestrians, with sidewalks large enough to feel like public spaces and places to stroll and to sit. They should be designed to facilitate automobile and transit traffic as well, allowing visibility to stores and providing convenient access to parking. Specifically, the following improvements should be acted upon:

A. *Redesign the corridors to support the envisioned development pattern of their districts.* Streetscape design for each corridor should be specific to the uses and character of each district, as follows:

- The Maclay District (Maclay Avenue between Fourth and Eighth): North of Fourth Street, the environment of Maclay Avenue should complement the residential development of the new neighborhood spine. Large deciduous trees should buffer the sidewalk and homes from traffic and parking lanes while providing an abundance of shade. Street lighting should be provided by pedestrian-scale (13' or less) decorative fixtures to emphasize the residential neighborhood scale and character (as opposed to engineering-styled "cobra-head" lights). Consistent landscaped setbacks should be required of new residential development. All of these improvements should work together to emulate the qualities and character of the residential neighborhoods located to either side of the corridor, and provide desirable "boulevard addresses" for new investment.
- The Downtown District: New streetscape design will be fundamental to the revitalization of Downtown. Key improvements include:
 - Redesign Maclay Avenue between First and Fourth Streets to prioritize the pedestrian and to slow traffic to create a "heart of the city" streetscape. Large open-habit trees should shade the sidewalk while providing visibility to retail establishments. Streetlights and furniture should transform the sidewalk area to a human-scaled public space, while new benches provide opportunities for seating. On-street parking along Maclay Avenue south of the railroad tracks should provide convenience parking for local shops and services, while buffering pedestrians from vehicular traffic.
 - Establish improved pedestrian links along First and Second Streets to encourage interaction between the Metrolink Station and Maclay Avenue's shops, restaurants, and other businesses. Way-finding signage should be expanded and mark the route to the Civic Center, and enrich the pedestrian routes along First and Second Streets.



Large open-habit street trees should provide visibility to retail establishments.



A downtown street with street trees in the parking zone in Santa Cruz, CA.



A rendering of the proposed streetscape for the San Fernando Road Mixed Use Sub-District.



The proposed gateway feature at the city's western border along San Fernando Road.



Architectural landmark features to define the Downtown District.



A parking lot is lined with a low wall, trellis structure, and benches.

- Truman Street serves as one of the city's most frequently traveled east-west roadways. Thus, capital improvements must support the role that the street plays by introducing a grand boulevard design while maintaining the street's ability to function as a primary corridor for local and regional traffic demands. At the corridor's eastern and western gateways, large vertical landscape elements such as palm trees should differentiate this district from the adjacent Los Angeles districts. Along its length, large deciduous shade trees should line the sidewalks, and new lights and street furniture should create a pleasant pedestrian realm, especially at public transportation stops. Crossing distances should be decreased where possible, especially along Truman between San Fernando Mission Boulevard and Brand Boulevard, and across its intersection with Maclay Avenue. Curb-side parking should be maintained where possible.
- Within the Downtown District, the streetscape of Truman Street between San Fernando Mission and Brand Boulevards should celebrate this linchpin intersection to create a kind of "front door" to the San Fernando Mall. Along the south side of Truman Street in the Downtown District, streetscape improvements, along with on-street parking, should ensure that pedestrians feel comfortable and safe as they walk along its downtown-scaled development.
- The Mixed Use Corridor District (San Fernando Road between Hubbard Avenue and San Fernando Mission Boulevard): In this District, streetscape design should enable the creation of an area where residential, live-work, office and convenience retail services are equally supported. The revitalized street environment should include large shade trees punctuated by palm trees or other city-specified trees and unique streetlights to help distinguish this sector of the corridor. Potential angled parking spaces along both sides of San Fernando Road will further extend the pedestrian-friendly street character of the Mall, serving to calm traffic movement while providing additional parking for local businesses and services. If the East San Fernando Transit Corridor's Tram alternative is introduced, on-street parking should be preserved to ensure convenient parking in front of Downtown stores, restaurants, and other businesses and consideration should be given to nar-

STREET FURNITURE - "FAMILY OF OBJECTS"



Columbia Cascade
Timberform Classics
Craftsmen
2663-6 & 2660-6



Columbia Cascade
Timberform Classics
Trash Receptacle
2667-AT



Urban Accessories
Chinook tree grate
5' square with hole for
uplight at corner



Tolar Highlands Ranch
Bus Shelter



Cycloops 2170 or similar
Bicycle Rack



Holophane "Prague
Series" Streetlight
(simulation)



Holophane "Prague Series"
Streetlight- color: black



Downtown architecture - Ventura, California.



Architectural details from historic Mission San Fernando Rey.

rowing the roadway in order to provide wider sidewalks.

- The Workplace Flex District: First Street contains many of the city's light industrial, warehouse, and general commercial uses and also provides the most direct link between the Metrolink Station and Maclay Avenue and the Civic Center. Capital improvements should be simple and directed toward making a more comfortable environment for cyclists and pedestrians while maintaining the street's ability to accommodate large trucks. Curbside parking should be maintained and large deciduous shade trees should be introduced between every second or third parking space. Traffic lanes should be striped as sharrows.
- B. *Define the entrance to the city along the corridors, with gateway features at city boundaries. The community pride felt by the citizens of San Fernando should be physically expressed at its gateways, to distinguish the city from its surrounding areas.*
- At the city's northern entrance along MacLay Avenue, introduce prominent building architecture to bolster the existing Eighth Street gateway arch in marking a prominent entry into San Fernando.
 - At the city's western border along San Fernando Road at Hubbard Street, define the City's entrance with architectural "landmarks" that give a visual cue to San Fernando's vivid identity and history, and the promise of a vibrant downtown at the center of the city.
 - At the city's eastern border along San Fernando Road at Fox Street, build upon the existing monument gateway via implementation of new signage and landscaping that announces the approaching Downtown District and the San Fernando Mall.
- C. *Utilize street and public space design to create a unified downtown, as follows:*
- Use architectural landmark features to define the Downtown District. Landmark features throughout downtown such as corner towers, two-or-more story buildings, and storefronts built up to the sidewalk edge can help to mark and define the City's core. Design elements may include opportunities for public art as well as enhanced street lighting.

- Maximize connections (visual and circulatory) between the Downtown, the Civic Center, and the Metrolink Station that lies to the west. Take opportunities to create view corridors and pedestrian passages to the Civic Center from Maclay Avenue. Consider future capital improvements along First and Second Streets to carry the fabric of downtown to Macniel Street and to provide a more inviting pedestrian and bicycle connection to the Metrolink Station.
 - Implement a signage and way-finding program to help commuters, visitors, and residents navigate the corridors in a legible way, marking destinations and interest points.
4. *Ensure high-quality development and design.* Development along the San Fernando corridors is on display, visible to both residents and visitors who travel along the roadways. The City should ensure that new development represents the strength and quality of the community. To this end, the following actions should be taken:
- A. Require developments that respect and enhance the corridors - their primary address - with facades that enliven the street wall and main entrances that front the street. Regulate minimum heights, setbacks and other unifying factors to ensure that development lives up to its role along the corridor. (The *specific plan design guidelines* for each district will give further specificity as to the character of district development.)
 - B. Direct new buildings to adhere to the spirit of the *specific plan design guidelines*, and to be compatible with the scale and character of its district. For example, in the Downtown District, new buildings should be designed with features of the “core” architecture - narrow facades, active frontages and intricate detailing. Along the neighborhood spine of the Maclay District, buildings should contribute to a feeling of “neighborhood”, architecturally subdivided and composed at a human scale with variation in massing and height. Within the Mixed-Use Corridor District, buildings should create a strong street edge, whether at the back of the sidewalk for commercial and mixed-use buildings, or behind a small landscaped setback for residential buildings. (Refer to the specific plan design guidelines for specific directions for buildings in each district.) New buildings along the south side of Second Street should step down to one story and be compatible in



Architectural details from historic Mission San Fernando Rey.

massing and scale to the single-family houses along the north side of the street.

- C. Recommend an architectural and landscape “design language” that reflects and relates to the architectural history of the city. Rather than imposing only one historic style, encourage a variety of styles, in keeping with the diverse and eclectic character of the city; there may be individual locations where greater coherence should be maintained. In the Downtown District, new buildings should respond to its history and fine-grained form. Near the San Fernando Mall, structures should build upon the Spanish influences that dominate this unique area. In other areas, the eclectic architectural personality of the city should be recognized, giving a wide-range of influence that includes historic and contemporary styles. All styles should emphasize craft, neighborhood scale, and quality of construction.

CHAPTER THREE: LAND USE FRAMEWORK AND URBAN DESIGN PRINCIPLES



This chapter contains the regulatory portion of the Specific Plan, providing guidance for all new investment along the Maclay, Truman, San Fernando and First Street corridors falling within the Specific Plan Area. It contains an overview of the districts upon which the policies of the specific plan are based. Following the overview is a breakdown of the district-based development standards and design guidelines. These provide the framework for new investment, ensuring that the specific plan goals are implemented. Proposals for new construction are required to adhere to the development standards and design guidelines for their respective city district and any sub-districts within which they may fall.

Following the overview of the development standards and design guidelines, a section entitled “The City District” describes the principles and value of cohesive city districts and the fundamental role they will play in revitalizing the San Fernando Corridors.

THE PURPOSE OF THE PLAN

The overall goal of the San Fernando Corridors Specific Plan is to breathe new life into the corridors that play such a strong role in the community’s daily life. The plan is set up to do so by shaping new investment in combination with site improvements, redesigned streets and new public spaces. The following land-use and design policies organize the land within the specific plan area into a series of districts around which the regulatory framework of the plan is based. These districts provide land use policies (typical of common zoning policy) and also form the basis for the development standards and design guidelines that will guide the look and feel of future development within the specific plan area.

THE CITY DISTRICT

A *city district* is an identifiable area of a city that contains closely integrated land uses and design character. City districts may be comprised of a mix of land uses, a variety of building types and open spaces, and populated by a diversity of peoples, yet they share a common and interrelated set of patterns and characteristics that distinguish them from surrounding areas. These work together to reinforce the community’s ability to identify a district as a specific, identifiable place in the city’s fabric.

District formation can often be the result of many influences, including physical, social, and temporal conditions. Physical conditions can help to form the structure of a city district, as in areas of settlement that were developed in relation to significant landforms such as ocean fronts, prospects, and river valleys. Social conditions

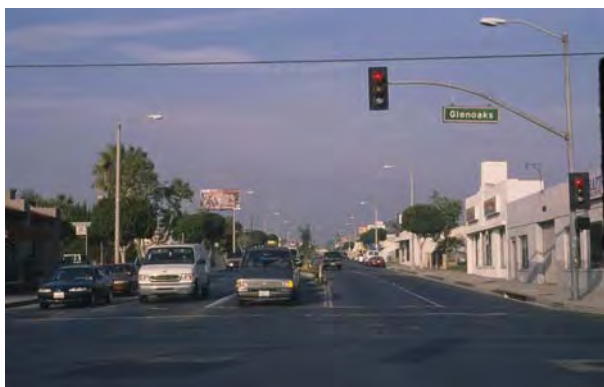
can also influence the creation of a city district. Areas often develop along historical settlement patterns that are based on socio-economic elements, such as a significant center of trade as in a seaport or rail depot. A district can also be created around a significant social milestone, such as the founding of the missions in many California cities, or be strengthened by an event that brings a community together.

The condition of time can also assist in district formation, as can be evidenced in areas that developed all at once as a result of municipal planning efforts, thereby developing a single cohesive character instead of growing and changing over time. Examples of these districts include “urban renewal” areas within existing cities that were wholly redeveloped in association with post-World War II redevelopment policies, or new towns and subdivisions that were created all at once on “greenfield” sites. These underlying common elements, ranging from a place’s physical pattern, to its social history, to its era of development, can give a unified meaning to a specific area. They serve to tie various components of the district – its buildings, spaces, residents and users – together to create a shared identity that is both recognizable and memorable.

The Value of a City District

Identifying and shaping city districts is valuable for a variety of reasons. First and foremost, the establishment of a common character and a set of compatible land uses can stabilize and even increase land values. Having agreed-on rules for character that guides development and design reduces the level of risk to prospective investors where similar establishments and complimentary land uses are proven occupants of the area.

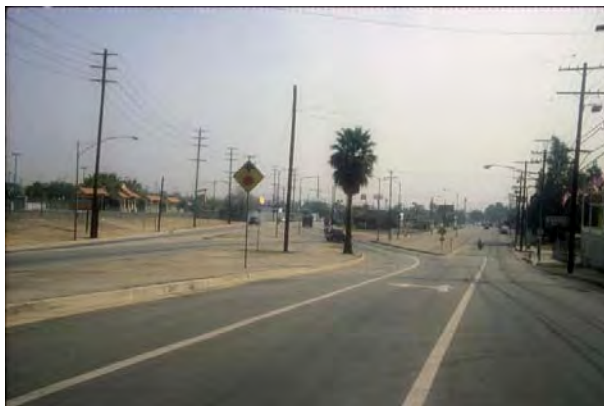
Secondly, a city district reinforces a place’s character. People grow to identify the community with memorable places. Each district is distinguished from other districts, with clear boundaries that let you know when you are inside and outside. One’s experience in the district forges an understandable sense of place, providing a framework upon which an individual can orient themselves in terms of local and regional context. The city district provides clear evidence of this context to its users through several cues. It can express its purpose through the forms and functions of the buildings, spaces, and to a certain degree, the behavior of its occupants. Common degrees of aesthetics, scale, and intensity can give an indication to the district’s purpose. The dimensions and orientation of its built forms and spaces can give expression to its identity. Perceptual qualities of sight and



Maclay Avenue in the Maclay District.



Storefronts in Downtown.



The City's entry in the Truman/San Fernando District.

sound - how lively a district feels, the hours it is used, and even the colors that permeate it - can further define one's understanding of a district. As a memorable and distinctive point of reference, a city district's identity is shared among individual members of a community, and reinforces their sense of belonging..

The Districts of the San Fernando Corridors

As described in Chapter 1: Orientation, the Maclay, Truman, San Fernando and First Street corridors presently do not belong to any noticeably identifiable city district. While each corridor plays a role in the physical pattern of the city and contains nodes that are unique centers of commerce and community gathering, they appear as places of unfocused commercial and light industrial land. Development along the corridors bears little allegiance to historical or local character, and has few ties to the social and economic patterns of the city. The goal of the policies that follow is to define the corridors as components of identifiable city districts, so as to encourage the type of investment and experience supportive of community identity within the City of San Fernando.

The policies for each city district are made up of controls on a set of uses, scale and intensity, as described in the Development Standards (Chapter 5); and recommendations for a complementary range of aesthetics, as described in the Design Guidelines (Chapter 6). While each district will be made up of a variety of land uses and building types, the policies will ensure that they have in common a particular set of qualities and attributes that unify them as a distinct piece of city fabric.

Examples of development scenarios that show how these policies could manifest along the corridors are depicted in the "Opportunity Site" illustrations that follow on page 54 and 54. These illustrations demonstrate an example of envisioned change over time, according to their district character, at selected opportunity sites along Maclay Avenue within the Maclay District, and along San Fernando Road within the City Center Sub-District.



Opportunity Site Concept 1:
Mixed use and Residential
development at the intersection of
Maclay Avenue and Glenoaks
Plan Scale 1" = 30'

Single-Family Townhouses establish
neighborhood presence along Maclay Avenue

Automobile 'paseo' serves to transition
between residential and mixed-use
development

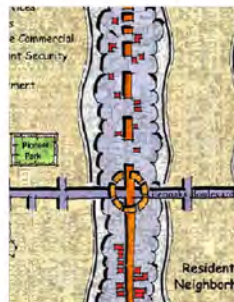
Neighborhood-serving mixed-use
development located at the intersection of
Maclay Avenue and Glenoaks Boulevard



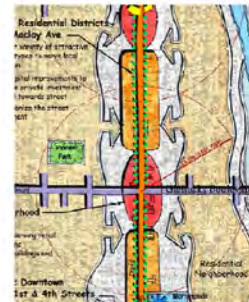
Perspective Sketch (not to scale)



Existing
Development



Patterns of
Development &
Change



Revitalization
Strategy "A"



Opportunities
for Change





CITY OF SAN FERNANDO
Macley & San Fernando/Truman Corridors Specific Plan

Opportunity Site 2- San Fernando Road



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CHAPTER FOUR: DEVELOPMENT STANDARDS



4.1. PURPOSE

This Chapter of the San Fernando Corridors Specific Plan provides detailed regulations for development and new land uses within the specific plan area. These provisions supersede and replace regulations in the *San Fernando City Code*, Chapter 106 (Zoning). This Development Code is intended:

- A. To revitalize the City's commercial corridors so they better represent San Fernando's small-town character and downtown's mixed-use heritage.
- B. To promote economic development by streamlining the planning and entitlement process.
- C. To allow and encourage greater mixed-use development in appropriate locations.
- D. To enable the creation of a walkable, mixed-use, multi-modal environment that accommodates retail, office, light industrial, and residential uses within walking distance of the Sylmar/San Fernando Metrolink Station and Downtown San Fernando.
- E. To facilitate the transition of the Maclay Avenue, Truman Street, San Fernando Road, and First Street corridors into, pedestrian-friendly, multi-modal streets that complement the land uses and development pattern planned for the corridors through the implementation of this specific plan.

4.2. APPLICABILITY

Proposed development, subdivisions, and new land uses within the plan area shall comply with all applicable requirements of this Chapter, as follows.

- A. **City Land Use Districts and Overlays.** Figure 4.1(City Land Use Districts and Overlays Map) shows the land use districts within the San Fernando Corridors Specific Plan area and identifies the parcels included within each district. The characteristics of each district are described in Section 4.3. (Characteristics of Specific Pan Districts).
- B. **Land Use and Permit Requirements.** Section 4.4 identifies the land use types allowed by the City in each of the districts established by Figure 4.1 (City Land Use Districts and Overlays Map). A parcel within the San Fernando Corridors Specific Plan area shall be occupied by the land uses identified as allowed within the applicable zone by Section 4.4, subject to the type of City approval (for instance permitted by right, conditional use permit) required by Section 4.4.
- C. **Development Standards.** The development standards in Section 4.5 regulate the building envelope

and the features of buildings that affect the public realm of the street, sidewalk, and public open spaces. The development standards regulate building intensity, building height, building setbacks, required frontage types, allowed architectural elements, required on-site open space, parking placement, and parking requirements and vary according to the zone applied to a parcel by the Figure 4.1 (City Land Use Districts and Overlays Map). Proposed development shall comply with all applicable standards in Section 4.5.

- D. **Design Guidelines.** The Design Guidelines in Chapter 5 are recommendations intended to increase the awareness of building owners and designers to the architectural, historic, and site planning features that are traditional to San Fernando; to illustrate options, solutions, and techniques to encourage high-quality design and construction; and to provide potential applicants and the City with a basis for proposing and reviewing development applications. These guidelines are not meant to dictate specific design solutions or stifle creative design

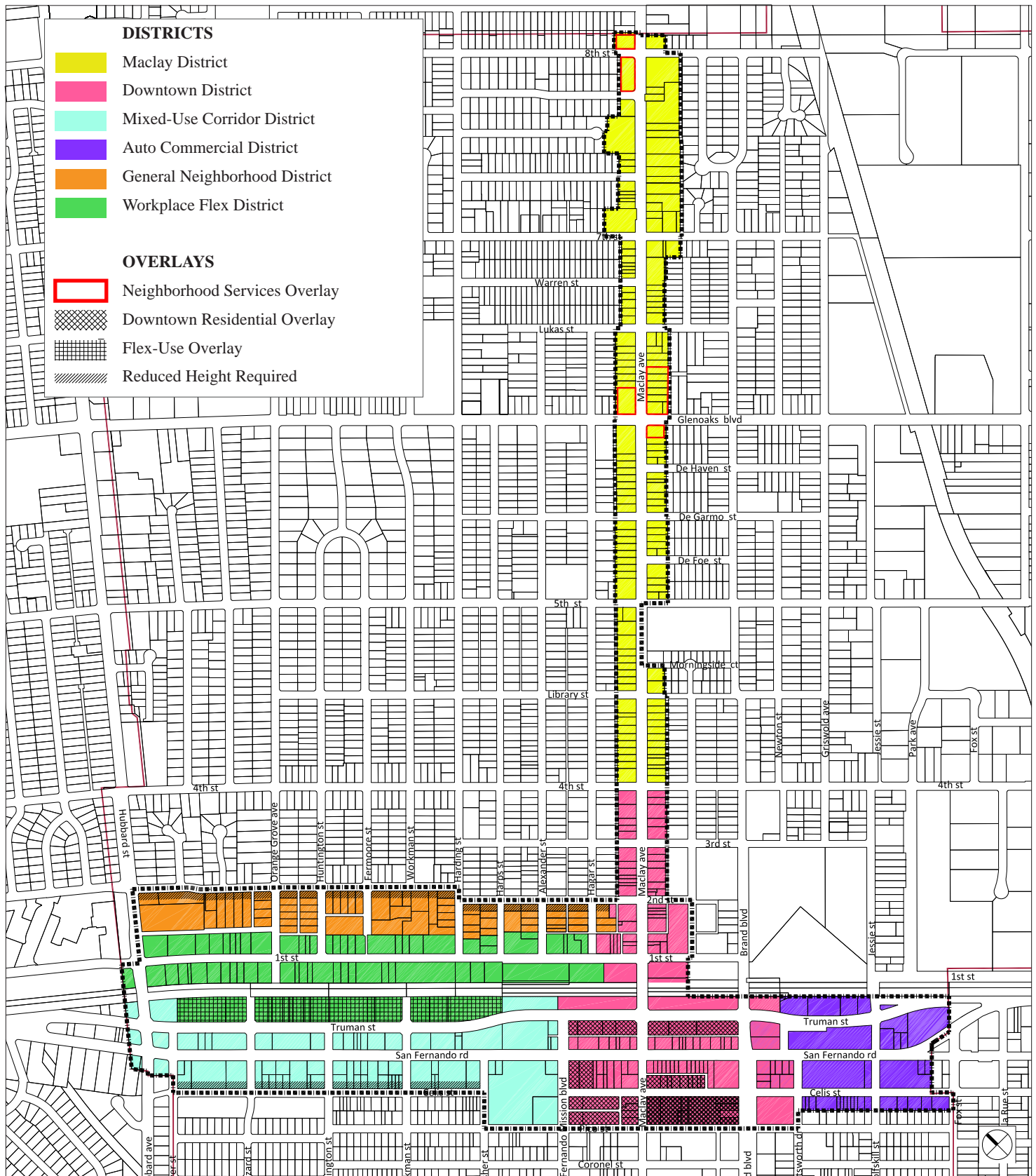
4.3. CHARACTERISTICS OF SPECIFIC PLAN DISTRICTS AND OVERLAYS

The following descriptions of each downtown district identify the characteristic uses, intensity of uses, and level of development intended for that district.

A. DISTRICTS

1. **Downtown District.** The Downtown District is established for the purpose of creating a lively, mixed-use, transit-oriented "center of the city" where the community of San Fernando comes together. Buildings are required to be located directly at the back of sidewalk with active storefronts that generate activity and interest along the streetfront. Buildings that do not contribute to such activity, such as freestanding stores, automobile-oriented uses and drive-up service windows, are not permitted.
2. **Mixed-Use Corridor District.** The Mixed-Use District is established for the purpose of creating a mixed-use, transit-oriented neighborhood between Downtown San Fernando and the Sylmar/San Fernando Metrolink Station. The Mixed-Use Corridor District will support development of a mix of use types, ranging from residential and office uses to retail stores and services and is dedicated to lively street-

FIGURE 4.1 - CITY LAND USE DISTRICTS AND OVERLAYS MAP



front activity, with commercial and mixed-use buildings located directly at the back of sidewalk and active storefront facades that add activity and interest along the street. Residential ground floors will be accessed from the sidewalk through lobbies or stoops and porches that provide direct access to each individual unit.

3. **Auto Commercial District.** The Auto Commercial District encompasses properties located at the eastern end of the Specific Plan Area that currently and historically accommodated auto dealerships. It includes the parcels located between Chatsworth Drive and Fox Street.

The Auto Commercial District will continue to serve as a center for auto sales and will also accommodate retail and office uses.

4. **Maclay District.** The Maclay District promotes the creation of new housing opportunities, while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. Examples of permitted uses include residential and commercial uses that are compatible with residential development.
5. **Workplace Flex District.** The Workplace Flex District is established for the purpose of providing a cohesive district that support the commercial and industrial uses of the city while providing appropriate areas for limited live-work and retail uses. The Workplace Flex District also provides a framework for creating a more inviting pedestrian, bicycle, and vehicular connection along First Street between the Metrolink Station and Maclay Avenue's "main street," the Civic Center, as well as along Truman Street between the Metrolink Station and Downtown.
6. **General Neighborhood District.** The General Neighborhood District is established to accommodate multi-family housing near Downtown and the Metrolink Station and to ensure that new housing along the south side of Second Street provides a suitable transition to the low-rise, single-family residential neighborhoods across the street to the north. New multi-family buildings facing Second Street will have two-story volumes at the front of the lot with massing that is in character with the single-family houses across the street. Higher massing – up to four stories – is allowed at the center and rear of the lot.

B. OVERLAYS

1. **Downtown Residential Overlay.** The Downtown Residential Overlay applies to select parcels within the Downtown District that are located near the San Fernando Mall. It increases maximum floor area ratio (FAR), permits an extra story of development, and allows upper floor residential uses, subject to a conditional use permit.
2. **Neighborhood Services Overlay.** The Neighborhood Services Overlay applies to parcels within the Maclay District located at and near the intersections of Maclay Avenue with Glenoaks Boulevard and with Eighth Street. It promotes the creation of mixed-use neighborhood centers with active, commercial ground floors.
3. **Flex-Use Overlay.** The Workplace Flex District accommodates the Flex-Use Overlay, which applies to the parcels designated Workplace Flex along the north side of Truman Street. It permits the light industrial uses allowed by the underlying Workplace Flex designation, while also allowing the residential, office, retail, and service uses allowed under the adjacent Mixed-Use Corridor designation.

4.4. LAND USE AND PERMIT REQUIREMENTS

- A. Table 4.1 (Land Use and Permit Requirements) identifies allowed uses and corresponding permit requirements for the San Fernando Corridor Specific Plan's six Districts. Definitions of the land uses can be found in Section 106-6 of the *San Fernando Zoning Ordinance* (Definitions).
- B. Additional permitted uses. Other uses not identified in Table 4.1, but deemed by the Community Development Director 1) to be similar to and compatible with the permitted uses identified in Table 4.1, and 2) to meet the purpose and intent of the district or overlay of this San Fernando Corridors Specific Plan, may be permitted subject to Community Development Director approval.
- C. Additional, non-residential conditionally permitted uses. Other non-residential uses not identified in Table 4.1, but deemed by the Community Development Director 1) to be similar to and compatible with the conditionally permitted uses identified in Table 4.1, and 2) to meet the purpose and intent of the district or overlay of this San Fernando Corridors Specific Plan, may be conditionally permitted

- subject to Community Development Director approval.
- D. Residential uses: All residential uses are subject to a conditional use permit and review by the Planning and Preservation Commission. Other residential uses not identified in Table 4.1, but deemed by the Community Development Director 1) to be similar to and compatible with the conditionally permitted uses identified in Table 4.1, and 2) to meet the purpose and intent of the district or overlay of this San Fernando Corridors Specific Plan, may be conditionally permitted subject to Planning Commission approval.
- E. Conditional uses shall be reviewed in terms of the location, design, configuration and impact of the proposed use, per *San Fernando City Code* Chapter 106 (Zoning), Article II Division 4 (Section 106-141 et seq.).
- F. Additional Requirements
- a. Alcoholic Sales:
 - i. Any sale of alcoholic beverages is subject to *San Fernando City Code* Chapter 106 (Zoning), Article II, Division 4, Subdivision II (Section 106-176 et seq.), except as otherwise provided in 2.4 (A).
 - ii. Alcoholic beverages may be served for on-site consumption ancillary to the operation of a sit-down restaurant, including micro-breweries, and sports bars, with table service that is a "bonafide public eating place" as that term is defined in *San Fernando City Code* Section 106-177.
 - b. Adult Uses: Adult uses are subject to limitations of *San Fernando City Code* Chapter 106 Article VI, Division 2 (Section 106-1021 et seq.) Adult theaters, massage parlors and modeling studios, adult motels or hotels, or other adult entertainment uses are not permitted.
 - c. Bed and breakfasts providing lodging and meals for guests are subject to a Conditional Use Permit, and provided:
 - i. The establishment is a private residence which is owner-occupied at all times.
 - ii. The establishment has no more than ten (10) guest rooms.
 - iii. The establishment serves food only to overnight guests.
 - iv. Overnight guests stay for no longer than seven (7) consecutive days.
 - v. The use is not located within 200 feet of a railroad right of way.
 - d. Live-Work and Home Occupations: Live-work and home occupations, where an occupation, hobby or profession may be conducted within a dwelling, are subject to a Conditional Use Permit and provided:
 - i. The residential use is the predominant use of the premises, and the commercial workplace activity is secondary. Permitted work activities shall be classified as a business and shall be subject to San Fernando City Code Chapter 106 (Zoning), Article VI, Division 9, (Section 106-1241 et seq.).
 - ii. Occupational activity is limited to a business office or a studio, including the making of arts and crafts or other occupational activity compatible with a residential use.
 - iii. Client visitation to a home occupation shall be by appointment only; walk-in trade is not permitted, except:
 - (a) Downtown District: Walk-in trade may be conditionally permitted by the chief planning official or designee.
 - iv. The maximum number of employees discounting the owner/occupant is limited to two.
 - e. Manufacturing: Manufacturing, assembling, repairing, testing, processing, packaging, warehousing, wholesaling, research or treatment of products may be conducted (other than those which may be obnoxious or offensive because of emission of odor, dust, smoke, gas, noise, vibration or other similar causes detrimental to the public health, safety or general welfare).
 - f. Outdoor Dining: Chairs and tables for outdoor dining that is accessory to an eating establishment is permitted in the public right-of-way (i.e., in sidewalk areas) provided that the business operator obtains a sidewalk encroachment permit from the City of San Fernando, and adheres to the following requirements:
 - i. The activity maintains a minimum five-foot wide sidewalk corridor which is clear and unimpeded for pedestrian traffic.
 - ii. The activity maintains a minimum five-foot wide clearance from the building entrance and all points of entry for building access.
 - iii. All outdoor furniture must be of commercial grade (i.e., manufactured for outdoor commercial use) with attractive, sturdy and durable materials. Tables should be no larger than two and one-half (2 ½) feet in any dimension.
 - iv. Other requirements specified by the chief public works official or designee.

TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS

Land Use	District					
	M	D	MUC	AC	WF	GN
Accessory Buildings and Structures						
Cabana Located on the Same Lot as the Principal Residential Use.	P	–	–	–	–	P
Garage	P	–	–	–	–	P
Recreation Room	P	–	–	–	–	P
Storage Shed	P	–	–	–	–	P
Workroom	P	–	–	–	–	P
Automobile and Vehicle Uses						
Automobile Sales and Services	–	–	–	P	CUP[1]	–
Automobile Rental Agencies	–	–	–	P	CUP	–
Gasoline Refueling Service Stations	CUP	–	CUP	P	CUP	–
Manufacturing and Light Industrial Uses: All uses permitted and conditionally permitted within the M-1 and M-2 Zones [2], including:						
Agriculture	–	–	P	–	P	–
Assembling	–	–	P	–	P	–
Assembly and Production Facilities	–	–	P	–	P	–
Maker Space	–	–	P	P	P	–
Manufacturing	–	–	P	–	P	–
Repairing	–	–	P	–	P	–
Research and Development	–	–	P	–	P	–
Testing	–	–	P	–	P	–
Warehousing	–	–	P	–	P	–
Wholesaling	–	–	P	–	P	–

KEY

P = use permitted by right
 CUP = use requires approval of conditional use permit
 – = use not permitted

M = Maclay District
D = Downtown District
MUC = Mixed-Use Corridor District
AC = Auto Commercial District
WF = Workplace Flex District
GN = General Neighborhood District

NOTES

- [1] Along Truman Street only: Auto repair activity is required to be conducted entirely within an enclosed building.
- [2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
- [3] A minimum of 60% of street frontage shall be occupied by business uses with retail-type shopfronts.
- [4] Use permitted only on parcels that face Hubbard Avenue
- [5] Use not permitted within 200 feet of railroad right-of-way.
- [6] Use permitted only within Downtown Residential Overlay and only on upper floors.
- [7] Lobbies providing entrance to upper floor Residential and Lodging facilities may occur on the ground floor.
- [8] Drive-up window allowed subject to a conditional use permits and provided it will not interfere with pedestrian traffic or service along public streets.
- [9] Commercial uses shall maintain hours of operation between seven o'clock (7:00) A.M. and eleven o'clock (11:00) P.M.
- [10] Within Neighborhood Service Overlay Areas: Use permitted only on ground floor.
- [11] Use allowed only along north side of Truman Street and south side of First Street.

TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Parking Structures and Facilities						
Privately-Owned	–	P[3]	P[3]	P	P	–
Publicly-Owned	–	P[3]	P[3]	P	P	–
Public Uses						
Civic and Cultural Facilities						
Libraries	–	P	–	–	–	–
Public Recreation Facilities	–	P	–	–	–	–
Museums	–	P	P	–	–	–
Art Galleries	P	P	P	–	–	–
Public and Institutional Uses						
Open Spaces, including Parks and Playgrounds	P	P	P	P	P	P
Public Health Services and Facilities	P	–	–	–	–	–
Public Assembly Uses						
Banquet Halls	–	CUP	CUP	–	–	–
Churches, Temples, or Other Places of Worship	CUP[4]	–	CUP	CUP	CUP	CUP[4]
Conference Facilities	–	CUP	CUP	–	–	–
Community Recreational Centers	CUP	–	CUP	–	–	P
Meeting Facilities	CUP	CUP	CUP	–	–	–
Movie Theaters	P	P	P	P	–	–
Public Clubs, Lodges, and Halls	CUP	CUP	–	–	–	–
Venues/Auditoriums for the Performing Arts	P	P	P	P	–	–

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TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Residential Uses						
Accessory dwelling unit [5]	–	–	–	–	–	–
Apartments [5]	CUP	CUP[6,7]	CUP	–	–	CUP
Community Care Facilities/Small [5]	P	P[6,7]	P	–	–	P
Community Care Facilities/Large [5]	CUP	CUP[6,7]	CUP	–	–	CUP
Condominiums [5]	CUP	CUP[6,7]	CUP	–	–	CUP
Duplexes [5]	–	–	–	–	–	P
Home Occupations in accordance with Division 9 of Article VI of the <i>San Fernando City Code</i>	P	P[6]	P	–	CUP	P
Live-Work	CUP	CUP[6,7]	CUP	–	CUP	–
Manufactured home as defined in <i>Health and Safety Code</i> Section 18007	–	–	–	–	–	–
Mobile Home Park	–	–	–	–	–	–
Primary single-family dwelling units, one per lot, in a permanent location	–	–	–	–	–	P
Single Room Occupancy	–	–	–	–	–	–
Supportive Housing [5]	CUP	CUP[6,7]	CUP	–	–	CUP
Townhouses [5]	CUP	–	CUP	–	–	CUP
Transitional Housing [5]	CUP	CUP[6,7]	CUP	–	–	CUP
Retail, Service, Entertainment, Lodging and Office Uses						
Business and Personal Service Shops						
Adult Businesses: Merchandise Sales	–	–	–	–	–	–
Banks, Credit Unions, Loan Companies, Title Companies	–	P	P[8]	P [8]	–	–
Barber & Beauty Shops	–	P	P	P	–	–

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TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Retail, Service, Entertainment, Lodging and Office Uses (continued)						
Dry Cleaning	P	P	P	P	–	–
Interior Decorating Studios	–	P	P	P	–	–
Laundromat	P	P	P	P	–	–
Market Hall, Food Hall	–	P	P	P	–	–
Nail Salons	–	P	P	P	–	–
Photocopy Shops	–	P	P	P	–	–
Repair Shops	–	–	P	P	–	–
Shoe Repair	–	P	P	P	–	–
Video Rental & Sales	P[9,10]	P	P	P	–	–
Entertainments Uses						
Billiards/Pool Parlors/Arcades	–	P	P	–	–	–
Bowling Alleys	–	P	P	P	–	–
Nightclubs	–	P	–	–	–	–
Skating/Skateboard Venues	–	–	CUP	–	–	–
Health and Exercise Clubs	P	P	P	–	–	–
Lodging						
Bed-and-Breakfasts	CUP	CUP[6,7]	CUP	–	–	–
Hotels, Motels	CUP	CUP[6,7]	CUP	CUP	–	–
Boardinghouses	–	–	CUP	–	–	–
Offices						
Administrative	P	P	P	P	–	–
Business	P	P	P	P	–	–
Design	P	P	P	P	–	–

KEY

- P = use permitted by right
 CUP = use requires approval of conditional use permit
 – = use not permitted

M = Maclay District

D = Downtown District

MUC = Mixed-Use Corridor District

AC = Auto Commercial District

WF = Workplace Flex District

GN = General Neighborhood District

NOTES

- [1] Along Truman Street only: Auto repair activity is required to be conducted entirely within an enclosed building.
- [2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
- [3] A minimum of 60% of street frontage shall be occupied by business uses with retail-type shopfronts.
- [4] Use permitted only on parcels that face Hubbard Avenue
- [5] Use not permitted within 200 feet of railroad right-of-way.
- [6] Use permitted only within Downtown Residential Overlay and only on upper floors.
- [7] Lobbies providing entrance to upper floor Residential and Lodging facilities may occur on the ground floor.
- [8] Drive-up window allowed subject to a conditional use permits and provided it will not interfere with pedestrian traffic or service along public streets.
- [9] Commercial uses shall maintain hours of operation between seven o'clock (7:00) A.M. and eleven o'clock (11:00) P.M.
- [10] Within Neighborhood Service Overlay Areas: Use permitted only on ground floor.
- [11] Use allowed only along north side of Truman Street and south side of First Street.

TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Retail, Service, Entertainment, Lodging and Office Uses (continued)						
Government	P	P	P	P	–	–
Incubator Business	–	P	P	P	P	–
Medical and Dental	P	P	P	P	–	–
Professional	P	P	P	P	–	–
Social, Cultural or Public Services, including Public and Non-Profit Organizations.	–	P	–	–	–	–
Restaurants						
Alcoholic Beverages for on-site consumption in conjunction with operation of a restaurant (up to Type 47 liquor license)	P	P	P	P	P[11]	–
Bar, tavern, cocktail lounge, (Type 48 liquor license)	CUP	CUP	CUP	CUP	–	–
Drive-up and/or Drive-in Restaurants, including Restaurants in a Single Free-Standing Building	–	–	–	CUP[8]	CUP	–
Full-Service Sit-Down Restaurants, including Walk-in Food Takeout Establishments	P[10]	P	P	–	P	–
Outdoor Dining, subject to encroachment permit	P	P	P	P	P	–
Retail Stores						
Antique Shops	–	P	P	P	–	–
Clothing Stores	–	P	P	P	–	–
Drug Stores and Pharmacies	P[10]	P	P[8]	P[8]	–	–
Electronic Equipment Stores	–	P	P	P	–	–
Florist Shops	–	P	P	P	–	–
Gift Shops	–	P	P	P	–	–
Grocery Stores	P[10]	P	P	P	–	–
Hardware Stores	–	P	P	P	–	–

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M = Maclay District**D** = Downtown District**MUC** = Mixed-Use Corridor District**AC** = Auto Commercial District**WF** = Workplace Flex District**GN** = General Neighborhood District**NOTES**

[1] Along Truman Street only: Auto repair activity is required to be conducted entirely within an enclosed building.

[2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.

[3] A minimum of 60% of street frontage shall be occupied by business uses with retail-type shopfronts.

[4] Use permitted only on parcels that face Hubbard Avenue

[5] Use not permitted within 200 feet of railroad right-of-way.

[6] Use permitted only within Downtown Residential Overlay and only on upper floors.

[7] Lobbies providing entrance to upper floor Residential and Lodging facilities may occur on the ground floor.

[8] Drive-up window allowed subject to a conditional use permits and provided it will not interfere with pedestrian traffic or service along public streets.

[9] Commercial uses shall maintain hours of operation between seven o'clock (7:00) A.M. and eleven o'clock (11:00) P.M.

[10] Within Neighborhood Service Overlay Areas: Use permitted only on ground floor.

[11] Use allowed only along north side of Truman Street and south side of First Street.

TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Retail, Service, Entertainment, Lodging and Office Uses (continued)						
Jewelry Stores	–	P	P	P	–	–
Music Stores	–	P	P	P	–	–
Pet Supply Stores	–	P	P	P	–	–
Photographic Equipment and Supply Stores	–	P	P	P	–	–
Shoe Stores	–	P	P	P	–	–
Specialty Foods	–	P	P	P	–	–
Sporting Goods Stores	–	P	P	P	–	–
Service Commercial						
Contractor Supply and Home Improvement Stores	P	–	P	P	P	–
Electrical Supply	–	–	P	P	P	–
Equipment Sales and Rentals	–	–	–	–	P	–
Film Laboratories	–	–	P	P	P	–
Home Furnishings, Hardware and Appliance Sales and Repair	P	P	P	P	P	–
Plumbing, Heating, Air Conditioning Equipment Supply and Repair	–	–	–	–	P	–
Plumbing Shops	–	–	P	P	P	–
Self Storage	–	–	–	–	–	–
Tool Sales and Rentals	–	–	–	–	P	–
Studios: Physical Activity and Instruction, including exercise and physical therapy, dance, martial arts, and similar activities.	P	P	P	P	P	–

KEY

<i>P</i>	=	use permitted by right
<i>CUP</i>	=	use requires approval of conditional use permit
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M	=	Maclay District
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MUC	=	Mixed-Use Corridor District
AC	=	Auto Commercial District
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NOTES

- [1] Along Truman Street only: Auto repair activity is required to be conducted entirely within an enclosed building.
- [2] *M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.*
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- [10] Within Neighborhood Service Overlay Areas: Use permitted only on ground floor.
- [11] Use allowed only along north side of Truman Street and south side of First Street.

TABLE 4.1 - LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

Land Use	District					
	M	D	MUC	AC	WF	GN
Retail, Service, Entertainment, Lodging and Office Uses (continued)						
Studios: Art and Design						
Artist Studio - all media	P	P	P	P	P	–
Design Professional Studio	P	P	P	P	P	–
Photography Studio	P	P	P	P	P	–
Studios: Movie and Television						
Post-Production Studios	P	–	P	P	P	–
Recording Studio	P	–	P	P	P	–
Television, Movie, and Media Arts Production Studio	P	–	P	P	P	–
Schools						
Business and Professional Practice	P	P	P	P	P	–
Nursery School/Day Care Facilities developed accordance with <i>San Fernando City Code</i> , Chapter 106 (Zoning), Article VI, Division 10 (Section 106-1271 <u>et seq.</u>	P	–	–	–	–	CUP
Performing and Fine Arts	P	P	P	P	P	–
Vocational Training for Trades	P	P	P	P	P	–

KEY

<i>P</i>	=	use permitted by right
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M	=	Maclay District
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NOTES

- [1] Along Truman Street only: Auto repair activity is required to be conducted entirely within an enclosed building.
- [2] *M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.*
- [3] A minimum of 60% of street frontage shall be occupied by business uses with retail-type shopfronts.
- [4] Use permitted only on parcels that face Hubbard Avenue
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- [9] Commercial uses shall maintain hours of operation between seven o'clock (7:00) A.M. and eleven o'clock (11:00) P.M.
- [10] Within Neighborhood Service Overlay Areas: Use permitted only on ground floor.
- [11] Use allowed only along north side of Truman Street and south side of First Street.

4.5. DEVELOPMENT STANDARDS

The development standards listed in Table 4.2 (Development Standards) are applicable to the San Fernando Corridors Specific Plan Districts. These standards, along with other development standards (e.g., landscaping requirements, signs, and parking standards) in this title, are intended to assist property owners and project designers in understanding the City's minimum requirements and expectations for high-quality development.

Most standards cover all development throughout the Downtown District. However, in some instances where special standards are necessary to maintain or create a particular character for a certain area, standards may vary by sub-district. These variations are marked by an asterisk (*).

A. MODIFICATIONS TO DEVELOPMENT STANDARDS.

Upon application by a property owner, modifications to the development standards in this Chapter may be granted as provided within *San Fernando City Code* Chapter 106 (Zoning), Article II, Division 5, Section 106-212.

B. SETBACKS.

1. **Non-residential ground floors.** All non-residential ground-floor uses are required to be built to the front property line or the back of side-walk. However, a portion of the building frontage may be recessed to provide for courtyards, forecourts, entry plazas or similar features, subject to the following:

- Courtyard recess is enclosed by buildings on three sides, with storefront entrances and windows fronting onto the courtyard.
- Courtyard recess extends no longer than 60' along the front property line.
- Courtyard recess extends no deeper than 25' from the front property line.

C. FRONTAGE

1. **Required shopfronts.** Ground floor shopfronts are required along the following street-facing frontages per Section 5.2 of the Design Guidelines (Chapter 5):

- Along the San Fernando Mall between San Fernando Mission Boulevard and Brand Boulevard.



A mixed-use building with inset windows and decorative balconies.



A prominent tower marks the corner of this mixed-use building.



A mixed-use building with a third-story covered balcony and second floor French balconies.

TABLE 4.2 - DEVELOPMENT STANDARDS

Standard	District					
	M	D	MUC	AC	WF	GN
A. Development Intensity						
1. Floor Area Ratio (max.) [1]						
a. Non-residential	1.0	3.0	2.0	3.0	2.0	–
b. Residential Mixed-use	1.5[2]	3.5[3]	2.5	–	2.0	–
2. Residential Density (dwelling units/acre)						
a. Minimum	12	24[3]	24	–	–	–
b. Maximum	37	50[3]	37	–	18	43
B. Height (see Figure 4.2)						
1. Building height as measured from sidewalk or finished grade to top of flat roof, cornice, or eave line of a peaked roof.						
a. Primary Building						
i. Minimum (stories / ft.)	–	1 / 24	1 / 24	–	–	–
ii. Maximum (stories / ft.)	3 / 40	3 / 40	3 / 40	3 / 40	3 / 40	4 / 45
iii. Maximum within Downtown Residential Overlay (stories / ft.)	–	4 / 50	–	–	–	–
iv. Maximum height adjacent to R-1 Single Family Residential Zone within a distance of 15 ft. from the property line (ft.)	No facade wall shall extend more than 10 feet above the height of the adjacent single family facade wall					
v. Maximum height along south side of Second Street between Hubbard Avenue and Maclay Avenue for a distance of 20 ft. behind the Second Street setback line (stories / ft.)	–	–	–	–	–	2 / 24[4]
vi. Maximum height along north side of Celis Street between Huntington Street and Kalisher Street for a distance of 10 ft. behind the Celis Street setback line (stories / ft.)	–	–	2 / 24[4]	–	–	–
b. Accessory Building, including non-dwelling units, such as free-standing individual car garages, service structures and tool sheds (ft.)	12	–	12	–	–	12
2. Ground floor height as measured above grade at building setback line (max.)						
a. Residential	4 ft.	4 ft.	4 ft.	4 ft.	4 ft.	4 ft.
b. Non-residential	–	–	–	–	–	–
3. Ground story height as measured from floor to floor						
a. Residential (ft.)	–	–	–	–	–	–
b. Non-residential (ft.)	14 min.	18 min.	18 min.	14 min.	14 min.	10 min.

NOTES

[1] Floor Area Ratio (FAR) defined as the floor area of the building divided by the total project site area. FAR calculations do not including parking facilities

[2] Only within Neighborhood Services Overlay.

[3] Only within Downtown Residential Overlay.

[4] Attic spaces of one- and two-story buildings with pitched roofs may be occupied and day lit with dormer windows.

[5] New surface parking lots may not front onto Maclay Avenue or San Fernando Road.

[6] Side Setbacks may be allowed to provide for driveways and pedestrian pathways, to a maximum of 12 ft.

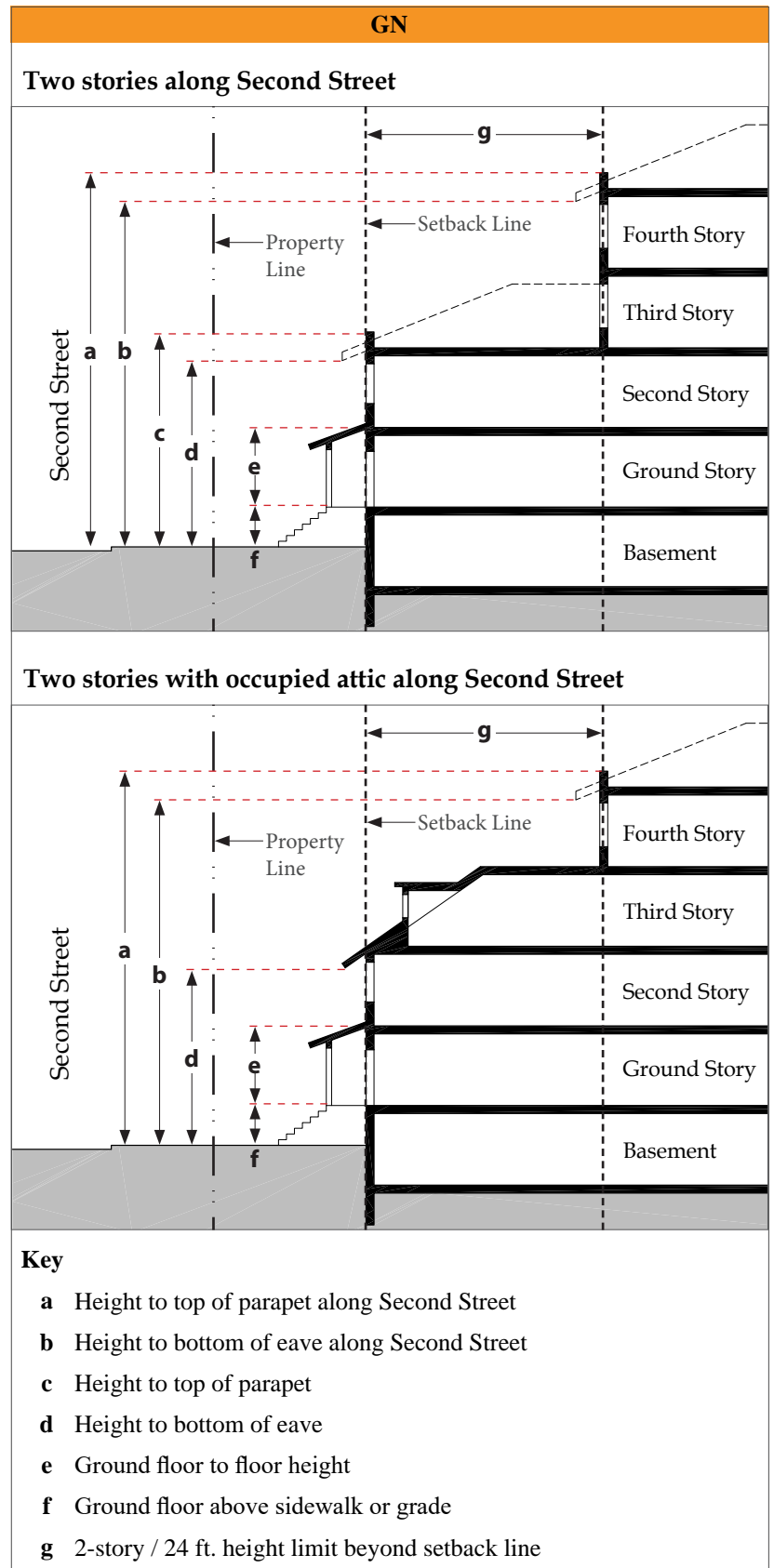
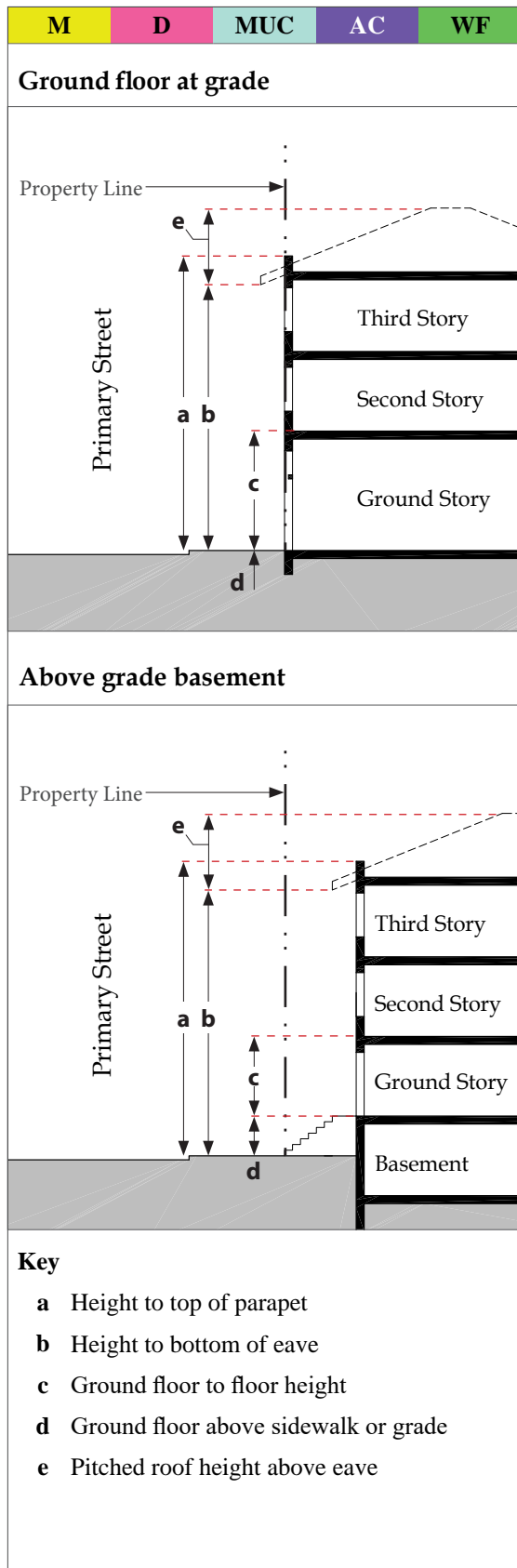
FIGURE 4.2 - BUILDING HEIGHT (SEE TABLE 4.2, SECTION B)

TABLE 4.2 - DEVELOPMENT STANDARDS (CONTINUED)

Standard	District					
	M	D	MUC	AC	WF	GN
4. Special architectural features, such as uninhabited towers (clock, bell, observation) or entry volumes	10 ft. max. above maximum building height					
5. Rooftop structures, such as elevator and mechanical equipment enclosures or roof deck trellises and gazebos	10 ft. max. above maximum height limit, provided structures are set back a minimum of ten (10) feet from building walls and are screened on all sides by a parapet or sloped roof that is architecturally integrated within the building design.					
C. Setbacks						
1. Primary Street Setback						
a. Non-Residential (ft.)	0 [2]	0	0	0	6 min.	–
b. Residential (ft.)	15	5 min.	10 min.	–	10 min.	20
c. At-Grade Parking Lot	15 min.	5 min.[5]	6 min.	6 min.	6 min.	–
d. Freestanding Parking Structure	15 min.	0	6 min.	6 min.	6 min.	–
2. Side Street Setback						
a. Non-Residential (ft.)	0 [2]	0	0	0	0	–
b. Residential (ft.)	5 min,	5 min.	5 min.	–	5 min,	20
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	5 min.	5 min.	–
d. Freestanding Parking Structure	5 min.	0	5 min.	5 min.	5 min.	–
3. Side Yard Setback						
a. Non-Residential (ft.)	5 min,	0 [6]	0 [6]	0 min. / 15 max.	0	–
b. Residential (ft.)	0 [2]	0 [6]	5 min. / 15 max.	–	5 min. / 15 max.	–
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	5 min.	5 min.	–
d. Freestanding Parking Structure	5 min.	0	5 min.	5 min.	5 min.	–
4. Rear Setback						
a. With alley, as measured from property line (ft.)	5	0	0	0	0	10
b. Without alley (ft.)	15	0	0	0	10	20
c. At-Grade Parking Lot	6	6	6	6	6	6
d. Freestanding Parking Structure	6	6	6	6	6	–

NOTES

[1] Floor Area Ratio (FAR) defined as the floor area of the building divided by the total project site area. FAR calculations do not including parking facilities

[2] Only within Neighborhood Services Overlay.

[3] Only within Downtown Residential Overlay .

[4] Attic spaces of one- and two-story buildings with pitched roofs may be occupied and day lit with dormer windows.

[5] New surface parking lots may not front onto Maclay Avenue or San Fernando Road.

[6] Side Setbacks may be allowed to provide for driveways and pedestrian pathways, to a maximum of 12 ft.

- b. Along San Fernando Road within the Mixed-Use Corridor District: new buildings with non-residential ground floors only.
- c. Along Hubbard Avenue between the railroad right-of-way and San Fernando Road.
- d. Within the Neighborhood Serving Overlay.

2. **Recommended Frontage Types.** All street-facing facades should provide at least one (1) of the frontage types listed in Table 5.1 of the Design Guidelines (see Chapter 5).

D. DRIVEWAY AND SERVICE ACCESS.

1. **Downtown District Driveway Access.** Driveway access must be located along streets other than Maclay Avenue or San Fernando Road wherever possible (i.e. from side streets or rear alleys). Where only front access is available, driveways should be constructed according to the Standards in below Subsections "B".
2. **Curb Cuts.**
 - e. The maximum number of curb cuts associated with a single building is one (1) two-way curb cut or two (2) one-way curb cuts. Where applicable, the maximum number of curb cuts is one (1) two-way curb cut or two (2) one-way curb cuts per one hundred fifty (150) feet of street frontage.
 - f. The maximum width of curb cuts is twelve (12) feet for one-way and twenty (20) feet for two-way driveways.
3. **Service Access.**
 - a. Downtown District: Service access must be from side streets, rear alleys and rear parking areas, wherever possible.
 - b. Maclay District: vehicular service access must be from alleys and rear parking areas.
 - c. Workplace Flex District: vehicular service access must be from alleys and rear parking areas wherever possible.

E. OPEN SPACE.*

1. **All Districts.**
 - a. Residential Developments: Outdoor space shall be provided as follows:
 - i. A minimum of one hundred fifty (150) square feet of usable publicly accessible open space. Open space provision shall not include required setback areas. Com-

mon open spaces for residential uses must be constructed on-site. Publicly accessible open space may be constructed on- or off-site. (See the *Design Guidelines for Site Improvements, Furnishings, Landscape and Lighting* for design of open space.)

- ii. A minimum of sixty (60) square feet of private open space per residential unit. Patios, porches, balconies, terraces, and decks may be used to provide private space within multifamily structures, at a minimum dimension of six (6) feet in any one direction. Private areas should be adequately separated to ensure the privacy of the units.
 - b. For all developments with common open space or other common interest facilities, the developer shall record binding agreements ("CC&R's") addressing issues of common interest regarding use, access and maintenance of common open space, tree planter areas, planting strips, walkways and parking and/or vehicular use areas.
2. **Downtown District.**
 - a. Commercial and Office Development: Developments of greater than 30,000 square feet shall provide a minimum of one hundred (100) square feet of publicly accessible open space for every 2000 square feet of ground floor retail space constructed, and a minimum of one hundred (100) square feet of publicly accessible open space for every 1000 square feet of office space constructed. Open space provision shall not include required setback areas. Open space may be constructed on- or off-site, or be satisfied through payment of an inlieu fee to fund the construction of public open space in the Downtown District. (See the Design Guidelines for Site Improvements, Furnishings, Landscape and Lighting for design of open space, including front setback areas.)
 - b. For Mixed Use Developments, publicly accessible open space provided will count towards the minimum public open space requirements for all uses.
 - c. All open spaces shall be publicly accessible during daylight hours, and shall be designed to connect with public rights-of-way and adjacent public open spaces in the vicinity.
 - d. Residential Developments: see below Subsection "A".

F. LANDSCAPING AND SCREENING.

1. Landscaping in setback areas.

- a. For all buildings, front setback areas within 12 feet of face of curb shall be hardscaped to match adjacent sidewalk.
- b. For buildings with ground floor residential uses, setback areas 12 feet or more from the face of curb, shall be landscaped with the installation of shrubs, ground cover, and trees, over at least 50% of the front setback area, exclusive of driveways.

2. Parking lot landscaping. Minimum five (5) feet wide planting area must be established at the perimeter of parking lots and driveways within the required setback area. Where parking lots are sited adjacent to or backing onto residential buildings, the parking lot should also be screened with an attractive screen fence or low wall, and planted with ground cover and trees adjacent to the screening fence or wall at a maximum spacing of twenty (20) feet on center.

4. Utilities and services screening. Utility, trash, recycling, food waste and service equipment, including satellite receiving dishes, must be located away from streets and enclosed or screened by landscaping, fencing or other architectural means. Trash facilities and recycling containers must always be within structural enclosures.

5. Rooftop Equipment screening. Rooftop equipment must be screened from view and architecturally integrated into the building design.

G. LIGHTING.

1. Exterior area lighting. All exterior area lighting shall be provided by full cut-off fixtures (where no light is emitted above the horizontal plane) with the light source fully shielded or recessed to preclude light trespass or pollution on adjacent or abutting property or up into the night sky.
2. Lighting adjacent to residential uses. All exterior area lighting adjacent to residential uses shall be located and designed to prevent light spill into residential units.

3. Luminaires. Freestanding luminaires shall be mounted no higher than eighteen (18) feet, measured from the finished grade. Building-mounted luminaires shall be attached to walls or soffits (the undersides of ceilings or overhangs), and the top of the fixture shall not exceed the height of the parapet or roof, whichever is greater. (Please refer to the *Design Guidelines for Site Improvements, Furnishings, Landscape and Lighting* for lighting design.)
4. Up-lighting. All decorative up-lighting, such as those illuminating building facades or landscaping, shall be operated on timers that turn off illumination after 12 midnight nightly.

H. UTILITY EASEMENTS.

1. Public utility easements. All public utility easements must be provided under or immediately adjacent to new public rights-of-way, or within other public easement areas acceptable to the chief public works official.
2. On-site utilities. All on-site utilities shall be placed underground unless specified otherwise by the chief public works official.

4.6. SIGNAGE STANDARDS*

1. Permitted Sign Types. All permanent signs are subject to design review, as per *San Fernando City Code Chapter 106 (Zoning), Article V, Division 5, Section 106-927*. A sign permit shall be required prior to the placing, erecting, moving, reconstructing, altering or displaying of any sign within the district.
 - a. Building-Mounted Signs, including wall signs and projecting signs are permitted as follows:
 - i. Individual lettering or characters or logos on signs may not exceed:
 - (a) Downtown District: three (3) feet in height.
 - (b) Maclay District: twelve (12) inches in height.
 - (c) Maclay district Neighborhood Service Overlay: two (2) feet in height.
 - (d) Workplace Flex District: three (3) feet in height.
 - ii. Wall signs:
 - (a) Downtown and Maclay Districts: wall signs should be located above the storefront of the building, in the sign band or on other useable wall area below the

- sign band. Wall signs may not project more than four inches from a building, and may not extend above the roofline or parapet wall of the building.
- (b) Workplace Flex District: Wall signs may not project more than four inches from a building, and may not extend above the roofline or parapet wall of the building.
 - iii. Projecting signs must be placed at minimum ten (10) feet above the ground, and must not project more than four (4) feet from the building face. They may not extend above the top of the storefront cornice or parapet, unless approved by the chief planning official in conjunction with a sign plan for the building as a whole that is determined to be complimentary to the building's design.
 - b. Awning and canopy signs are permitted.
 - i. Sign copy (letters and graphics) on awnings is limited to the front valence of the awning, and must consist of no more than one line of lettering. Individual lettering or characters or logotypes comprising this line may not exceed twelve (12) inches in height.
 - ii. Awnings must generally be centered over the entrance or storefront, and located a minimum 10 feet above the ground. Awnings and canopies should not obscure transom or clerestory windows.
 - iii. Window Signs are permitted.
 - (a) All Districts: The combination of all window signs, including both primary and temporary window signs may not cover more than twenty-five percent (25%) of the total window area.
 - (b) Downtown Districts: Individual letters on windows may not exceed twelve (12) inches in height.
 - c. Free standing signs and pole signs are not permitted. Exceptions include:
 - i. Downtown District and Workplace Flex District:
 - (a) Directory signs or kiosks, to a maximum height of four (4) feet, and a maximum area of thirty (30) square feet. These may be considered for sidewalk locations; those for private arcades or building complexes should be on private property, located in publicly accessible courts, access ways or passages. Proposed locations are subject to design review for pedestrian and ADA clearance and conformance with street and sidewalk character.
 - (b) Signs attached to architectural elements such as archways, trellises, and entry piers are permitted only for addresses, project identity signs, or directories.
 - (c) Parking Entry and Incidental Traffic Control Signs.
 - ii. Downtown District
 - (a) Portable signs for restaurants only, i.e. sandwich and menu boards for restaurants only, provided they are stored indoors after hours of operation.
 - d. Roof-top mounted signs are not permitted.
 - e. Temporary banner signs shall not exceed a maximum area of thirty (30) square feet, and shall be limited to the width of the storefront for the business displaying the banner sign. No more than one banner sign is permitted per street frontage per business, unless otherwise approved by the chief planning official.
 - f. Identification signs: Identification signs for residential uses (i.e. those identifying multiple dwellings or roominghouses) are permitted on residential buildings provided:
 - i. The sign indicates only the name and address of the premises.
 - ii. The sign does not exceed six square feet in area and four feet in any dimension.
 - g. Signs that are developed as part of a themed sign program for an overall development may be considered under the provisions of a Planned Sign Program.
 - h. To allow for creative signing solutions, the Community Development Director may issue a permit for a sign that is not specifically permitted or prohibited by this specific plan. Examples of such signs may include a vertically oriented sign for a movie theater marquee or an existing projecting sign that extends above the height of the parapet.

2. Sign Area.

- a. For primary building frontage, the sum total area for any combination of permitted sign types for both ground floor and upper floor uses is one (1) square foot per one (1) linear foot of ground-floor tenant street frontage, not to exceed:

- i. Downtown District:
 - (a) 100 square feet of total sign area, or 50 square feet in any single sign face display, including both ground floor and upper story uses.
 - (b) San Fernando Mall (San Fernando Road between Brand Boulevard and San Fernando Mission Boulevard): two (2) square foot per one (1) linear foot of ground-floor ten-ant street frontage, not to exceed 120 square feet of total sign area.
- ii. Maclay District: eighty (80) square feet of total sign area for any single business or occupancy.
- iii. Workplace Flex District: 120 square feet of total sign area or 100 square feet in any single sign face display.
- b. For secondary building frontage, the allowable sign area is:
 - i. Downtown District:
 - (a) One-half (0.5) square foot per one (1) linear foot of tenant street frontage, not to exceed 50 square feet of total sign area.
 - (b) San Fernando Mall (San Fernando Road between Brand Boulevard and San Fernando Mission Boulevard): one (1) square foot per one (1) linear foot of tenant street frontage, not to exceed 50 square feet of total sign area.
 - ii. Maclay District: one (1) square foot per one (1) linear foot of tenant street frontage, not to exceed forty (40) square feet of total sign area.
 - iii. Workplace Flex District: one-half (0.5) square foot per one (1) linear foot of tenant street frontage, not to exceed fifty (50) square feet of total sign area.
 - iv. All Districts: Any signs facing abutting residentially zoned property shall have no internal illumination, and any spotlights or other sources of illumination shall be shielded to prevent glare.

3. Sign Content.

- a. Signs displayed pursuant to this section shall refer only to businesses or occupants located on the premises where the signage is located and only to products and/or services available on the premises.

- b. Each business or building occupant with exterior sign display shall include within its sign content the name of the business or occupant in letters of the roman alphabet that are at least six inches in height, and that are legible to the public and to emergency service responders.
- c. All signs pertaining to the sale of alcoholic beverages or to the sale of tobacco products shall comply with San Fernando City Code, Chapter 106, Article V, Division 5, Sections 106-940, and 106-941.

4.7. VEHICULAR PARKING STANDARDS.

1. Required number of parking spaces.

- a. The minimum number of parking spaces required to be supplied for each category of use shall be provided as indicated in Table 4.3. For some uses, a maximum number of parking spaces is also indicated, in order to promote the efficient use of land and to provide a better pedestrian environment in the district. Parking requirements for building renovation, enlargement or use change apply only to net new floor area and/or the incremental increase in parking required for a new use with a higher parking requirement for a given floor area.
- b. Requirements may be satisfied either on-site, on-street along adjacent public street frontages, by constructing or purchasing spaces in off-site parking structures located within one-half (1/2) mile of the subject property, and/or by payment of an in-lieu parking fee to fund shared public parking. Curbside parking directly in front of a parcel, including partial spaces where at least seventy-five percent (75%) of their length lies directly in front of a parcel, may count towards minimum parking requirements for that site.
- c. Unless otherwise provided for in the *San Fernando City Code*, the Planning and Preservation Commission may grant a reduction in off-street parking requirements for shared parking upon granting of a Conditional Use Permit (CUP). A CUP will be granted provided the applicant demonstrates that the uses have differing peak hours of parking demand, or that the total parking demand at any one time would be adequately served by the total number of parking spaces provided.
- d. Shared parking is permitted, especially where nearby uses generate parking demands during different hours. Shared parking will be ap-

proved provided the area where the sharing occurs is not heavily impacted by a parking shortage as determined by a parking study prepared and updated periodically for the city parking authority and provided:

- i. A shared parking agreement is developed between property owners and the agreement is approved by the planning department for review prior to recording the agreement with the county recorder; and
 - ii. A conformed copy of the recorded shared parking agreement is transmitted to the planning director prior to issuance of a building permit.
- e. **Mixed-use.** When there are two or more different uses located on the same lot or within the same building, the minimum number of parking spaces required shall equal the sum of requirements, including fractional amounts, for each use, unless shared parking is possible.
- f. **Residential.**
- i. Required parking for dwelling units must be provided on-site.
 - ii. Guest parking may be provided off site through payment of an in-lieu fee.
 - iii. Requirements for residential parking may be satisfied by payment of an in-lieu parking fee only if a Conditional Use Permit (CUP) is granted for that purpose.

2. Off-Street Parking Lots and Structures.

- a. Parking lots and structures should be located at the rear or at the side of buildings.
- b. **Downtown District:** New surface parking lots may not front Truman Street, San Fernando Road, Maclay Avenue, Brand Boulevard, or San Fernando Mission Boulevard. New surface parking lots may front onto streets other than those listed above no more than fifty percent (50%) of the width of the parcel.
- c. **Mixed-Use Corridor District.** New surface parking lots may not front San Fernando Road or San Fernando Mission Boulevard. New surface parking lots may front onto streets other than those listed above no more than fifty percent (50%) of the width of the parcel.
- d. **Maclay District:** Surface parking lots may front onto Maclay Avenue for no more than thirty percent (30%) of the width of the parcel. Parking lots and structures may not be located on street corners, and should be located at the rear or at the side of buildings where possible.

- e. **Workplace Flex District:** Surface parking lots may front onto Truman Street or First Street for no more than fifty percent (50%) of the width of the parcel. Parking lots and structures may not be located on corner parcels or be adjacent to parks, courtyards, or plazas, and should be located at the rear or at the side of buildings where possible.
- f. **Design:** The layout and design of parking lots and areas, including access to required parking spaces, turning radii, angle of parking and aisle width shall be as set forth in parking lot design standards adopted in accordance with *San Fernando City Code* Chapter 106 (Zoning) Article V, Division 3, Subdivision III, Section 106-868.
 - i. The perimeter of parking areas and driveways must be landscaped as described herein above in 6.3 Landscaping & Screening.
 - ii. Surface parking areas must be planted with shade trees at a ratio of at least one (1) tree for every four (4) spaces. They must also meet the landscape requirements in accordance with San Fernando City Code Chapter 106 (Zoning), Article V, Division 3, Subdivision II, Section 106-833, and lighted in accordance with Code Section 106-834.

3. Bicycle Parking Requirements.

- a. For all uses, there shall be one (1) off-street bicycle parking space per ten (10) automobile parking spaces as required above.
- b. Off-street bicycle rack facilities for separate uses may be provided collectively if the total number of spaces provided collectively is not less than the sum of the separate requirements for each such use and provided that all regulations governing location of accessory parking spaces in relation to the use served are adhered to.

TABLE 4.3 - VEHICULAR PARKING REQUIREMENTS

Land Use	Off-Street Parking Required
Accessory Buildings and Structures	
Cabana	None required
Garage	
Recreation Room	
Storage Shed	
Workroom	
Automobile and Vehicle Uses	
Automobile Sales and Services	3.3 spaces per 1,000 sf
Automobile Rental Agencies	3.3 spaces 1,000 sf
Gasoline Refueling Service Stations	–
Manufacturing and Light Industrial Uses	
Assembling	3.3 spaces 1,000 sf
Assembly and Production Facilities	
Manufacturing	
Repairing	
Research and Development	
Testing	
Warehousing	
Wholesaling	
Parking Structures and Facilities	
Privately-Owned	None required
Publicly-Owned	None required
Public Uses	
Civic and Cultural Facilities	2.5 spaces per 1,000 sf
Libraries	
Public Recreation Facilities	
Museums	
Art Galleries	
Public and Institutional Uses	1 spaces per 10,000 sf of net land area
Open Spaces, including Parks and Playgrounds (min. spaces / 10,000 sf net land area)	
Public Health Services and Facilities	

TABLE 4.3 - VEHICULAR PARKING REQUIREMENTS (CONTINUED)

Land Use	Off-Street Parking Required
Public Assembly Uses	
Banquet Halls	1 space per 5 fixed seats, or 20 spaces per 1,000 sf
Churches	1 space per 7 seats, or 1 space per 10-1/2 linear feet of pew
Conference Facilities	1 space per 5 fixed seats, or 20 spaces per 1,000 sf
Community Recreational Centers	
Meeting Facilities	
Movie Theaters	
Meeting Facilities	
Public Clubs, Lodges, and Halls	
Venues/Auditoriums for the Performing Arts	
Residential Uses	
Community Care Facilities	To be determined for each conditional use permit based primarily upon the facility’s licensed capacity, type of care and number of employees
Multi-Family (Apartments, Condominiums, Duplexes, Live-Work, Townhouses)	
Studio and one-bedroom unit	1 space per unit
Two-bedroom unit or larger	2 spaces unit
Guest	0.2 space per unit
Primary single-family dwelling units	2 space per unit
Second dwelling units	None required
Supportive Housing	To be determined for each conditional use permit based primarily upon the facility’s licensed capacity, type of care and number of employees
Transitional Housing	
Neighborhood Services Overlay Areas	
General Offices	3.3 spaces per 1,000 sf
Retail	1.6 spaces per 1,000 sf min.; 3.3 spaces per 1,000 sf max.
Eating Establishment Uses	

TABLE 4.3 - VEHICULAR PARKING REQUIREMENTS (CONTINUED)

Land Use	Off-Street Parking Required
Retail, Service, Entertainment, Lodging and Office Uses	
Business and Personal Service Shops	2.5 spaces per 1,000 sf
Adult Businesses: Merchandise Sales	
Banks, Credit Unions, Loan Companies, Title Companies	
Barber & Beauty Shops	
Dry Cleaning	
Interior Decorating Studios	
Laundromat	
Nail Salons	
Photocopy Shops	
Repair Shops	
Shoe Repair	
Video Rental & Sales	
Entertainments Uses	3.3 spaces per 1,000 sf
Billiards/Pool Parlors	
Bowling Alleys	
Nightclubs	
Skating/Skateboard Venues	
Health and Exercise Clubs (/ 1,000 sf)	5 spaces per 1,000 sf min; 10 spaces per 1,000 sf max
Lodging	1.125 spaces per unit min. (one space for each living or sleeping unit, plus one space for each 10 such units)
Bed-and-Breakfasts (min.)	
Hotels, Motels (min.)	
Rooming and Boardinghouses	
Offices	2.5/1,000 sf min.; 5/1,000 sf max
Administrative	
Professional	
Government	
Business	
Social, Cultural or Public Services, including Public and Non-Profit Organizations.	5/1,000 sf min.; 10/1,000 sf max.
Medical and Dental	
Business	2.5/1,000 sf min.; 5/1,000 sf max

TABLE 4.3 - VEHICULAR PARKING REQUIREMENTS (CONTINUED)

Land Use	Off-Street Parking Required
Restaurants	
Alcoholic Beverages for on-site consumption in conjunction with operation of a restaurant (Type 47 liquor license Type 47 only)	3.3/1,000 sf min.; 16.5/1,000 sf max.
Bar, tavern, cocktail lounge, (Type 48 liquor license)	
Drive-up and/or Drive-in Restaurants, including Restaurants in a Single Free-Standing Building	
Full-Service Sit-Down Restaurants, including Walk-in Food Takeout Establishments	
Outdoor Dining	
Retail Stores	
Antique Shops	3.3 spaces per 1,000 sf min.; 6.6spaces per 1,000 sf max.
Clothing Stores	
Drug Stores and Pharmacies	
Electronic Equipment Stores	
Florist Shops	
Gift Shops	
Grocery Stores	
Hardware Stores	
Jewelry Stores	
Music Stores	
Pet Supply Stores	
Photographic Equipment and Supply Stores	
Shoe Stores	
Specialty Foods	
Sporting Goods Stores	
Service Commercial	
Contractor Supply and Home Improvement Stores	5 spaces per 1,000 sf
Electrical Supply	
Equipment Sales and Rentals	
Film Laboratories	
Home Furnishings, Hardware and Appliance Sales and Repair	
Plumbing, Heating, Air Conditioning Equipment Supply and Repair	
Plumbing Shops	
Tool Sales and Rentals	
Studios for Physical Activity and Instruction, including exercise and physical therapy, dance, martial arts, and similar activities (min/1,000 sf)	2 spaces per 1,000 sf

TABLE 4.3 - VEHICULAR PARKING REQUIREMENTS (CONTINUED)

Land Use	Off-Street Parking Required
Studios: Art and Design	
Artist Studio - all media	2 spaces per 1,000 sf
Photography Studio	
Studios: Movie and Television	
Post-Production Studios	2 spaces per 1,000 sf
Recording Studio	
Television, Movie, and Media Arts Production Studio	
Schools	
Business and Professional Practice	8 spaces per 1,000 sf
Nursery School/Day Care Facilities	Determined for each conditional use permit based primarily upon the facility’s licensed capacity, type of care and number of employees
Performing and Fine Arts	8 spaces per 1,000 sf
Vocational Training for Trades	8 spaces per 1,000 sf

4.8. NOISE

- Maximum Noise Levels.** Sounds generated from all sources within the district shall be subject to the limitations specified in the *San Fernando City Code*, Chapter 34, Article II (Noise),(Section 34-26, et seq.).

4.9. MUNICIPAL CODE STANDARDS

- Applicable Regulations.** The development and occupancy of property in the Downtown District shall be subject to the provisions and procedures of the *San Fernando City Code*, except that the permitted and conditional uses and the development standards for the Downtown District as specified herein above shall supersede any conflicting regulation of the municipal code.

CHAPTER FIVE: DESIGN GUIDELINES



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5.1 PURPOSE

The design guidelines in this chapter provide direction for the design of buildings, appurtenances and site elements. The materials, methods, and forms herein are recommended. Alternative forms may be permitted when approved in writing by the Community Development Director, based on a finding that they conform to the design intent of this Specific Plan or are otherwise required by law. The design guidelines in Sections 5.3 - 5.6 are organized by *city district*, to insure that the development, activities and visual character of each district work together to create a cohesive identity (see Figure 4.1 in Chapter 4).

5.2 FRONTAGE TYPES

Frontages are critical to defining district character, both in appearance and function. Frontages are comprised of the street facade of a building, including any projecting elements, and the landscape, hardscape, walls and fences of the front yard where present. Frontages provide an appropriate transition from the public environment of the street to the semi-private and private environments of street-facing ground floor spaces and front yards. They also signal the location of the building entrance and provide a semi-public space within which patrons, neighbors and visitors can interact. Frontages can also help to screen any on-site parking areas.

All buildings should provide at least one street-facing primary pedestrian entry and street-facing windows on all floors as shown in Table 5.1 below. The Frontage Types herein may modify the configuration of those doors and windows but should not replace them.

TABLE 5.1. RECOMMENDED FRONTAGE TYPES

Standard	District and Sub-District					
	M	D	MUC	AC	WF	GN
A. Recommended Frontage Types						
All street-facing facades should provide at least one (1) of the frontage types listed below.						
1. Arcade	–	P	P	P	–	–
2. Gallery	–	P	P	P	–	–
3. Shopfront	–	P	P	P	P	–
4. Stoop	–	P	P	–	P	P
5. Porch	P	–	–	–	–	P
6. Dooryard	–	P	P	–	P	P
B. Permitted Encroachments						
1. Encroachments into Public Right-of-Way (ft.)						
a. Canopies and awnings, provided they allow for eight feet clear height above sidewalk grade (ft.)	6 max.	6 max.	6 max.	0	0	0
2. Encroachments into Primary Street and Side Street Setback (ft.)						
a. Arcade, Gallery	0	0	0	0	0	
b. Porch, Stoop	5 max.	5 max.	5 max.	5 max.	0	
c. Door Yard	1 ft. from property line					
d. Upper Floor Balconies, Bay Windows	5 max.	5 max.	5 max.	5 max.	5 max.	
e. Exterior Stairs	5 max.	5 max.	5 max.	5 max.	0	

A. SHOPFRONT

1. **Description.** Shopfronts are large openings in the facade at or near the sidewalk, enclosed with doors and transparent glass in a storefront assembly. The primary shop entrance is at the grade of the sidewalk and provides direct access to the commercial/retail use(s) on the ground floor. The basic required architectural elements comprising the storefront are large windows, doors with glass, transom windows, and a solid base (bulkhead). Optional elements include awnings, cantilevered shed roof or canopy, signage, lighting, and cornices.

Awnings or canopies may encroach into the public right-of-way over the sidewalk, extending to a distance within two feet of the face of curb. Primary Street and Side Street setbacks, if any, are to be paved with a paving material that is consistent with or matches the adjacent sidewalk.

2. **Design Standards.**

- a. Storefront assemblies (doors, display windows, bulkheads, and associated framing) should not be set back within the Shopfront openings more than 2 ft.
- b. Doors should match the materials, design, and character of the display window framing. "Narrowline" aluminum doors are prohibited.
- c. Display windows:
 - i. Storefront(s) opening(s) along the primary frontage should comprise at least 70 percent of the ground floor wall area.
 - ii. Walls without openings should not exceed 10 linear feet along Primary Street frontages and 25 linear feet along Side Street frontages.
 - iii. Storefront glass that is clear, lightly tinted (e.g., less than 15%, low emissivity, solar) without reflective coating or dark tinting is encouraged. Instead, frontage types such as arcades and galleries and architectural elements such as awnings and canopies are encouraged to shade shopfront openings.
 - iv. Transom windows (horizontal glass panels immediately above the storefront) are encouraged. Glass in clerestory windows may be clear, stained glass, or frosted glass.

FIGURE 5.1. SHOPFRONT DIMENSIONS

Frontage Element	Min.	Max.
a¹ Height to top of transom (clear)	10 ft.	16 ft.
a² Height to bottom of awning/canopy (clear)	8 ft.	10 ft.
b Width of storefront bay(s)	10 ft.	15 ft.
c Height of bulkhead	1 ft.	3 ft.
d Glass area % of ground floor wall area	70	90
e Storefront on second frontage	25 ft.	—

- d. Bulkheads:
- Storefront bulkheads should be of material similar or complementary to the main materials of the building and should be made of the same materials or materials that appear to be visually “heavier” than the adjacent walls.
 - Recommended materials include ceramic tile, polished stone, or glass tile.
- e. Awning widths should correspond to storefront openings and shall not extend across the entire facade.
- f. New or renovated storefronts within historic buildings should emulate or recreate a previous storefront (from historic photos or drawings) in order to harmonize with the overall building architecture. This can be flexibly interpreted, for example, when the general form of a new storefront is like the original but the materials are contemporary.



Large glazing area of display windows, glass door, clerestory and retractable awnings.



Shopfronts behind an arcade with prominent, stylized awnings between each arcade opening.



Large glazing area of display windows, wood bulkhead under windows, glass entry door, and awning.

B. ARCADE

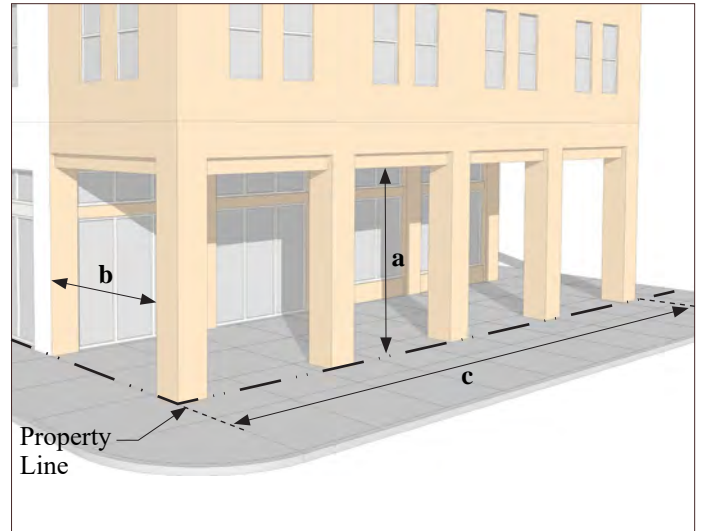
1. **Description.** Arcades are facades with a ground floor colonnade that supports the upper stories of the building or, for one-story buildings, the roof. Arcades contain ground-floor shopfronts, making them ideal for retail or restaurant use, as the arcade shelters the pedestrian while shading the storefront glass, preventing glare that might obscure views of merchandise.

Planter boxes or pots may be placed in between the columns to provide enclosure for such uses as cafe seating.

2. **Design Standards.**

- a. Arcades should be no less than 10' wide clear in all directions.
- b. Along primary frontages, the arcade column spacing should correspond to storefront openings.
- c. Column height should be four to five times the column width. Column spacing and colonnade detailing, including lighting, should be consistent with the style of the building to which it is attached.
- d. Along Primary Street, walls without openings should not exceed 10 linear feet.

FIGURE 5.2. ARCADE DIMENSIONS



Frontage Element	Min.	Max.
a Height (sidewalk to ceiling)	12 ft.	16 ft.
b Depth (facade to interior column face)	8 ft.	16 ft.
c Length along frontage (percent of building facade width)	75	100



Illustrative Photo

C. GALLERY

1. **Description.** Galleries are facades with ground floor colonnades that support a cantilevered shed roof or a deck that covers the sidewalk. Galleries contain ground floor storefronts, making them ideal for retail use. Railing on top of the gallery is only required if the gallery roof is accessible as a deck.

Planter boxes or pots may be placed in between columns to provide enclosure for such uses as cafe seating, provided that adequate throughway access is maintained.

2. **Design Standards.**

- a. Galleries may be roofed. When roofed, the materials, style and design should be consistent with the building.
- b. Galleries should be combined with the Shop-front type (Section 5.10.020).
- c. Galleries may encroach over the sidewalk in the public right-of-way, subject to the issuance of an encroachment permit or license agreement prior to issuance of a building permit.
- d. Column height should be four to five times the column width. Column spacing and colonnade detailing, including lighting, should be consistent with the style of the building to which it is attached.
- e. Columns should be placed in relation to curbs to allow passage around and for passengers of cars to disembark.
- f. Along primary street, walls without openings should not exceed 10 linear feet.

FIGURE 5.3. GALLERY DIMENSIONS



Frontage Element	Min.	Max.
a Height (sidewalk to ceiling)	12 ft.	16 ft.
b Depth (facade to interior column face)	12 ft.	16 ft.
c Length along frontage (percent of building facade width)	75	100

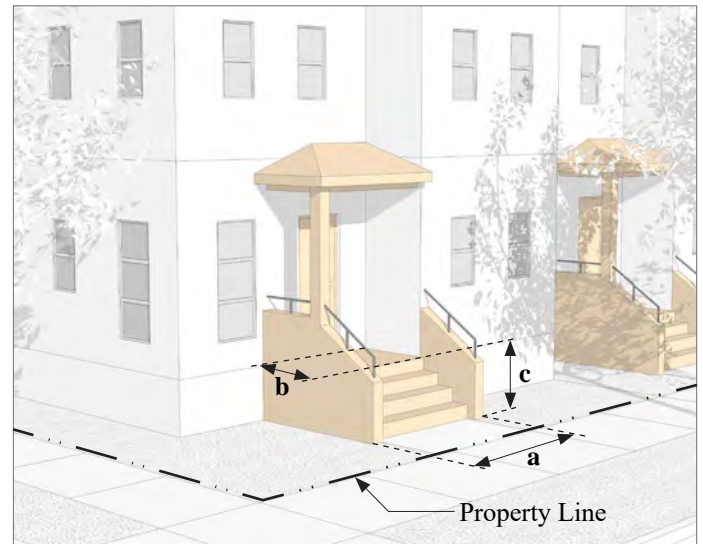


Illustrative Photo

D. STOOP

1. **Description.** A stair and landing leading directly from the sidewalk to a building entrance. The ground floor of the building is raised to provide privacy for the rooms facing the public street. This frontage type is ideal for ground floor housing that is near the street.
2. **Design Standards.**
 - a. Stoops should correspond directly with the building entry(s) they provide access to.
 - b. The exterior stairs may be perpendicular or parallel to the adjacent sidewalk.
 - c. The landing may be covered or uncovered.
 - d. Landscaping should be placed on the sides of the stoop, either at grade or in raised planters.

FIGURE 5.4. STOOP DIMENSIONS



Frontage Element	Min.	Max.
a Stoop width	4 ft.	10 ft.
b Stoop depth (not including stairs)	4 ft.	10 ft.
c Stoop floor height (measured from adjacent finished grade)	18 in.	3 ft.
d Planter/fence height	–	3 ft.



Stoop Example - stairs, landing, and landscape area.

E. DOORYARD

1. **Description.** An elevated or at-grade garden or terrace that is located in the front yard setback and that is enclosed by a low wall located at or near the property line. For elevated Door Yards, access from the sidewalk to the Door Yard is via a stair or ramp.

The Door Yard can accommodate a variety of activities, ranging from dining patios for commercial uses to patios for residential uses. In addition, the interior building spaces are separated from the adjacent sidewalk by the depth of the Door Yard and in the case of raised Door Yards, by the terrace height.

2. **Design Standards.**

- a. Door Yards are enclosed by decorative low walls.
- b. The average grade of elevated door yards should not be more than three feet higher than the adjacent sidewalk or public open space. Walls may extend an additional two (2) feet in height and fences or railings to the height required by the California Building Code (CBC).
- c. Wall and/or fence design, materials, and finishes should be consistent with the architectural style of the building.

FIGURE 5.5. DOOR YARD DIMENSIONS



Frontage Element	Min.	Max.
a Size of Terrace	per building setback	
b Wall height above adjacent sidewalk	—	4 ft.
c Wall height above terrace floor	—	3 ft.
d Door Yard terrace floor height above adjacent sidewalk	—	3 ft.
e Fence/rail height above terrace floor	per CBC	



Door Yard Example - an outdoor patio for a single family house.

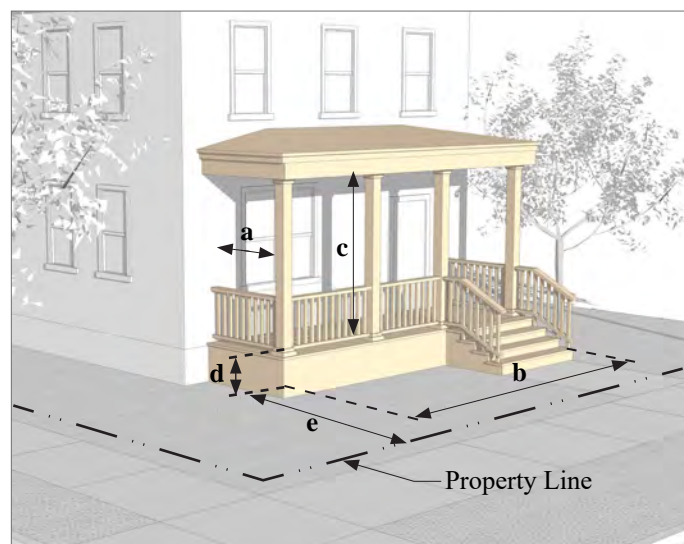


Door Yard Example - outdoor seating areas raised above the adjacent sidewalk and accessed by stairs. A low wall above the terrace level provides a place to sit.

F. PORCH

1. **Description.** A roofed, unenclosed room attached to the exterior of a building that provides a physical transition between the sidewalk and the building. Porches may be provided on buildings that are set back from the Primary and/or Side Street property lines and may encroach into the front yard and side street yard.
2. **Design Standards.**
 - a. Porch materials and design shall be compatible with the design of the rest of the building.
 - b. Front yards shall be landscaped. Paved areas shall be limited to walks and driveways, where present.
 - c. Porches may be enclosed with insect screens if recessed from the exterior wall plane and if visibility is maintained from the sidewalk.

FIGURE 5.6. STOOP DIMENSIONS



Frontage Element	Min.	Max.
a Porch depth (between wall and inside column face)	7 ft.	–
b Porch width (between corner columns)	12 ft.	–
c Porch height (measured from porch surface to top of porch columns)	8 ft.	12 ft.
d Floor height (measured from adjacent finished grade)	18 in.	3 ft.
e Separation between porch and fence or sidewalk.	5 ft.	n/a



Porch Example - raised porch and front yard create separation from street while maintaining relationship with sidewalk.

5.3 THE DOWNTOWN DISTRICT, THE MIXED-USE CORRIDOR DISTRICT, AND AUTO COMMERCIAL DISTRICT

A. INTENT

The Downtown District is intended to be the most vibrant part of the city. It is intended as a center for its citizens, the place where its residents come together to shop and engage with the rest of their community. The design of the buildings in this district should support that role by providing interest and activity at the scale of the pedestrian. Buildings should be multi-storied (as is appropriate in the city's densest district), with the focus placed on the ground level. Building design elements should encourage interaction, with a high level of detail to stimulate the eye, generous windows to provide visibility into downtown activities and businesses, and an overall character that holds the district together as a recognizable, unified center of the community.

The Downtown District at one time contained a number of significant buildings that contributed to its unique character. However, much of the downtown's historic architecture was damaged or destroyed in the 1971 earthquake. Post-earthquake architecture has developed with little stylistic relation to the city and region. The design guidelines that follow will ensure that new buildings support not only the identity of the city, but specifically the Downtown District, creating a collection of buildings that contribute to the recognition of the district as the "center of the city".

B. BUILDING MASS AND INCREMENT

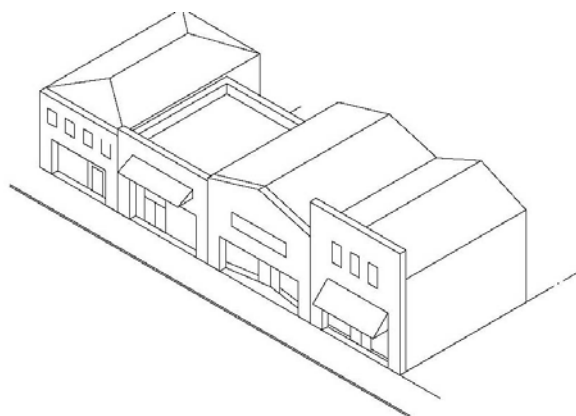
1. **Building Siting and Orientation.** Buildings should be sited to define the street edge of the Maclay and San Fernando corridors in the Downtown District, by establishing a continuous building wall along their primary street frontages.
 - a. Buildings should orient towards their primary street frontage, fronting either Maclay Avenue, Truman Street, or San Fernando Road. Where a parcel has frontage on both Truman Street and San Fernando Road, buildings should front San Fernando Road. Buildings should not orient to parking lots at the sides or rears of buildings.



Downtown should be the most active, vibrant part of the City.



The San Fernando Mall should continue to be a destination shopping center.



Buildings are required to be built to the property line, to create a consistent "street wall" along the sidewalk

- b. Building facades along the primary street frontage should contain elevations activated by doors and windows that look onto the street. Frontages should be public in nature and open to view from the street.
- c. Buildings are required to be built to the property line (see Development Standards for the Downtown District), to create a consistent "street wall" with active storefronts and other facades along the sidewalk. Where portions of the building frontage are recessed for entryways, recessed areas should be treated as part of the public sidewalk, with special design elements, detailing and paving.

2. **Horizontal Mass - Commercial and Mixed-**

Use Buildings. Facades of commercial and mixed-use buildings should be architecturally subdivided into segments that correspond to the small-scale increment of the Downtown District's historic development pattern. Building increments should range from the typical lot increment of twenty four (24) feet wide, to a maximum of thirty (30) feet wide. Some methods of creating building increment are listed below:

- a. Vertical architectural features:
 - i. Apply a vertical pier, pilaster or column between facades. The maximum horizontal protrusion of pilasters into the public right-of-way should be six (6) inches.
 - ii. Apply a vertical slot or recess between facades with a six (6) inch minimum recess depth and a fifteen (15) inch minimum width.
- b. Individualized roof forms:
 - i. Use variation in roof forms to subdivide the building profile, by utilizing different forms over towers, bays or other building volumes.
 - ii. Utilize a change in roof pitch or orientation at special places along the facade.
- c. Towers or building volumes:
 - i. Project a part of the building volume out away from the façade; such as a horizontal mass that punches out horizontally, or as a vertical tower that holds several stacked rooms.
 - ii. Insert a tower with a roof extending above the main building volume, into the facade.
- d. Window/façade composition:
 - i. From one façade segment to the next, use different window sizes, orientations (e.g.



Buildings of different heights along Ventura's Main Street.



Building increment can be created by a vertical pilaster or column between facades.



Building increment can be created by extending the parapet up at the building corners.



A four story building with a ground floor base that is scaled to adjacent one-story buildings.



A rowhouse building that is horizontal in massing with a greater length than height.



Windows, entrances, and balconies create modules along this building facade and help clearly mark the entrance to each unit.

horizontal or vertical proportions), and/or operating types (e.g. single-hung, multi-pane, etc.) to create variety. Windows should maintain consistency in shape and in location across the facade; while variation is recommended, the overall effect should still create a harmonious pattern across the facade.

e. Change in storefront facade:

- i. Ground-floor facades should be designed to give individual identity to each retail establishment. Each shop should have a distinct façade with a unique character.
- ii. At adjacent storefronts, the change in establishments should be clearly evident through a change in storefront façade, for example different base material, window type, and/or door type. This is particularly important for storefronts located in the same building.

3. Horizontal Mass - Residential Buildings within the Mixed-Use Corridor Sub-District.

Buildings in the Mixed-Use Corridor Sub-District should be horizontal in massing, and where possible should have a greater length than height. The overall mass of buildings should be subdivided to modules that express the individuality of each unit, or group of units. Each module should use building volumes or architectural features such as wall breaks, projections, distinct color schemes and individual roof treatments to distinguish them from the larger mass of the building. Modules should occur at a maximum of every fifty (50) feet across the façade. Some methods of breaking up horizontal mass are noted below.

a. Openings and Façade Elements:

- i. Use grouping of façade elements, such as windows and balconies, to create modules along the building facade. Façade elements should be of a consistent size and style so they are readable from module to module.
- ii. Use building projections, overhangs or other articulation at entranceways of each module to clearly mark the entrance to each unit or module of units.

b. Building Volume and Massing:

- i. Design building facades to give individual identity to each vertical module, for example use building projections to denote each segment as a grouping of units.

- ii. Project a part of the building volume from the façade, such as a horizontal mass that punches out horizontally, or a vertical tower that holds several stacked rooms.
 - c. **Building Wall:**
 - i. Use detailing or a change in material to punctuate building modules - for example use brick framing to call out a building bay. Changes in material should be accompanied by a change in plane.
 - ii. Vary portions of the building wall along the front "build-to" line, using porches, bays or building volumes to create change along the front façade.
 - d. **Individualized Roof Forms:**
 - i. Use individual roof forms; for example, provide separate roofs over each module of units, or a single roof that expresses individual units through a series of smaller gables or dormers.
 - ii. Utilize a change in roof type (e.g., shed to gable) or orientation at special places along the façade, with shifts in height and design along the street façade.
- 4. Base Treatment.** Because of the pedestrian nature of the Downtown District, all buildings should maintain a readable base treatment that visually establishes a human scale at the horizontal ground plane. Base treatment should extend around all visible sides of a building. In the Downtown District, base treatments should occur at two scales:
- a. At the individual scale of a person, between one and one-half and three feet (1½ - 3') in height. Ways of accomplishing this include the creation of a base ledge (for example a visibly thicker portion of the building wall) along the ground, or by a material and/or color change of the base wall relative to the building wall above.
 - b. At the scale of the building, marking the ground floor of a multi-story building. This may be created by designing the ground floor of the building to read as heavier than the stories above (e.g. of darker color and/or a stronger material such as masonry), or by a horizontal architectural feature at the first story, such as a ground-floor arcade, loggia or colonnade, a protruding horizontal band, or a cornice line.
- 5. Corner Buildings.** Buildings located at intersections should be designed to define and give prominence to the corner on which they



Base treatments should occur at the individual scale of a person, and at the scale of the building.



Techniques for corner buildings include creation of a corner tower with a special roof . . .



. . . or a corner entrance .



At mixed-use buildings, entrances to residential uses should be clearly distinguishable from retail entrances.



Entrances can be indicated by a recessed entry.



The gap between these liner buildings together with the canopy that extends into the sidewalk make for an easily identifiable pedestrian entrance to this parking structure.

are sited, by acknowledging both street facades with façade articulation and detail. Techniques include:

- a. Creation of a landmark roof form, such as a dome, conical or pyramidal roof.
- b. Creation of a corner tower with a special roof.
- c. A storefront, building protrusion, bay, porch element or arcade that wraps around the corner.
- d. A corner entrance that protrudes or is cut-away from the corner.
- e. A change in roofline; such as a gabled end to mark the corner.

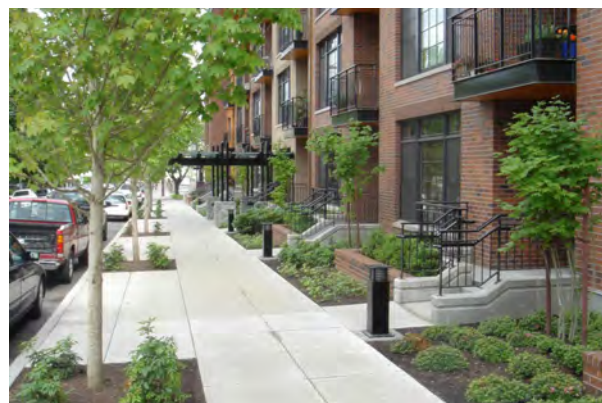
6. Main Entrance. The main entrance of a building should be located along the primary street façade of the building, fronting Maclay Avenue, Truman Street or San Fernando Road. At buildings that have frontage on both Truman Street and San Fernando Road, main entrances should face onto San Fernando Road.

- a. At all buildings, entrances should be clear and easily identifiable, using one or more of the following treatments:
 - i. Marked by a taller mass above, such as a modest tower, or within a volume that protrudes from the rest of building surface;
 - ii. Indicated by a projection from the building façade, and covered by means of a building overhang, awning or canopy that projects from the building face;
 - iii. Indicated by a recessed entry. Recommended treatments include special paving materials such as ceramic tile; ornamental ceiling treatments, such as coffering; decorative light fixtures; and attractive decorative door pulls, escutcheons, hinges, and other hardware;
 - iv. Denoted by a single arch or series of arches to indicate entry. Arcaded entry porches or passageways are also recommended;
 - v. Framed by special architectural elements, such as columns, archways, and overhanging roofs;
 - vi. Emphasized by a small roof overhang over the entrance, change in roofline or a major break in the surface of the subject wall.
- b. At mixed-use buildings, entrances to residential, office or other upper story uses should be clearly distinguishable in form & location from retail entrances, through the following treatments.

- i. Accented by architectural elements that are “residential” in character, such as small windows above the door, sidelights, and ornamental light fixtures, front stoops or plantings.
- ii. Indicated by a recessed entrance, for example a vestibule or lobby.
- c. At residential buildings within the Mixed-Use Corridor Sub-District, multiple entrances are required on the front façade. Entrances should be included within each module of units described in “Horizontal Mass”, above. The following elements are recommended for residential entrances:
 - i. Raised stoops, open porches, entrance vestibules and/or dooryards to increase the privacy threshold between street and residence. At attached residences, these should correspond to the vertical modules of units.
 - ii. Low hedges or walls, with or without entry gates, to separate private front yards from the public sidewalk. Chain link fences are not permitted. (See *San Fernando City Code* Section 106-970: Fences and Walls.)
 - iii. Ornamental lighting of porches, along walks and driveways to highlight entrances and enhance security.
 - iv. A rise in grade (of two to three feet) from the public roadway to the residence if no dooryard is defined, to protect the privacy of residential units.
 - v. Special landscape materials to define front yard spaces and/or accent the entry sequence.

7. Accessory Buildings and Additions. Accessory structures include any structures subordinate to the primary building, such as garages, storage facilities and other ancillary buildings. Their design should be consistent with the prevailing architectural style of the primary structure, and should incorporate the following design components:

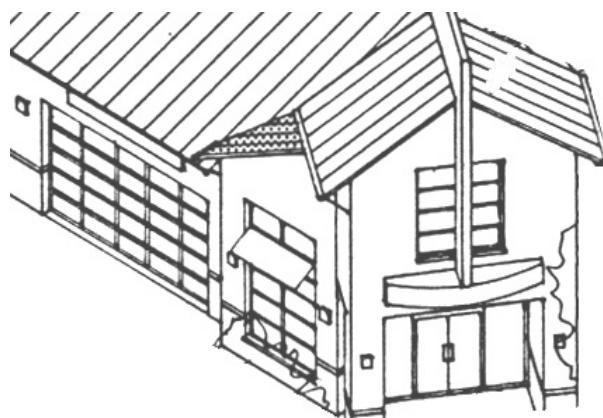
- a. The existing siding should be carried onto the addition or building.
- b. Buildings should include articulation in the form of windows and doors, in the same style as the main structure.
- c. Additions should continue the existing roofline. Buildings should follow the roof style of the main building.



Stoops extend into landscaped front yards, provide access to ground floor units.



Individual entrances provide access to each unit. Low walls and landscaping define the front yard space.



Service entrances and loading docks should be located to the side or rear of the building.



The San Fernando Rey Mission demonstrates many characteristics of the Mission style.



The Spanish Colonial style is typified by plain wall surfaces.

8. Loading and Service Entrances. Loading and services entrances should not intrude upon the public view, or interfere with streetfront activities.

- a. Service entrances should not face Maclay Avenue or San Fernando Road. All service entrances and associated loading docks and storage areas should be located to the side or rear of the building.
- b. Portions of the building facade containing service or truck doors should be integrated into the architectural composition of the larger building facade design. Architectural treatments, materials, and colors should be extended from building facade areas into the facade portion containing truck doors.
- c. Roll-up security doors should be detailed to conceal door housings and tracks, and provide an attractive and finished appearance for all exposed components.

9. Parking Podiums. Parking garages and podiums should be treated with wall textures, colors, and dimensional modules that are coordinated with the architecture of the building.

- a. The pedestrian entrance to a parking structure or podium should be designed as an easily noticeable change within the facade treatment.
- b. Podium entrances should not be located along Maclay Avenue, San Fernando Road, Truman Street, or other primary streets. Entrances should be located to the side or rear of the building.
- c. Vehicle entrances should be treated with architectural articulation and landscape materials, to “mark” a frequently used common entrance for residents and guests. Treatments should include architectural frames or pergolas consistent with the architectural style of the building, decorative doorframe ornament, ornamental lighting, et cetera.
- d. Exposed podiums are prohibited to face Maclay Avenue, San Fernando Road, Truman Street, or other primary streets.
- e. Exposed podiums should not have blank concrete walls. Podium wall textures, colors, and dimensional modules should be coordinated with those of the residential architecture above the podium. Detailing and design, such as decorative scoring, concrete blocks with special surface textures (split-face block, combinations with precision face, etc.), integral color and/or

inset tiles are recommended to provide additional surface articulation.

C. ARCHITECTURAL STYLE

The discussion that follows provides a “stylistic” framework for the design of new structures. The design guidelines below do not prescribe specific styles for new buildings. Rather, the guidelines are set up to allow for a range of architectural styles and types, so as to encourage creativity in design. The guidelines set up a framework for quality design by establishing a framework for a) good urban design relationships between buildings, and b) an assured level of quality in terms of construction.

Projects should draw from San Fernando’s history, and the best of its building traditions. Much of San Fernando’s architectural character is derived from the San Fernando Rey Mission founded in 1797. The primary influences of this era are reflected in the city’s significant public and civic buildings, which draw heavily upon Mission, Spanish Colonial Revival, Mediterranean and even Monterey styles. Other architectural styles that are found elsewhere in San Fernando and may be appropriate to the Downtown District include traditional early 20th century commercial buildings, Craftsman, and Art Deco. Below are a list of features from San Fernando’s most common commercial architecture styles:

1. Elements of Mission architecture.

- Craftsmanship and high quality natural materials
- Simple design that reflected nature in its colors, patterns, and texture
- Thick walls and deeply inset windows.
- Smooth stucco siding
- Large square pillars
- Twisted columns
- Timberwork, wood framing and balustrades
- Corner towers
- Wide eaves with exposed beams and roof rafters
- Sloping, low-pitched or hipped roofs, or flat roofs with parapets.
- Red roof tiles, wood shingles or clay tiles.



Library Square displays elements of the Mediterranean style.



The Monterey style often displays cantilevered balconies or upper-story porches.



An example of the 20th Century Commercial style.



Brick can be used as a primary material, as shown above.



Ceramic tile can be used as an accent material, as shown above.



Stone veneer can be used as an accent material, as shown above.

2. Elements of Spanish Colonial Revival architecture.

- Stucco, brick, wood, or combinations of these materials
- Little or no overhanging eaves
- Plain wall surfaces, Stucco siding
- Arches, especially above doors, porch entries and main windows
- Arcades and other shaded or sheltered outdoor areas
- Decorative ironwork, particularly at balconies, porches and on roof forms.
- Courtyards
- Red tile roofs

3. Elements of Mediterranean architecture.

- Asymmetrical shape with cross-gables and side wings
- Carved doors
- Ornate detailing including molded decoration, carved wood and stonework, or cast ornament
- Spiral columns and pilasters
- Courtyards
- Carved stonework or cast ornaments
- Patterned tile floors and wall surfaces
- Flat roof and parapets, or a hipped roof

4. Elements of the Monterey style.

- Wooden verandas
- Cantilevered balconies or upper-story porches
- Ornate wood spindlework
- Low pitched, hipped or gabled roofs, often covered with shingles

5. Elements of the Art Deco style

- Angular form, often with stepped back façade
- Symmetrical or asymmetrical massing
- Strong vertical accents
- Use of glass or tile on wall surfaces
- Bands of design and carving
- Ornament in cubic forms and zigzag designs, often in colorful terra cotta

6. Elements of the Early 20th Century Commercial style

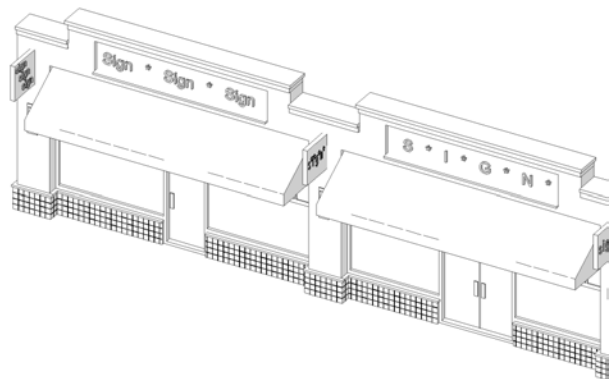
- Flat or slightly pitched roof
- Brickwork or corbels along the cornice or parapet
- Recessed entrances
- Clerestory and transom windows

D. FACADE COMPOSITION

1. **Building Materials.** Highly articulated wall surfaces are recommended for downtown buildings. Detail should be given through intricate storefront design, textures, and accent materials and colors.
 - a. *Primary materials* are those that clad the main building walls. Materials to be used as the primary cladding include:
 - i. **Stucco:** Stucco, cement plaster or stucco-like finishes are acceptable finishes. Attention should be paid to detail and trim elements for a high quality installation. Highly textured surface textures are not recommended. The pattern of joints should be architecturally coordinated with the overall facade composition, and sealant colors should be coordinated with surface and other building colors.
 - ii. **Brick:** Red brick should not be used; lighter colored brick is appropriate. Full size brick veneer is preferable to thin brick tile. Brick veneers should be mortared to give the appearance of structural brick. Brick veneer applications should use wrap-around corner and bullnose pieces to minimize a veneer appearance. An anti-graffiti coating is recommended.
 - iii. **Wood:** Horizontal sidings such as clapboard and tongue-in-groove, vertical siding such as board and batten, and other horizontal sidings such as smaller wood shingles and shakes may be suitable. The larger, more rustic styles of shingles and shakes should not be used. Trim elements should be used, and traditional Craftsman styling such as timber detailing and exposed bracing are recommended.
 - b. *Accent materials* may be used as to add interest and variety at a more intimate scale, for example along architectural elements such as cornices, or on portions of buildings or walls. Accent materials in-



Storefront bases can be made of precast or poured-in-place concrete.



Windows should comprise the majority of the building wall at ground floors and storefronts.



Where window openings are paneled, they should be separated as true divided light windows.



Window sills and surrounds should be proportioned to relate to the window size.



Doors should be detailed and scaled to the individual.

clude stucco, brick and wood, as listed above, and also include:

- i. Ceramic tile: Tile should be limited in use to a facade cladding or decorative wall accent material. Grout color should be coordinated with tile and other building colors.
- ii. Stone and stone veneers: Stone should be used as a base or as a special decorative material for wall panels or sills in combination with stucco or EIFS materials.
- c. *Base materials* are those used along the bottoms of building walls, and can be carried to vertical portions of buildings such as columns, pilasters, or piers, to impart a sense of permanence and solidity. Primary materials are often carried to the building base, but may also include:
 - i. Precast Concrete: Textures, pigments, and special aggregates should be used to create rich surfaces. Precast concrete copings and trim are recommended for use with other materials such as poured-in-place concrete, concrete block, brick, stone, stucco and EIFS. The location of joints between castings and expansion joints should be incorporated into the facade composition. Grout and sealant colors should be coordinated with castings and other building colors. An anti-graffiti coating is recommended.
 - ii. Poured-in-Place Concrete: Concrete walls should generally be clad with stucco or other finish materials; poured concrete may be exposed as an architectural base or a site-work material. Where exposed, the location of formwork tie-holes, expansion joints and control joints should be incorporated into the facade composition. Textured form liners, pigments, stains, and special aggregates should be used to create rich surfaces. An anti-graffiti coating is recommended.
 - iii. Concrete Block: Concrete blocks of various block sizes, surface textures, and colors should be used as an architectural base or a sitework material; plain stack bond concrete block walls are not recommended. Decorative treatments should be used, such as alternating courses of differing heights, different surface textures (precision face and split face) and patterns of colored blocks; and cap and trim pieces should be used. Grout colors should be coordinated with block and other building colors. An anti-graffiti coating is recommended.

- 2. Windows.** As the Downtown District is intended as the most public district in the city, windows should make up a large proportion of the building wall. Repetition of windows is recommended across facades, to create a recognizable pattern of openings along the building wall. This pattern can be reinforced with unifying architectural elements such as similar trim, common operating types, common sill or header lines.
- At ground floors and storefronts, windows should make the majority of the building wall, encompassing a *minimum* of sixty percent (60%) of the facade. Where greater privacy is desired, and for non-commercial uses, restaurants or professional services, windows should be divided into smaller panes - see example at left.
 - At upper stories, windows should encompass a *minimum* of twenty-five percent (25 %) of each floor's facade.
 - Buildings should include vertically proportioned façade openings, with windows that have a greater height than width (an appropriate vertical/horizontal ratio ranges from 3:2 to 2:1).
 - Where window openings are paneled, for example divided with multiple groups of vertical windows, true divided light windows or sectional windows are recommended. Snap-in muntins and those located within double-paned glass should not be used.
 - Window frames should not be set flush with walls. Glass should be inset a minimum of two (2) inches from the exterior wall and/or frame surface.
 - At deeply inset windows (greater than 4" from the exterior wall); the framing may be simple and relatively unarticulated. At shallower insets (2-4" from the exterior wall), projecting sills, molded surrounds, lintels and/or trim should be used to frame openings.
 - Sills and surrounds should be proportioned to relate to the window size. For windows less than 48" in width, surrounds should not exceed 6" in width. For windows greater than 48" in width, surrounds should not exceed 8" in width.
 - Shaped frames and sills, detailed with architectural elements such as projecting sills, molded surrounds, and/or lintels (for example horizontal beams bridging the opening), should be used to enhance openings and add additional relief. They should be proportional to the glass area



Single, discrete awnings should be used for each building bay.



Horizontal ornament can be used as facade decoration.

framed, for example thicker framing members at larger windows.

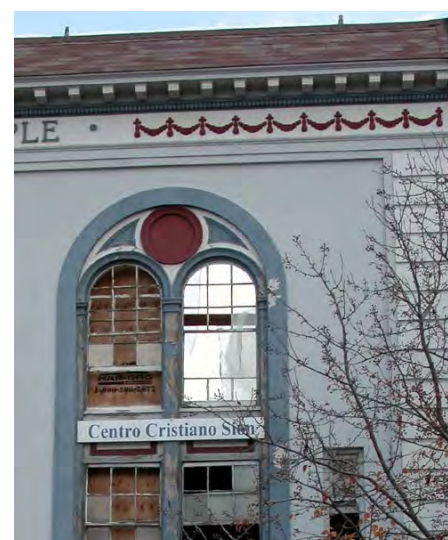
- g. Decorative treatments on windows or balconies are recommended if consistent with building style, for example, iron railings at the base of deeply inset windows on Mission style buildings.
 - h. Aluminum sliding windows should be designed to have substantial framing members, at a minimum width of two (2) inches.
 - i. Clear glass is recommended. Reflective glazing should not be used. Non-reflective films, coatings, low emissivity glass, and external and internal shade devices should be used for heat and glare control.
 - j. 10. Deeply tinted glass or applied films should not be used. If tinted glazing is used, light tints and green, gray and blue hues are recommended.
 - k. 11. Fritted glass, spandrel glass and other decorative treatments are recommended to add privacy and aesthetic variety to glass where desired.
3. **Doors.** As a highly public, pedestrian-oriented district, doors at Downtown District buildings will be highly visible, and frequently seen and touched by the pedestrian. They should be detailed and scaled to the individual, as follows:
- a. High quality materials such as crafted wood, stainless steel, bronze, and other ornamental metals are recommended.
 - b. Windows and glass are recommended to provide visibility into ground-floor establishments.
 - c. Doorways leading to upper story uses should be distinguishable from those leading to retail establishments.
4. **Openings and Façade Elements.** Other design elements may be used along the building façade, in cooperation with windows and doors, to reinforce a recognizable pattern across the facade. Recommended elements include:
- a. Awnings, trellises, canopies, and other building-mounted accessories over storefronts. Single, discrete awnings should be used for each storefront or building bay, rather than one continuous run-on awning. These items should be located above the display windows and below the storefront cornice or sign panel, and include:



Flat roofs should be edged with architecturally profiled cornices,



... or with shaped parapets.



Sloping roof forms should be detailed with corbels and decorative supports.

- i. Storefront Awnings - Colored fabric mounted over a metal structural frame or permanent architectural awnings utilizing materials from the building architecture are both acceptable. Internally illuminated fabric awnings with signage should not be used.
- ii. Trellises and Canopies - Materials, colors, and form should be derived from the building architecture. A trellis painted the same color as a building's trim scheme is appropriate.
- b. Ground floor arcades may be used to provide shade at the ground level of the building. Arcades should be located at the setback line, and may step back to the second story, or may be designed to be flush with the building wall above.
- c. Architectural ornament and detailing, including:
 - i. Horizontal ornament such as awnings or belt courses, string courses or cornice lines.
 - ii. Three-dimensional ornament like pilasters, wood detailing and embossed relief.
 - iii. Ornamental wall-mounted outdoor lighting (sconces) can be used to accent entries or rhythms of repeating pilasters.
- d. Alcoves, balconies and porches at upper stories, to provide outdoor spaces for upper story tenants.
- e. Window boxes, or other wall-mounted elements below storefront windows, to add interest at a pedestrian scale.

E. ROOFS

1. **Roof Types.** Downtown District buildings should have a highly articulated roof profile, created through a range of roof forms, varying building heights, interesting cornices.
 - a. Flat roofs should always be edged with parapet walls; and should be treated with one or more of the following conditions:
 - i. An architecturally profiled cornice and/or expressed parapet cap should be used to terminate the top of parapet wall.
 - ii. Surface mounted cornices, continuous shade elements, or trellises should be used to strengthen a parapet wall design.
 - iii. A single layer, flush sheet metal parapet cap (for example a simple inverted U of sheet metal over the top of a parapet wall) without a substantial built-up edge should



Terra Cotta or concrete tile roofs are recommended.



Asphalt, slate or cement shingles may also be used.

not be used, as these installations often display warped sheet metal (oil-canning) and a low-quality appearance. If used, sheet metal parapet caps should provide a formed (compound folded) overhanging edge termination and a heavy gauge sheet metal thickness selected to avoid oil-canning distortion.

- b. Sloping roof forms may include pitched, gable, hip, and pyramidal roofs; and should be designed as follows:
 - i. Roof overhangs are recommended. Brackets and corbels (for example decorative supporting pieces designed to bear the weight of projected overhangs), or other expressed roof overhang supports are recommended to add richness to detailing. The spacing module of repeating supports should relate to the building's structural bay spacing.
 - ii. The soffit (for example the underside surface of the roof overhang) should be incorporated into the overall architectural composition with beams, coffers, light fixtures and other design articulation.
 - iii. Vertical roof edge fascia should be vertically sub-divided by additional horizontal layers, stepbacks, trim, and other detailing.

- c. Special forms such as domes, conical roofs and pyramidal roofs should be restricted to special locations, for example to mark major intersections, to denote civic buildings, or to announce unique elements such as a major public entry or a theater.

2. Roof Materials. Selection of roof materials should be made with consideration for the neighborhood context. Roof materials and color should be selected with consideration for views from above. Recommended roof materials include:

- a. Clay, Terra Cotta or Concrete Tile: Tile roofs are recommended wherever sloping roof forms are used. Projects should use authentic terra cotta barrel tiles, and avoid simulated products. A double row of tiles should be used to terminate the roof at the edge of rooflines.
- b. Asphalt, Slate or Cement/Slate-type Shingles: Projects using shingles should use the highest quality commercial grade materials, and be provided with adequate trim elements.
- c. Corrugated and Standing-Seam Metal Roofing: The structural support detailing of corrugated metal roofing should insure that metal roof edges and panels will not sag, bend, or be vulnerable to impacts and denting, especially where undersides and edges of corrugated metal roofing are visible. Finishes should be anodized, fluorocoated or painted. Flat, unarticulated metal roof tiles and metal roof sheeting are not recommended.
- d. Tar and Gravel, Composition, or Elastomeric Roofs: These roof materials should be limited to flat roof locations, and should be screened from view from adjacent buildings and sites by parapet walls. They should be avoided where prominently viewable from adjacent uphill areas.

3. Roof Equipment and Screening.

- a. Roof mounted equipment such as cooling and heating equipment, antennae and receiving dishes should be completely screened by architectural enclosures that are derived from or strongly related to the building's architectural expression, or enclosed within roof volumes.
- b. In the design of screening enclosures, use dimensional increments of window spacing, mullion spacing, or structural bay spacing taken from the facade composition. Materials, architectural styles, colors and/or other elements from the facade composition should also

be used to strongly relate the screening to the building's architecture.

- c. The location, spacing, materials, and colors of down-spouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the facade and roof. Downspouts should be concealed within walls or located to harmonize with window spacing and facade composition.

F. COLOR

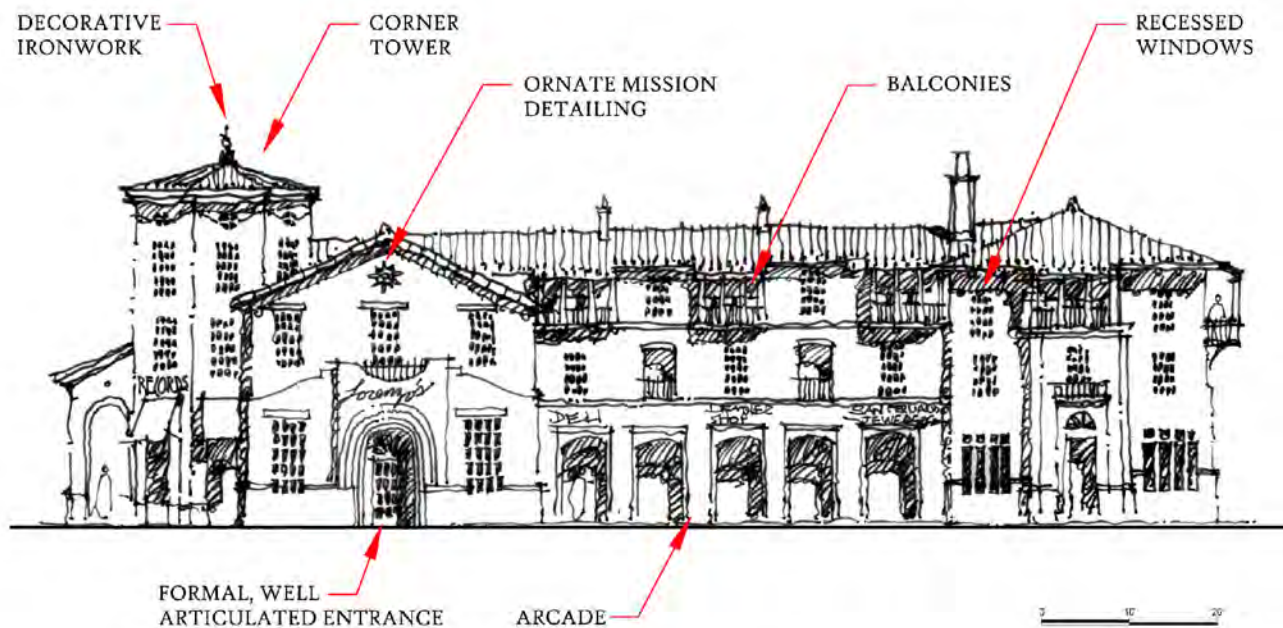
1. Consistent Color Palette. A consistent color palette is recommended for the Downtown District, to ensure that new buildings are compatible with existing buildings. An example of the color range that falls in this palette is shown on the following page.

- a. Primary building colors should be light in tone, and neutral in hue. Appropriate colors may range from white to soft cream and yellows to warm beige, as shown on the color palette that follows. Stark, extreme colors like black should not be used as primary wall colors.
- b. Secondary and accent colors can be used to highlight special architectural features such as building bases or wainscots, columns, cornices and bands, trim on doorframes, storefront elements and similar features. They may also be used sparingly at fabric awnings, banners, window frames, or special architectural details. Secondary and accent colors may be stronger, and more saturated in hue than primary colors - accents of deeper reds and dark browns are recommended, as shown on the color palette that follows on page 90. If used in limited amounts, such as at building signage, rich and vivid colors may be used. Fluorescent colors should not be used.
- c. For tiled roofs, red and terra cotta colors are recommended. For shingle and other roof styles, grey or earth tones are recommended. Light colored roofs may also be used to reduce solar radiation; these should be screened from view by architectural enclosures such as parapet walls or other screening treatment.

COLOR PALETTE



ARCHITECTURAL DETAILS



LARGER RETAIL USES HAVE FORMAL, WELL ARTICULATED ENTRANCES



ARCADES PROVIDE SHADE FOR PEDESTRIANS



BALCONIES PROVIDE SHADED OUTDOOR SPACE & ENRICH THE BUILDING'S FACADE



ORNATE MISSION DETAILS USED SPARINGLY ON PROMINENT VOLUMES



THE SAN FERNANDO MISSION



HISTORIC SAN FERNANDO ARCHITECTURE

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5.4. THE MACLAY DISTRICT

A. PURPOSE

The “grand residential boulevard” is a part of American history. In the past, cities put their grandest residences on display along their primary thoroughfares. Large homes presided over tree-lined streets, and often defined the character of one’s entry into the city.

In San Fernando as in other communities, commercial development has laid claim to the primary roadway corridors. New residential buildings on the Maclay District will re-establish these roadway corridors as part of the fabric of the community, and bringing back the traditions of the “grand residential boulevard”. The Maclay District will serve as the “face” of the city’s neighborhoods. The housing built along its length will provide homes for new and returning residents, and will connect the corridor back to the homes and neighborhoods that lie behind it.

Housing along such a corridor must be designed to be compatible with its more public setting. Along such a highly visible corridor, buildings should be generously proportioned and impressive in scale, as larger versions of the city’s single-family homes. Architecture should be designed to contribute to the impression of Maclay Avenue as a residential boulevard, with grand buildings that are graciously set back from the roadway. They should maintain a certain level of solidity on the ground floor, to maintain privacy along the public thoroughfare, and become more permeable – with more windows, more openings – on upper stories. Plantings and landscaped setbacks can increase the prominence and grandeur of the project, while giving residences more privacy from the public realm of the street. Residential entrances above street level can create a sense of privacy and distance from the street. Individual units should be organized in groups, as a part of a larger whole, to create buildings that are of a scale and character appropriate to a wide, frequently traveled road.

B. BUILDING MASS AND INCREMENT

1. **Building Siting and Orientation.** Buildings should be sited to define the street edge of the Maclay corridor, by establishing a strong building wall along the street frontage.
 - a. Buildings should orient towards Maclay Avenue. Buildings should *not* orient to parking lots at the sides or rears of buildings.
 - b. Building facades along the primary street frontage should contain elevations activated by doors and windows that look onto the street.



A “grand residential boulevard”.



Housing along a wide corridor should be setback from the roadway.



Residential entrances should be raised above street level.



Front porches and building volumes should be used to create variation along the setback line.



Groupings of façade elements can be used to create modules along the building facade.



Horizontal building volumes or vertical towers can be used to break up the horizontal mass of the building.

Frontages should be of a substantial scale and character, reading as “grand mansions” or simply as larger versions of the city’s single-family homes.

- c. A minimum percentage of the building façade is required to be built to the setback line (see *Development Standards for the Maclay District*), in order to create a consistent “street wall” along Maclay Avenue. However, variation along this setback line is recommended through use of protrusions such as front porches, and building volumes (see “Horizontal Mass”, below).

2. Horizontal Mass. Buildings in the Maclay District should be horizontal in massing, and where possible should have a greater length than height. The overall mass of buildings should be subdivided to modules that express the individuality of each unit, or group of units. Each module should use building volumes or architectural features such as wall breaks, projections, distinct color schemes and individual roof treatments to distinguish them from the larger mass of the building. Modules should occur at a maximum of every fifty (50) feet across the façade. Some methods of breaking up horizontal mass are noted below.

- a. **Openings and Façade Elements:**
 - i. Use grouping of façade elements, such as windows and balconies, to create modules along the building facade. Façade elements should be of a consistent size and style so they are readable from module to module.
 - ii. Use building projections, overhangs or other articulation at entranceways of each module to clearly mark the entrance to each unit or module of units.
- b. **Building Volume and Massing:**
 - i. Design building facades to give individual identity to each vertical module, for example use building projections to denote each segment as a grouping of units.
 - ii. Project a part of the building volume from the façade, such as a horizontal mass that punches out horizontally, or a vertical tower that holds several stacked rooms.
- c. **Building Wall:**
 - i. Use detailing or a change in material to punctuate building modules - for example use brick framing to call out a building bay. Changes in material should be accompanied by a change in plane.

- ii. Vary portions of the building wall along the front “build-to” line, using porches, bays or building volumes to create change along the front facade.
- d. Individualized Roof Forms:
 - i. Use individual roof forms; for example, provide separate roofs over each module of units, or a single roof that expresses individual units through a series of smaller gables or dormers.
 - ii. Utilize a change in roof type (e.g., shed to gable) or orientation at special places along the façade, with shifts in height and design along the street facade.

3. **Vertical Mass.** Multi-story buildings in the Maclay District should be articulated so as to reduce the impression of vertical mass and height, stepping back at sides and rear facades towards the existing neighborhoods.

- a. All multi-story buildings should maintain a readable base treatment at the ground level, to separate it from upper stories. A building base may be created by any of the following treatments:
 - i. Design the ground floor of the building to read as a base for the rest of the structure; for example use arcades and loggias, or entry porticos and front porches, to wrap the building at its base.
 - ii. Establish a visibly thicker portion of the wall along its base at the ground level, where the wall above the base sets back and openings within the base are more deeply recessed.
 - iii. Use a material and/or color change to distinguish the base wall from the building wall above. The base material should generally be heavier (e.g. of darker color and/or a stronger material), with a lighter quality at stories above (e.g., predominantly masonry at the ground, larger windows and more glass above).
- b. All multi-story buildings should step back at sides and rear facades towards the existing neighborhoods. Methods of vertical subdivision include the following:
 - i. Use design elements to accentuate the horizontal layers of a building and differentiate the ground level from upper stories of the building; for example use smaller roofs over porches or other architectural elements at the building base.



Individual roof forms can be used to denote individual units.



A front porch can create a base for the building.



Multi-story buildings should step down toward existing neighborhoods, as shown here.



At major intersections, corner treatments may include the creation of a landmark roof form,



... or a corner tower with a special roof.



Entrances can be denoted by a pediment or overhang.

- ii. Use step-backs or partial indentations at upper stories. Elements such as balconies, outdoor decks, and trellises are recommended to soften the transition from upper to lower stories.
- iii. Use a change in material or treatment combined with a change in depth or plane.
- iv. Use applications of decorative moldings or cornices to accentuate the horizontal layers of a building.

4. **Corner Buildings.** Buildings located at intersections should be designed to emphasize the corner on which they are sited, by acknowledging both street facades with façade articulation and detail.
 - a. At major intersections, such as the intersection of Maclay Avenue with Eight Street, corner treatments may include:
 - i. Creation of a landmark roof form, such as a dome, conical or pyramidal roof.
 - ii. Creation of a corner tower with a special roof.
 - b. At minor intersections, such as the intersection of Maclay Avenue with Glenoaks Boulevard, a modest articulation of the building mass is recommended to join the two street facades. Treatments may include:
 - i. A storefront, building protrusion, bay, porch element or arcade that “wraps” the corner.
 - ii. A corner entrance that protrudes or is cut-away from the corner.
 - iii. A change in roofline; for example a gabled end to emphasize the corner.

5. **Main Entrance.** The main entrance of a building should be located along the primary street façade of the building, fronting Maclay Avenue. Entrances should be designed to be consistent with the overall architectural style of the building.
 - a. Building entrances should front onto the street, and be prominent and easy to identify, using one or more of the following treatments:
 - i. Marked by a taller mass above, such as a modest tower, or within a volume that protrudes from the rest of building surface;
 - ii. Indicated by a projection from the building façade, and covered by means of a porch or portico that projects from the building face;

- iii. Indicated by a recessed entry-recommended treatments include special paving materials such as ceramic tile; ornamental ceiling treatments such as coffering; decorative light fixtures; and attractive decorative door pulls, escutcheons, hinges, and other hardware;
 - iv. Denoted by a single arch or series of arches to indicate entry-arcaded entry porches or passageways are also recommended.
 - v. Framed by special architectural elements, such as columns, archways, and overhanging roofs;
 - vi. Emphasized by a small roof overhang over the entrance, change in roofline or a major break in the surface of the subject wall.
- b. At residential buildings, multiple entrances are recommended on the front façade. Where possible, entrances should be included within each module of units described in “Horizontal Mass”, above. The following elements are recommended for residential entrances:
- i. Raised stoops, open porches, and/or entrance vestibules to increase the privacy threshold between street and residence. At attached residences, these should correspond to the vertical modules of units.
 - ii. Low hedges, fences and/or entry gates to separate private front yards from the public sidewalk. Chain link fences are not permitted. (See *San Fernando City Code* Section 106-970: Fences and Walls.)
 - iii. Ornamental lighting of porches, along walks and driveways to highlight entrances and enhance security.
 - iv. A rise in grade (of two to three feet) from the public roadway to the residence, to protect the privacy of residential units.
 - v. Special landscape materials to define front yard spaces and/or accent the entry sequence.

6. **Accessory Buildings and Additions.** Accessory structures include any structures subordinate to the primary building, such as garages, storage facilities and other ancillary buildings. Their design should be consistent with the prevailing architectural style of the primary structure, and should incorporate the following design components:

- a. The existing exterior finish and treatment of main structure on the site should be carried onto any addition or out-building.



Entrances should include raised stoops, front porches and landscaping.



Landscape materials can be used to accent the entry sequence.



Accessory structures should include articulation in the same style as the main structure.



Garages should be loaded from rear alleys



Rear alleys should be well-lit and planted with trees.



Carriage style garage doors should be used where compatible with architectural style.

- b. Buildings should include articulation in the form of windows and doors, in the same style as the main structure.
- c. Out-buildings should follow the roof style of the main building. Additions should continue existing rooflines where possible.

7. Loading and Service Entrances. Loading and services entrances should not intrude upon the public view, or interfere with street front activities.

- a. Service entrances should not face Maclay Avenue. All service entrances and associated loading docks and storage areas should be located to the side or rear of the building.
- b. Portions of the building facade containing service or truck doors should be integrated into the architectural composition of the larger building facade design. Architectural treatments, materials, and colors should be extended from building facade areas into the facade portion containing truck doors.
- c. Roll-up security doors should be detailed to conceal door housings and tracks, and provide an attractive and finished appearance for all exposed components.

8. Residential Garages and Vehicular

Entrances. Where possible, garage entrances should be located to the rear or side of the property to minimize visual impact to the street.

- a. Garages should be loaded from rear alleys where possible. Alleys are required to be well-lit.
- b. Where garage doors are located at front facades, the garage door should be recessed at least two feet into the wall of the unit in which it is located, and the garage shall not constitute more than forty percent (40%) of the front façade of that unit. No more than two garages may be lined up consecutively on a front façade.
- c. The design of the garage door should relate to the particular architectural style selected. Garage doors should appear to be set into the walls rather than flush with the exterior wall, and carriage style garage doors are recommended where compatible with architectural style.
- d. Single-car garage doors are strongly recommended to avoid a car-dominated appearance on the facade. Where double car widths are used, doors may not exceed a width of twenty feet.

ty (20) feet maximum, and elements such as trellises should be used to subdivide the width of the door.

9. **Parking Podiums.** Podiums should be considered part of the building base, with wall textures, colors, and dimensional modules that are coordinated with the residential architecture.
 - a. Podium entrances should not be located along primary streets. When the only way to access podiums is along primary street frontage, garage entrances must be recessed behind the front wall of the building to minimize visual impact to the street, and should not exceed twenty (20) feet in width.
 - b. Vehicle entrances should be treated with architectural articulation and landscape materials so as to identify a frequently used common entrance for residents and guests. Treatments should include architectural frames or pergolas consistent with the architectural style of the building, decorative doorframe ornament, ornamental lighting, et cetera.
 - c. Exposed podiums should not have blank concrete walls. Podium wall textures, colors, and dimensional modules should be coordinated with those of the residential architecture above the podium. Detailing and design, such as decorative scoring, concrete blocks with special surface textures (e.g., split-face block, combinations with precision face, etc.), integral color and/or inset tiles are recommended to provide additional surface articulation.

C. ARCHITECTURAL STYLE

The discussion that follows provides a “stylistic” frame-work for the design of new structures. The Design Guidelines below do not prescribe specific styles for new buildings. Rather, the guidelines are set up to allow for a range of architectural styles and types, so as to encourage creativity in design. The Guidelines set up a framework for quality design by establishing a framework for a good urban design relationships between buildings and an assured level of quality in construction.

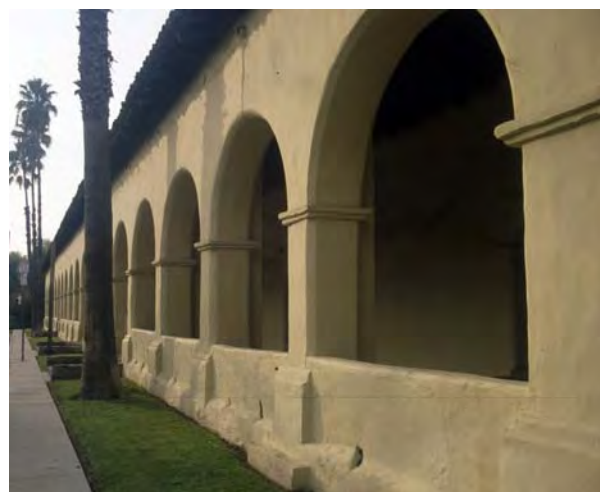
Residential influences in San Fernando are eclectic, ranging from Spanish-inspired styles to east coast influences. New residential buildings should build upon these roots, and draw from the broad menu of residential styles the city has to offer. These include Mission, Mediterranean, Spanish Colonial Revival, and Monterey Mediterranean styles; as well as Southern California variations on the Craftsman, bungalow and various Victorian styles. Below are some of the primary features found in each architectural style:



Vehicle entrances should be treated with architectural articulation.



Exposed podiums should include detailing and design such as concrete blocks with special surface textures.



The San Fernando Rey Mission displays stucco siding and square pillars.



A Spanish Colonial facade with arched windows and ironwork.



A Mediterranean styled home in San Fernando.



An example of the California bungalow.

1. Elements of Mission architecture.

- Plain, smooth stucco siding
- Large square pillars and twisted columns Timberwork, wood framing and balustrades Bell or corner towers
- Sloping, low-pitched or hipped roofs or flat roofs with parapets.
- Red roof tiles, wood shingles or clay tiles.

2. Elements of Spanish Colonial Revival architecture.

- Stucco, brick, wood, or combinations of these materials.
- Little or no overhanging eaves
- Deeply inset windows within thick stucco walls Arches, especially above doors, porch entries and main windows
- Decorative ironwork, particularly at balconies, porches and on roof forms.
- Courtyards, porches, pergolas and other shaded or sheltered outdoor areas Red tile roofs
- Red tile roofs

3. Elements of Mediterranean architecture.

- Asymmetrical shape with cross-gables and side wings
- Carved doors
- Ornate detailing including molded decoration, carved wood and stonework, or cast ornament Spiral columns and pilasters
- Carved stonework or cast ornaments
- Patterned tile floors and wall surfaces
- Flat roof and parapets, or a hipped roof

4. Elements of the Monterey style.

- Paneled doors with sidelights
- Double-hung windows with mullions
- Ornate wood spindlework
- Projecting continuous balconies or porches on upper-stories Wooden verandas
- Low pitched, hipped or gabled roofs, often covered with shingles

5. Elements of the Craftsman style.

- Full- or partial-width porches
- Pedestal-like, tapered columns
- Overhanging eaves and exposed roof rafters
- Low-pitched gabled roof
- River rock exterior elements
- Horizontal wooden clapboard siding
- Smooth stucco or concrete building exterior

6. Elements of the California Bungalow house.

- An offset entryway
- A projecting bay on the façade
- Large front porch with square columns
- One or one and a half stories
- Low-pitched roof
- River rock exterior elements
- Horizontal wooden clapboard siding
- Smooth stucco or concrete building exterior

7. Elements of the Victorian (Queen Anne and Eastlake) styles.

- Asymmetrical facades
- Elaborate spindlework ornamentation
- Corner or curved towers
- Extensive, wrap around porches on the first floor
- Surfaces with a variety of patterning, i.e. clapboard or patterned shingles Protruding bay windows
- Steeply pitched roofs

8. Elements of the Art Deco style.

- Angular form, often with stepped back façade
- Symmetrical or asymmetrical massing
- Strong vertical accents
- Use of glass or tile on wall surfaces
- Bands of design and carving
- Ornament in cubic forms and zigzag designs, often in colorful terra cotta



A Victorian home in San Fernando.



An example of an Art Deco multi-family building.



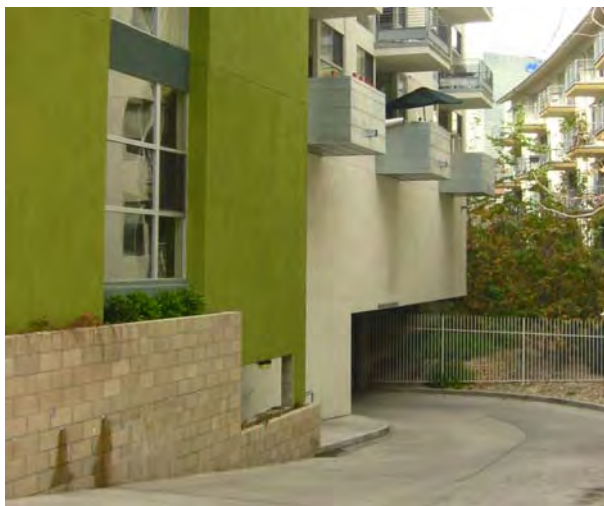
A residential example of Streamline Moderne.



Stucco may be used as a primary building material.



Wood timber detailing may be used as an accent material.



Concrete block may be used as a base material.

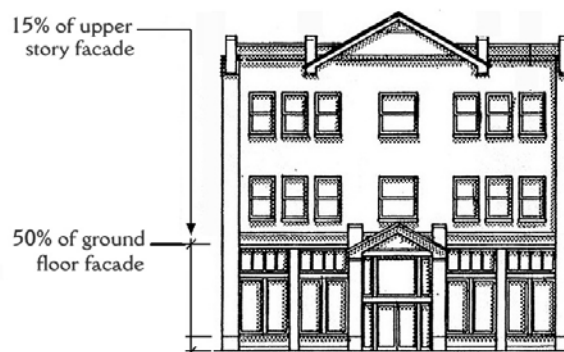
9. Elements of the Streamline Moderne style.

- Horizontal building orientation
- Technological and nautical themes / references
- Smooth, rounded building corners
- White or light in color
- Long bands of windows
- Rounded edges, corner windows, and glass block walls

D. FACADE COMPOSITION

1. **Building Materials.** A variety of detail is recommended for buildings in the Maclay District, to contribute to a neighborhood character, where each building has its own, highly personalized detail and design elements. Where appropriate, combinations of surfaces and textures may be used to achieve this variety.
 - a. *Primary materials* are those that clad the main building walls. Materials to be used as the primary cladding include:
 - i. **Stucco:** Stucco, cement plaster or stucco-like finishes are acceptable finishes. Attention should be paid to detail and trim elements for a high quality installation. Highly textured surface textures are not recommended. The pattern of joints should be architecturally coordinated with the overall facade composition, and sealant colors should be coordinated with surface and other building colors.
 - ii. **Brick:** Full size brick veneer is preferable to thin brick tile. Brick veneers should be mortared to give the appearance of structural brick. Brick veneer applications should use wrap-around corner and bull-nose pieces to minimize a veneer appearance. An anti-graffiti coating is required.
 - iii. **Wood:** Horizontal sidings such as clapboard and tongue-in-groove, vertical siding such as board and batten, and other horizontal sidings such as smaller wood shingles and shakes may be suitable. The larger, more rustic styles of shingles and shakes should not be used. Trim elements should be used, and traditional Craftsman styling such as timber detailing and exposed bracing are recommended.

- b. *Accent materials* may be used as to add interest and variety at a more intimate scale, for example at porches, or at window surrounds or other architectural framing. Accent materials include stucco, brick and wood, as listed above, and also include stone and stone veneers. Stone should be used only as a base or as a special decorative material for wall panels or sills in combination with stucco or EIFS materials.
- c. *Base materials* are those used along the bottoms of building walls, and can be carried to vertical portions of buildings such as columns, pilasters, or piers, to impart a sense of permanence and solidity. Primary materials are often carried to the building base, but may also include:
 - i. *Precast Concrete*: Textures, pigments, and special aggregates should be used to create rich surfaces. Precast concrete copings and trim are recommended for use with other materials such as poured-in-place concrete, concrete block, brick, stone, stucco and EIFS. The location of joints between castings and expansion joints should be incorporated into the facade composition. Grout and sealant colors should be coordinated with castings and other building colors. An anti-graffiti coating is required.
 - ii. *Poured-in-Place Concrete*: Concrete walls should generally be clad with stucco or other finish materials; poured concrete may be exposed as an architectural base or a site work material. Where exposed, the location of formwork tie-holes, expansion joints and control joints should be incorporated into the facade composition. Textured form liners, pigments, stains, and special aggregates should be used to create rich surfaces. An anti-graffiti coating is recommended.
 - iii. *Concrete Block*: Concrete blocks of various block sizes, surface textures, and colors should be used as an architectural base or a site work material; precision concrete block walls are not recommended. Decorative treatments should be used, such as alternating courses of differing heights, different surface textures (e.g., precision face and split face) and patterns of colored blocks; and cap and trim pieces should be used. Grout colors should be coordinated with block and other building colors. An antigraffiti coating is recommended.



At retail clusters, windows should comprise a minimum of 50% of the ground floor facade and 15% percent of upper story façade .



Windows should have a greater height than width.



Windows should NOT be set flush with walls.



At shallow insets, projecting sills, molded surrounds, lintels and/or trim should be used to frame openings.



Sills and surrounds should be proportioned to relate to the window size.



Aluminum sliding windows should not be used.

2. **Windows.** Windows should be grouped so that they recognizably belong to a building module or volume, and create a recognizable composition within each unit with a clear hierarchy of major and minor windows, rather than being repeated uniformly across a wide façade with multiple components.
 - a. At residential ground levels, windows should be designed and oriented so as to preserve privacy for ground floor units, and should comprise a *minimum* of fifteen (15%) percent of the building wall area.
 - b. At retail clusters, ground floor and storefront windows should comprise a minimum of fifty percent (50%) of the ground floor facade surface area. To restrict visibility into private residences, windows at the ground level in the Maclay District should be restricted in proportion to the building wall. Windows should increase in number and size at upper stories.
 - c. At upper stories, windows should comprise a minimum of fifteen (15%) percent of each floor's façade wall surface area.
 - d. Buildings should include vertically proportioned façade openings; with windows that have a greater height than width (an appropriate vertical/horizontal ratio ranges from 1.5:1 to 2:1).
 - e. Where window openings are paneled, for example divided with multiple groups of vertical windows, true di-vided light windows or sectional windows are recommended. Snap-in muntins and those sandwiched within double-paned glass should not be used.
 - f. Window frames should not be set flush with walls. Glass should be inset a minimum of two (2) inches from the exterior wall and/or frame surface.
 - i. At deeply inset windows (greater than 4" from the exterior wall); the framing may be simple and relatively unarticulated. At shallower insets (2-4" from the exterior wall), projecting sills, molded surrounds, lintels and/or trim should be used to frame openings.
 - ii. Sills and surrounds should be proportioned to relate to the window size. For windows less than 48" in width, surrounds should not exceed 6" in width. For windows greater than 48" in width, surrounds should not exceed 8" in width.

- g. Special Windows – Individual elements such as bays or dormers should be used to add interest and a domestic character to the facade. Decorative treatments on windows or balconies, such as wood or metal grilles on windows or balconies, wood balcony columns and balustrades, and simple detailed trim are recommended.
- h. Aluminum sliding windows should not be used.
- i. Clear glass is recommended. Reflective glazing should not be used. Non-reflective films, coatings, low emissivity glass, and external and internal shade devices should be used for heat and glare control.
- j. Deeply tinted glass or applied films should not be used. If tinted glazing is used, light tints and green, gray and blue hues are recommended.
- k. Fritted glass, spandrel glass and other decorative treatments are recommended to add privacy and aesthetic variety to glass where desired.

3. **Doors.** Doors should match or complement the materials, design and character of the primary building; for example ornate carved doors at Mediterranean style homes, and simply styled doors with subdued ornamentation at Craftsman style residences.

- a. High quality materials such as crafted wood, stainless steel, bronze, and other ornamental metals are recommended.
- b. Doorways leading to upper story uses should be distinguishable from those leading to retail establishments.

4. **Openings and Façade Elements.** Other design elements may be used in coordination with windows and doors, to create a consistent effect of openings across the facade wall. Openings and façade elements should be organized along the façade so that each grouping is recognizable as belonging to an individual unit or module of units.

- a. Façade elements should create an ordered composition across the building façade, to create a recognizable grouping of elements that defines each individual unit or module within the larger building.
- b. Buildings should not have large, blank or monotonous surfaces except when such façade wall areas are used in contrast to concentrated



Balconies and porches should be used to provide outdoor spaces for upper story tenants.



Special architectural features such as bay windows and dormers should be used.



The vertical edge of the roof should be detailed in accordance with the building's architecture.



Roof brackets and supports should be used.

detail in other areas of the façade, as in Spanish architecture styles. Designs should include sufficient articulation, such as bay windows, entrance vestibules and dormers, to create appropriately scaled, interesting facades.

- c. Alcoves, balconies, porches or other indoor-outdoor elements should be used to provide outdoor spaces for upper story tenants, and to articulate the unit on the façade. Balconies should be designed as individual elements; run-on or continuous balconies that extend across the length of a façade should not be used except where integral to a building's architectural style such as with Monterey Style.
- d. Special architectural features should be used to create articulated, interesting facades that look custom-made for each individual building, rather than mass produced for a complex or development. These include features such as recessed windows with authentic muntins, architectural trim with substantial depth and detail, bay windows, window boxes, dormers, entry porches, et cetera.

E. ROOFS

1. **Roof Types.** Buildings in the Maclay District should use a variety of roof forms on each building, to accentuate the fine grain of the neighborhood-scaled district and to denote individual units where possible. No single roof form may extend for more than one hundred (100) feet in length, without incorporating a change in orientation, slope or roof type.
 - a. All continuous sloping roof forms (i.e., without flat horizontal portions) are recommended. These include pitched, gable, hip, and pyramidal roofs, which should be designed as follows:
 - i. Roof overhangs are recommended. Brackets and corbels (i.e., decorative supporting pieces designed to bear the weight of projected overhangs), or other expressed roof overhang supports are recommended to add richness to detailing. The spacing module of repeating supports should relate to the building's structural bay spacing.
 - ii. The soffit (i.e., the underside surface of the roof overhang) should be incorporated into the overall architectural composition with beams, coffers, light fixtures and other design articulation.

- iii. The vertical edge of the roof should be detailed to demonstrate additional horizontal layers, stepbacks, trim, and other detailing.

- b. If used, flat roofs should always be edged with parapet walls; and softened with residential accessories such as shading elements, or trellises.

2. **Roof Materials.** Selection of roof materials should be made with consideration for the neighborhood context. Roof materials and color should be selected with consideration for views from above. Recommended roof materials include:

- a. Clay, Terra Cotta or Concrete Tile: Tile roofs are recommended wherever sloping roof forms are used. Projects should use authentic terra cotta 2-piece barrel tiles, and avoid simulated products. A double row of tiles should be used to terminate the roof at the edge of rooflines.
- b. Asphalt, Slate or Cement/Slate-type Shingles: Projects using shingles should use the highest quality commercial grade materials, and be provided with adequate trim elements.
- c. Tar and Gravel, Composition, or Elastomeric Roofs: These roof materials should be limited to flat roof locations, and should be screened from view from adjacent buildings and sites by parapet walls. They should be avoided where prominently viewable from adjacent multistory buildings or nearby uphill areas.

3. **Equipment and Screening.**

- a. Roof mounted equipment such as cooling and heating equipment, antennae and receiving dishes should be completely screened by architectural enclosures that are derived from or strongly related to the building's architectural expression, or enclosed within roof volumes.
- b. In the design of screening enclosures, use dimensional increments of window spacing, mullion spacing, or structural bay spacing taken from the facade composition. Materials, architectural styles, colors and/or other elements should strongly relate the screening to the building's architecture.
- c. The location, spacing, materials, and colors of down-spouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the facade and roof. Downspouts should be concealed within walls or located to harmonize with window spacing and facade composition.

F. COLOR

- 1. Consistent Color Palette.** A consistent color palette is recommended for the Maclay District, to ensure that new buildings are compatible with existing buildings. An example of the color range that falls in this palette is shown on the following page.
 - a. Variety across adjacent buildings is recommended to personalize each building, and to contribute to a vibrant neighborhood character. Lighter colors ranging from white to soft cream, yellow and deep beige, are recommended at primary building walls, as shown on the color palette that follows. Dark colors like deep brown or black should not be used as primary wall colors.
 - b. Accent colors can be used to highlight special architectural features such as building bases or wainscots, windows and window frames, railing, shutters, ornament, fences, and similar features. Secondary and accent colors may be stronger, and more saturated in hue than primary colors - accents of deeper reds and dark browns are recommended, as shown on the color palette that follows on page 113. At Spanish-influenced styles, accent colors should be a darker shade against a light-colored primary building wall. Fluorescent colors should not be used.
 - c. For tiled roofs, red and terra cotta colors are recommended. For shingle and other roof styles, grey or earth tones are recommended. Light colored roofs may also be used to reduce solar radiation; these should be screened from view by architectural enclosures such as parapet walls or other screening treatment.

COLOR PALETTE



5.5. THE WORKPLACE FLEX DISTRICT

A. PURPOSE

The purpose of the Workplace Flex District is to serve as the city's workplace district. It will be a place where the working elements of the City – the community's service areas, industrial, workshop, and creative office workplaces – coexist.

The workplace and mixed-use buildings that are envisioned for the Workplace Flex District should reflect their setting, along First Street, which provides a direct connection between Maclay Avenue's retail offerings, the Civic Center, and the Metrolink Station. Their design should be simple and dignified, appropriate to both the uses the buildings serve as well as to the First Street's workplace character. The Guidelines that follow will ensure that they reflect the working qualities of the Workplace Flex Distr

ict, as the area where the community will come to meet their needs. Buildings are recommended to be horizontal in both proportion and form, with a greater length than height. They are directed to have an appropriate level of articulation, with building volumes and architectural features serving to subdivide long building masses. Windows and other façade elements will give interest and expression to simple elevations.

B. BUILDING MASS AND INCREMENT

1. **Orientation.** Buildings should be sited to define the street edge of the First Street, by establishing a strong building wall along the street frontage.
 - a. Buildings should orient towards their primary street frontage and front the street. Buildings should not orient to parking lots at the sides or rears of buildings.
 - b. Building facades along the primary street frontage should contain the most articulated elevation of the building, with doors and windows that look onto the street. Frontages should be of a substantial scale and character, to appropriately define the "street wall" and create an inviting and comfortable pedestrian and bicycle experience along this important connection to the Metrolink Station.
2. **Horizontal Mass.** Buildings in the Workplace



The Workplace Flex District is the City's workplace district.



The Workplace Flex District has a number of vacant lots that provide opportunities for infill development.



Buildings should front the primary street, with parking lots to the side or rear of the site.



Buildings along the primary street frontage should be articulated with doors and windows, not blank facades like this building.



Buildings along the primary street frontage should be articulated with doors and windows, not blank facades



A live-work building where the massing is broken down into smaller house-scale volumes and the architectural elements reflect the scale of the street.

Flex District should be horizontal in proportion and form, and where possible should have a greater length than height. The horizontal length of the building should be articulated with building volumes, projections or other architectural elements to break up the larger building mass. These architectural subdivisions should occur at a maximum of every seventy-five (75) feet across the façade. Some methods of subdivision are noted below.

a. Vertical Architectural Features:

- i. Use large-scaled vertical piers, pilasters or columns to interrupt the length of a façade. These features should be a minimum of one and a half (1.5) feet wide to be readable from the street.
- ii. Use a slender tower form to accentuate a portion of the building.

b. Building Volumes and Massing:

- i. Use a horizontal volume that projects from the façade to emphasize an important aspect of the building or use, for example the primary building entry or a major display space.
- ii. Use a vertical volume that projects above the primary building mass.

c. Building Wall:

- i. Use detailing or a change in material to subdivide building bays - e.g., use brick framing to call out a building bay. Changes in material should always be accompanied by a change in plane.
- ii. Vary the setbacks of portions of the building wall along the front façade to create a varied front façade, i.e. repeated building bays that represent a series of workrooms along the façade.

d. Individualized Roof Forms:

- i. Use variation in roof forms to subdivide the building profile, by utilizing different forms over towers, bays or other building volumes.
- ii. Utilize a change in roof type (e.g., shed to gable) or orientation at special places along the façade.

3. Vertical Mass. Buildings in the Workplace Flex District may be subdivided across the horizontal plane, to create a base treatment that assists in visually establishing a human scale for pedestri-

an users and passers-by. When provided, base treatment should extend around all sides of a building visible from the street. A building base may be created by any of the following treatments:

- a. A visibly thicker and continuous base portion of the wall along the ground, where the wall above the base sets back, and openings within the base are seen to be more deeply recessed.
- b. A material and/or color change of the base wall relative to the building wall above. The base material should generally be heavier (e.g. of darker color and/or a stronger material), with a lighter quality at stories above (e.g. predominantly masonry at the ground, larger windows and more glass above).
- c. Pronounced architectural feature at the ground floor, such as an entrance volume, arcades, or a horizontal building projection.

4. **Corner Buildings.** Buildings located at the intersection of Hubbard Avenue should be designed to "mark" the corner on which they are sited and to create a gateway into San Fernando from the Metrolink Station, by acknowledging both street facades with façade articulation and detail. Recommended corner treatments may include:

- a. Creation of a landmark roof form, such as a dome, conical or pyramidal roof.
- b. Creation of a corner tower with a special roof.
- c. At minor intersections, a modest articulation of the building mass may be used to join the two street facades. Treatments may include:
 - i. A building protrusion or bay that "wraps" the corner.
 - ii. A corner entrance that protrudes or is cut-away from the corner.
 - iii. A change in roofline; e.g., a gabled end to "mark" the corner.

5. **Main Entrance.** The main entrance of a building should be located along the primary street façade of the building. Entrances should be designed to be consistent with the overall architectural style of the building.

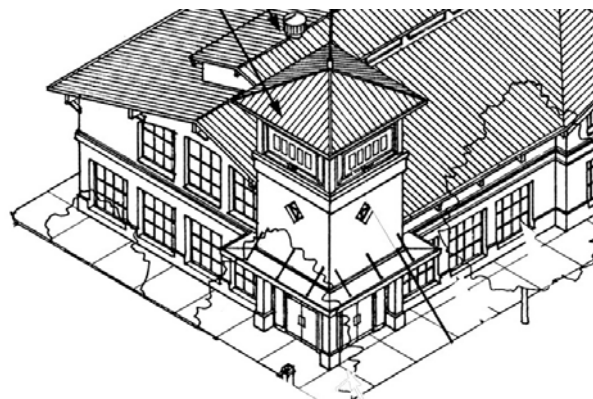
- a. The main entrance of a building should be located at the primary street façade of the building, and should be architecturally treated in a manner consistent with the building style.



Live-work units with the residential portion accessed by stoops.



A 3-story live-work building clad in brick over a cast-in-place concrete base.



Recommended corner treatments include the creation of a corner tower with a special roof.



The main entrance of a building should be located at the primary street façade of the building.



The main entrance to the upper floors of this building is highlighted by tile surround.

- b. At all buildings, entrances should be clear and easily identifiable, using one or more of the following treatments:
 - i. Indicated by a projection from the building façade, and covered by means of a portico (formal porch) projecting from or set into the building face;
 - ii. Indicated by a recessed entry. Recommended treatments include special paving materials; ornamental ceiling treatments; decorative light fixtures; and attractive decorative door pulls, escutcheons, hinges, and other hardware.
 - iii. Denoted by a single arch or series of arches to indicate entry. Arcaded entry porches or passageways are also recommended.
 - iv. Framed by special architectural elements, such as columns, archways, and overhanging roofs;
 - v. Denoted by a small roof overhang over the entrance, change in roofline or a major break in the surface of the subject wall;
 - vi. Marked by a taller mass above, such as a modest tower, or within a volume that protrudes from the rest of building surface.
- c. Where buildings include a mix of uses, entrances to upper story uses and to live-work uses should be clearly distinguishable in form and location from retail and workplace entrances, through the following treatments.
 - i. Accented by architectural elements that are “residential” in character, such as small windows above the door, sidelights, and ornamental light fixtures, front stoops or plantings.
 - ii. Indicated by a recessed entrance, i.e. a vestibule or lobby.
- d. At live-work buildings, multiple entrances are recommended on the front façade. Entrances should be coordinated with the architectural elements described in “Horizontal Mass”, above. The following elements are recommended for live-work entrances:
 - i. Raised stoops, open porches, and/or entrance vestibules to increase the privacy threshold between street and residential portion of the live-work unit. At attached residences, these should correspond to the vertical modules of units.

- ii. Low hedges, fences and/or entry gates to separate private front yards from the public sidewalk. Chain link fences should not be used.
- iii. Ornamental lighting along walks and drive-ways to highlight entrances and enhance security.
- iv. A rise in grade (of two to three feet) from the public roadway to the live-work unit, to protect the privacy of the residential uses.
- v. Special landscape materials to define front yard spaces and/or accent the entry sequence.

6. Accessory Buildings and Additions. Accessory structures include any structures subordinate to the primary building, such as garages, storage facilities and other ancillary buildings. Their design should be consistent with the prevailing architectural style of the primary structure, and should incorporate the following design components:

- a. The existing siding should be carried onto the addition or out-building.
- b. Accessory buildings should include articulation in the form of windows and doors, in the same style as the main structure.
- c. Out-buildings should follow the roof style of the main building. Additions should continue existing rooflines where possible.

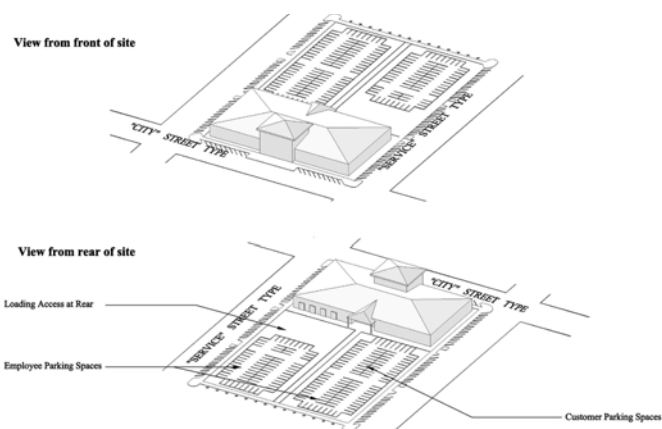
7. Loading and Service Entrances.

The visual impact of loading and services entrances should be minimized. Facilities should be incorporated into the overall composition of the façade.

- a. Service entrances and facilities, such as loading docks and storage areas, should be considered in the site layout. They should be sited to the side or rear of the building where possible.
- b. Portions of the building facade containing service or truck doors should be integrated into the architectural composition of the larger building facade design. Architectural treatments, materials, and colors should be extended from building facade areas into the facade portion containing truck doors.



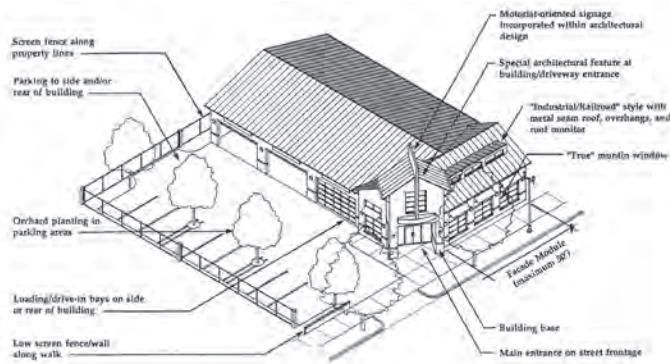
Vertically street-oriented windows arrayed in a traditional pattern.



Parking and loading entrances should be sited to the side or rear of the building.



Storage and trash areas should be sited to the rear of the building, and not be visible from the street.



Garage entrances should be sited to the side or rear of the building.



The large windows and simple landscaping of this light industrial building contribute to a pedestrian-friendly environment.



Storage and trash areas should be sited to the rear of the building, and not be visible from the street.

- c. Roll-up security doors should be detailed to conceal door housings and tracks, and provide an attractive and finished appearance for all exposed components.

8. **Vehicular Entrances.** Whenever possible, garage entrances should be located to the rear or side of the property to minimize visual impact to the street.

- a. Garages should be loaded from rear alleys where possible. Alleys are required to be well-lit.

9. **Parking Podiums.** Podiums should be considered part of the building base, with wall textures, colors, and dimensional modules that are coordinated with the rest of the building.

- a. No building may have more than one garage or podium entrance per streetfront.
- b. Podium entrances should not be located along First Street. When the only way to access podiums is along First Street frontage, garage entrances must be recessed behind the front wall of the building to minimize visual impact to the street, and should not exceed twenty (20) feet in width.
- c. Vehicle entrances should be treated with architectural articulation and landscape materials, to "mark" a frequently used common entrance for residents and guests. Treatments should include architectural frames or pergolas consistent with the architectural style of the building, decorative doorframe ornament, ornamental lighting, et cetera.
- d. Exposed podiums should not have blank concrete walls. Detailing and design, such as decorative scoring, concrete blocks with special surface textures (e.g., split-face block, combinations with precision face, etc.), integral color and/or inset tiles are recommended to provide additional surface articulation.

C. ARCHITECTURAL STYLE

The discussion that follows provides a "stylistic" framework for the design of new structures. The design guidelines below do not prescribe specific styles for new buildings. Rather, the guidelines are set up to allow for a range of architectural styles and types, so as to encourage creativity in design. The guidelines set up a framework for quality design by establishing a framework for a) good urban design relationships between buildings, and b) an assured level of quality in terms of construction.

No particular architectural style is recommended for the commercial and mixed-use corridor buildings in the Workplace Flex District. Buildings may reference existing styles along Truman Street and First Street, which include an eclectic mix of modern, traditional workplace and industrial building types to Spanish-influenced styles. In general, buildings should have a simple straight-forward character, with clean lines and detailing. Ornament should be used where appropriate, in a manner that is consistent with the overall building style. Each building should contribute to a unified, identifiable image for the district, through the use of similar and/ or complementary colors, materials and roof forms.

The following features are appropriate for use on buildings in the Workplace Flex:

- A restrained use of materials (i.e. usually one primary material), ranging from solid materials such as stucco, brick masonry, manufactured or natural stone, and precast concrete; to more visually dramatic materials such as architectural metal, glass and steel;
- Restrained building decoration, such as molded decoration, cornice lines, or articulation of the structural framework;
- Dark timberwork, or other use of accent materials;
- Flat roofs topped by decorative or capped parapets, pediments or cornices;
- Very low-pitched sloped or shed roofs; and
- Clay and concrete roof tiles, metal roofing.

D. FACADE COMPOSITION

1. **Building Base.** All buildings should create a base treatment that assists in visually establishing a human scale for pedestrian users and passers-by. Base treatment should extend around all visible sides of a building. A building base may be created by any of the following treatments:
 - a. A visibly thicker and continuous base portion of the wall along the ground, where the wall above the base sets back, and openings within the base are seen to be more deeply recessed.
 - b. A material and/or color change of the base wall relative to the building wall above. The base material should generally be heavier (e.g. of darker color and/or a stronger material), with a lighter quality at stories above (e.g. predominantly masonry at the ground, larger windows and more glass above).



Buildings may reference the Spanish-influenced styles for newer workplace and industrial buildings.



Materials ranging from stucco to brick masonry are appropriate for buildings in the Workplace Flex District.



A contemporary building with brick cladding and large architectural glass.



A new brick mixed-use office building next to an existing light industrial building.



A simple light industrial building clad in corrugated metal with large street-facing windows.

- c. Pronounced architectural feature at the ground floor, such as an entrance volume, arcades, or a horizontal building projection.

2. **Building Materials.** Simple wall surfaces are recommended for buildings along Truman Street and First Street. Articulation should be given through basic façade elements, such as deeply inset windows and doors or expression of the structural framework.

- a. *Primary materials* are those that clad the main building walls. Materials to be used as the primary cladding include:
 - i. Stucco: Stucco and cement plaster are acceptable finishes. Attention should be paid to detail and trim elements for a high quality installation. Highly textured surface textures are not recommended. The pattern of joints should be architecturally coordinated with the overall facade composition, and sealant colors should be coordinated with surface and other building colors.
 - ii. Brick: Full size brick veneer is preferable to thin brick tile. Rock veneer may also be used. Brick veneers should be mortared to give the appearance of structural brick. Brick veneer applications should use wrap-around corner and bullnose pieces to minimize a veneer appearance. An anti-graffiti coating is recommended.
 - iii. Wood: Horizontal sidings such as clapboard and tongue-in-groove, vertical siding such as board and batten, and other horizontal sidings such as smaller wood shingles and shakes may be suitable. The larger, more rustic styles of shingles and shakes should not be used. Trim elements should be used.
 - iv. Precast Concrete: Textures, pigments, and special aggregates should be used to create rich surfaces. Precast concrete copings and trim are recommended for use with other materials such as poured-in-place concrete, concrete block, brick, stone, stucco and EIFS. The location of joints between castings and expansion joints should be incorporated into the facade composition. Grout and sealant colors should be coordinated with castings and other building colors. An anti-graffiti coating is recommended.
 - v. Contemporary Materials such as Architectural Metal, Glass and Steel: Profile, corrugated, and other metal surfaces (i.e. sheet, rolled and extruded) should be detailed with adequate thickness to resist dents and

impacts. All materials should be maintained to ensure a quality appearance.

- b. *Accent materials* may be used as to add interest and variety at a more intimate scale, for example at window surrounds or other architectural framing. Accent materials include stucco, brick, wood, precast, metal glass and steel, as listed above, and also include:
 - i. *Ceramic tile*: Tile should be limited in use to a facade cladding or decorative wall accent material. Grout color should be coordinated with tile and other building colors.
 - ii. *Manufactured or Natural Stone, and Stone Veneers*: Natural stone is preferable; synthetic materials should be reviewed for quality appearance. Stone should be used as a base or as a special decorative material for wall panels or sills in combination with other materials, such as stucco, brick or concrete.
- c. *Base materials* are those used along the bottoms of building walls, and can be carried to vertical portions of buildings such as columns, pilasters, or piers, to impart a sense of permanence and solidity. Primary materials are often carried to the building base, but may also include:
 - i. *Poured-in-Place Concrete*: Concrete walls should generally be clad with stucco or other finish materials; poured concrete may be exposed as an architectural base or a site-work material. Where exposed, the location of formwork tie-holes, expansion joints and control joints should be incorporated into the facade composition. Textured form liners, pigments, stains, and special aggregates should be used to create rich surfaces. An anti-graffiti coating is recommended.
 - ii. *Concrete Block*: Concrete blocks of various block sizes, surface textures, and colors should be used as an architectural base or a sitework material; plain stack bond concrete block walls are not recommended. Decorative treatments should be used, such as alternating courses of differing heights, different surface textures (e.g., precision face and split face) and patterns of colored blocks; and cap and trim pieces should be used. Grout colors should be coordinated with block and other building colors. An anti-graffiti coating is recommended.



Brick is an appropriate materials to be used as the primary cladding.



The street-facing mass of this light industrial building provides ample street-facing windows and is articulated with canopies.



The street-facing mass of this light industrial building provides ample street-facing windows and is articulated with canopies.



The facade of this light industrial buildings incorporates the goods that are sold within: doors and windows of different sizes and shapes.



The continuous plate glass windows of this auto repair shop provide views into the repair space.



Brick light industrial buildings with large windows.

3. **Windows.** Windows should be organized to reflect and reinforce the architectural elements described in "Horizontal Mass", above, to create a recognizable composition across the façade.
 - a. Windows should comprise a minimum of twenty percent (50%) of the ground floor façade.
 - b. Where window openings are paneled, i.e. divided with multiple groups of vertical windows, true divided light windows or sectional windows are recommended. Snap-in muntins and those sandwiched within double-paned glass should not be used.
 - c. Window frames should not be set flush with walls. Glass should be inset a minimum of two (2) inches from the exterior wall and/or frame surface.
 - i. At deeply inset windows (greater than 4" from the exterior wall); the framing may be simple and relatively unarticulated. At shallower insets (2-4" from the exterior wall), projecting sills, molded surrounds, lintels and/or trim should be used to frame openings.
 - ii. Sills and surrounds should be proportioned to relate to the window size. For windows less than 48" in width, surrounds should not exceed 6" in width. For windows greater than 48" in width, surrounds should not exceed 8" in width.
 - d. Aluminum sliding windows should be designed to have substantial framing members, at a minimum width of two (2) inches.
 - e. Clear glass is recommended. Reflective glazing should not be used. Nonreflective films, coatings, low emissivity glass, and external and internal shade devices should be used for heat and glare control.
 - f. Deeply tinted glass or applied films should not be used. If tinted glazing is used, light tints and green, gray and blue hues are recommended.
 - g. Fritted glass, spandrel glass and other decorative treatments are recommended to add privacy and aesthetic variety to glass where desired.
4. **Doors.** At doors leading to workplace and commercial buildings, doors should be simple in style, with clean lines that are appropriate to the buildings' style.
 - a. High quality materials such as crafted wood, stainless steel, bronze, and other ornamental metals are recommended. Contemporary designs utilizing metal, glass, or other materials

derived from the building architecture may be appropriate.

- b. Doorways leading to upper story uses should be distinguishable from those leading to retail and workplace establishments.
- c. Doors should coordinated with architectural features that can give shelter from weather and sun, i.e. a projecting awning or canopy, or a permanent architectural awning.

5. Openings and Façade Elements. Buildings should maintain consistency across their façades. The overall effect of openings should create a harmonious pattern across the street wall.

- a. Window and door openings should create an ordered composition across the façade. Common window header line or sill line, and/or aligned vertical centerlines of windows and doors can serve as unifying elements across a façade.
- b. Buildings should not have large, blank or monotonous surfaces; designs should include sufficient detailing, texture, color differentiation and three-dimensional articulation to create appropriately scaled, interesting façades. Elements that add human scale, such as bay windows, entrance vestibules, porches, balconies, dormers, etc. should be used.

E. ROOFS

- 1. Roof Types.** Roof forms should be simple, with accents at entrances, corners, important building volumes and other architectural elements. However, no single roof form may extend for more than one hundred (100) feet in length, without incorporating a change in orientation, slope or roof type.
 - a. All continuous sloping roof forms (i.e., without flat horizontal portions) are recommended. These include pitched, gable, hip, and pyramidal roofs, which should be designed as follows:
 - i. Roof overhangs are recommended. Brackets and corbels (i.e., decorative supporting pieces designed to bear the weight of projected overhangs), or other expressed roof overhang supports are recommended to add richness to detailing. The spacing module of repeating supports should relate to the building's structural bay spacing.
 - ii. The soffit (i.e., the underside surface of the roof overhang) should be incorporated into the over-all architectural composition with



Doors should be simple in style, with clean lines that are appropriate to the building's style.



Window openings should create an ordered composition across the façade.



An industrial building with contemporary saw-tooth roof form



A contemporary building with skeleton frame canopy.



Canopy made of wooden slats on their sides providing shade but not protection from rain



A metal light industrial building with a gabled roof.

- beams, coffers, light fixtures and other design articulation.
- iii. Vertical roof edge fascia should be vertically sub-divided by additional horizontal layers, stepbacks, trim, and other detailing.
- b. Flat roofs with parapet walls should be treated with one or more of the following conditions:
 - i. An architecturally profiled cornice and/or expressed parapet cap should be used to terminate the top of parapet wall.
 - ii. Surface mounted cornices, continuous shading elements, or trellises should be used to strengthen a parapet wall design.
 - iii. An ornate parapet decoration, such as a pediment, may be used to add ornament to the roof form.
 - iv. A single layer, flush sheet metal parapet cap (i.e., a simple "inverted U" of sheet metal over the top of a parapet wall) without a substantial built-up edge should not be used, as these installations often display warped sheet metal (oil-canning) and a low-quality appearance. If used, sheet metal parapet caps should provide a formed (compound folded) overhanging edge termination and a heavy gauge sheet metal thickness selected to avoid oil-canning distortion.
- c. Special forms such as domes, conical roofs and pyramidal roofs are recommended at entry towers.

2. **Roof Materials.** Selection of roof materials should be made with consideration for the neighborhood context. Roof materials and color should be selected with consideration for views from above. Recommended roof materials include:
 - a. Clay, Terra Cotta or Concrete Tile: Projects using Mediterranean or Spanish Mission Revival architectural styles should use authentic terra cotta barrel tiles and avoid simulated products.
 - b. Asphalt, Slate or Cement/Slate-type Shingles: Projects using shingles should use the highest quality commercial grade materials, and be provided with adequate trim elements.
 - c. Corrugated and Standing-Seam Metal Roofing: The structural support detailing of corrugated metal roofing should insure that metal roof edges and panels will not sag, bend, or be vulnerable to impacts and denting. This is important at locations where undersides and edges of corru-

gated metal roofing are visible. Finishes should be anodized, fluorocoated or painted. Copper, zinc, and other exposable metal roofs should be natural or oxidized. Flat, unarticulated metal roof tiles and metal roof sheeting are not recommended.

- d. Tar and Gravel, Composition, or Elastomeric Roofs (flat roof locations): Use of these roof materials should be avoided at locations prominently viewable from nearby uphill residential neighborhoods. When used, these materials should be screened from view from adjacent buildings and sites by parapet walls.

3. **Equipment and Screening.**

- a. Roof mounted equipment such as cooling and heating equipment, antennae and receiving dishes should be completely screened by architectural enclosures that are derived from or strongly related to the building's architectural expression, or enclosed within roof volumes.
- b. In the design of screening enclosures, use dimensional increments of window spacing, mullion spacing, or structural bay spacing taken from the facade composition. Materials, architectural styles, colors and/or other elements from the facade composition should also be used to strongly relate the screening to the building's architecture.
- c. The location, spacing, materials, and colors of down-spouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the facade and roof. Down-spouts should be concealed within walls or located to harmonize with window spacing and facade composition.

F. COLOR

1. Consistent Color Palette. A consistent color palette is recommended for the district, to ensure that new buildings are compatible with existing buildings. An example of the color range that falls in this palette is shown on the following page.

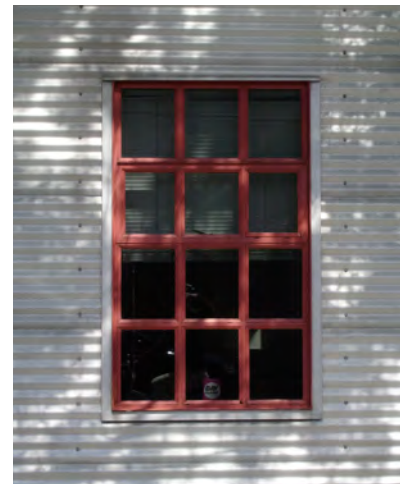
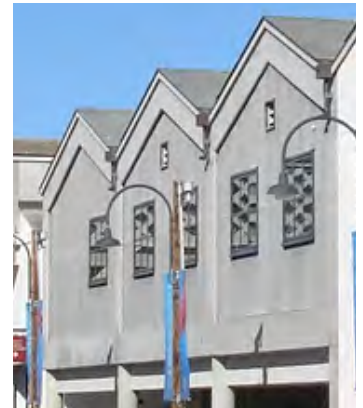
- a. Colors should be compatible with other buildings in the district. Cool colors like white and grey are recommended for primary building walls; deeper, warmer colors ranging from cream to brick red may also be used. Dark colors like brown or black should not be used as primary wall colors.

- b. Accent colors can be used to highlight special architectural features such as building bases, building entries, columns, cornices, capitals, and bands. Accent colors may either be a lighter shade than the primary building wall, or a stronger, more saturated hue. Fluorescent colors should not be used.
- c. For tiled roofs, red and terra cotta colors are recommended. For shingle and other roof styles, grey or earth tones are recommended. Light colored roofs may also be used to reduce solar radiation; these should be screened from view by architectural enclosures such as parapet walls or other screening treatment.



Corrugated and standing-seam metal roofing should be carefully detailed so that roof edges will not sag or bend.

COLOR PALETTE



5.6. THE GENERAL NEIGHBORHOOD DISTRICT

A. PURPOSE

The General Neighborhood District, which allows multi-family buildings up to four stories in height, occupies the parcels between Downtown San Fernando and the single-family neighborhoods to the north of Second Street. Accordingly, housing along Second Street must be designed to be compatible with the single-family houses – most of which are only one-story in height – across the street. The front portions of new buildings that directly front onto Second Street should be conceived as larger versions of the single-family homes across the street. Architecture should be residential in massing, scale, proportion, and detailing. Plantings and landscaped setbacks can increase the prominence and grandeur of the project, while giving residences more privacy from the public realm of the street. Residential entrances above street level can create a sense of privacy and distance from the street. Individual units should be organized in groups, as a part of a larger whole, to create buildings that are of a scale and character appropriate to a wide, frequently traveled road.

B. BUILDING MASS AND SCALE

1. **Building Site and Orientation.** Buildings should be sited to define the street edge.
 - a. Buildings should orient towards the street. Buildings should not orient to parking lots at the sides or rears of buildings.
 - b. Building facades along the primary street frontage should contain elevations activated by doors and windows that look onto the street.
 - c. Street-facing building masses should be of a substantial scale and character, reading as “grand mansions” or simply as larger versions of the City’s single-family homes.
2. **Massing and Scale.**
 - a. The portions of buildings located within 25 feet of the front setback line front directly onto Second Street should be designed per the following massing recommendations:
 - i. Buildings should be horizontal in massing, and where possible should have a greater length than height.



Second Street is currently lined predominantly by one-story, single-family houses.



This street-facing facade of this multi-family building contains street-facing windows and doors.



These multi-family units face and are accessed directly from the street.



The massing and scale of this multi-family building is consistent with the architectural character of the rest of the block.



AVOID: The massive, featureless bulk of the building violates the scale of its existing neighbor.

- ii. Buildings should be designed as simply massed volumes that are compatible in scale and form with surrounding buildings. Recommended methods include:
 - a) Matching existing building heights or exceeding them by only one story. When a two-story building is proposed next to existing one-story buildings, the second floor plate height should be no more than twice that of the existing one-story building.
 - b) Modulating taller buildings stories by stepping them down to provide one-story volumes/wings next to one-story buildings.
 - c) Matching the prevalent eave height of existing buildings on the street. The eave height of new one-story buildings should approximate the prevalent eave height of the existing buildings on the street
 - d) Including elements such as porches, galleries, arcades, etc. to relate the scale of facades to those of existing buildings. Eave heights of these frontage elements should approximate the prevalent eave height of the existing buildings on the street.
 - e) Reducing building bulk by introducing dwelling space in attic spaces of pitched roof buildings and providing natural light with dormer windows or gable windows. Mansard roofs are not permitted.
 - f) Subdividing the overall mass of buildings into modules that express the individuality of each unit, or group of units. Modules should occur at a maximum of every fifty (50) feet across the façade.
 - g) Designing buildings as simple assemblies of house-scale forms that face and are accessed from the street.



A 3-story multi-family housing project fits into the existing 1-story building fabric by placing the third-story within an attic space that is lit by dormer windows.

- iii. To be compatible in scale and form with adjacent one- or two-story buildings, portions of buildings fronting adjacent to should consider:
 - a) Modulating side yard and rear yard setbacks to provide as much distance as possible between the facades of a proposed building and existing buildings in order to preserve the privacy of the outdoor spaces of both.
 - b) Orienting side yard open spaces of proposed buildings to face the side yard open spaces, where present, of adjacent buildings.
 - c) Introducing landscape and/or trees as a screen between existing and new buildings.
- b. Excessive massing breaks, roof breaks and complicated hipped or gabled roof forms should be avoided.

3. **Main entrance.** The main entrance of a building should be located along the primary street façade of the building. Entrances should be designed to be consistent with the overall architectural style of the building.

- a. Building entrances should front onto the street, and be prominent and easy to identify, using one or more of the following treatments:
 - i. Marked by a taller mass above, such as a modest tower, or within a volume that protrudes from the rest of building surface;
 - ii. Indicated by a projection from the building façade, and covered by means of a porch or portico that projects from the building face;
 - iii. Indicated by a recessed entry—recommended treatments include special paving materials such as ceramic tile; ornamental ceiling treatments such as coffering; decorative light fixtures; and attractive decorative door pulls, escutcheons, hinges, and other hardware;
 - iv. Denoted by a single arch or series of arches to indicate entry—arcaded entry porches or passageways are also recommended.
 - v. Framed by special architectural elements, such as columns, archways, and overhanging roofs;
 - vi. Emphasized by a small roof overhang over the entrance, change in roofline or a major break in the surface of the subject wall.



Upper floors are incorporated into the attic spaces and are daylit by dormer windows.



This multi-family building is subdivided into modules that express the individuality of each unit.



Units to this multi-family courtyard building are accessed through an inviting, covered passage.



A low wall and landscaping separates the private front yard of this unit from the common area.



The garages of this multi-family building are oriented towards the alley rather than to the street.

- b. At residential buildings, multiple entrances are recommended on the front façade. Where possible, entrances should be included within each module of units described in “Horizontal Mass”, above in Section 2. The following elements are recommended for residential entrances:
 - i. Raised stoops, open porches, and/or entrance vestibules to increase the privacy threshold between street and residence. At attached residences, these should correspond to the vertical modules of units.
 - ii. Low hedges, fences and/or entry gates to separate private front yards from the public sidewalk. Chain link fences are not permitted. (See Neighborhood District Development Standards, Section 7.3 Landscaping and Screening.)
 - iii. Ornamental lighting of porches, along walks and driveways to highlight entrances and enhance security.
 - iv. A rise in grade (of two (2) to three (3) feet) from the public roadway to the residence, to protect the privacy of residential units.
 - v. Special landscape materials to define front yard spaces and/or accent the entry sequence.

4. Accessory Buildings and Additions.

Accessory structures include any structures subordinate to the primary building, such as garages, storage facilities and other ancillary buildings. Their design should be consistent with the prevailing architectural style of the primary structure, and should incorporate the following design components:

- a. The existing siding should be carried onto the addition or out-building.
- b. Accessory buildings should include articulation in the form of windows and doors, in the same style as the main structure.
- c. Out-buildings should follow the roof style of the main building. Additions should continue existing rooflines where possible.

5. Residential Garages and Vehicular Entrances.

Where possible, garage entrances should be located to the rear or side of the property to minimize visual impact to the street.

- a. Garages should be loaded from rear alleys where possible. Alleys are required to be well-lit.
- b. The design of the garage door should relate to the particular architectural style selected. Garage doors should appear to be set into the walls rather than flush with the exterior wall, and carriage style garage doors are recommended where compatible with architectural style.
- c. Single-car garage doors are strongly recommended to avoid a car-dominated appearance on the facade. Where double car widths are used, doors may not exceed a width of twenty (20) feet maximum, and elements such as trellises should be used to subdivide the width of the door.

6. Parking Podiums. Podiums should be considered part of the building base, with wall textures, colors, and dimensional modules that are coordinated with the residential architecture.

- a. No building may have more than one (1) garage or podium entrance per streetfront.
- b. Exposed podiums should not have blank concrete walls. Podium wall textures, colors, and dimensional modules should be coordinated with those of the residential architecture above the podium. Detailing and design, such as decorative scoring, concrete blocks with special surface textures (e.g., split-face block, combinations with precision face, etc.), integral color and/or inset tiles are recommended to provide additional surface articulation.
- c. Podium entrances should not be located along Second Street. When the only way to access podiums is along the Second Street frontage, garage entrances must be recessed behind the front wall of the building to minimize visual impact to the street, and should not exceed twenty (20) feet in width.
- d. Vehicle entrances should be treated with architectural articulation and landscape materials so as to identify a frequently used common entrance for residents and guests. Treatments should include architectural frames or pergolas consistent with the architectural style of the building, decorative doorframe ornament, ornamental lighting, et cetera.



An arched opening provides vehicular access from the street to tuck-under parking garages within the center of the lot.



The arched opening to the parking of this multi-family building is coordinated with the architecture of the rest of the building.



A Spanish Colonial Revival multi-family building with a courtyard as the shared open space.



A multi-family building in the Monterey style with a second floor balcony.



Multi-family residential building in the Craftsman style. Massing, porches, structural elements, windows, materials, trim, and landscape are true to the style's rules.



A shared courtyard with a fountain as its focal point.

C. ARCHITECTURAL STYLE

The discussion that follows provides a “stylistic” framework for the design of new structures. The Design Guidelines below do not prescribe specific styles for new buildings. Rather, the guidelines are set up to allow for a range of architectural styles and types, so as to encourage creativity in design. The Guidelines set up a framework for quality design by establishing a framework for a good urban design relationships between buildings and an assured level of quality in construction.

Residential influences in San Fernando are eclectic, ranging from Spanish-inspired styles to east coast influences. New residential buildings should build upon these roots, and draw from the broad menu of residential styles the city has to offer. These include Mission, Mediterranean, Spanish Colonial Revival, and Monterey Mediterranean styles; as well as Southern California variations on the Craftsman, bungalow and various Victorian styles. Below are some of the primary features found in each architectural style:

1. Elements of Mission architecture.

- Plain, smooth stucco siding
- Large square pillars and twisted columns
- Timberwork, wood framing and balustrades
- Bell or corner towers
- Sloping, low-pitched or hipped roofs or flat roofs with parapets.
- Red roof tiles, wood shingles or clay tiles.

2. Elements of Spanish Colonial Revival architecture.

- Stucco, brick, wood, or combinations of these materials.
- Little or no overhanging eaves
- Deeply inset windows within thick stucco walls
- Arches, especially above doors, porch entries and main windows
- Decorative ironwork, particularly at balconies, porches and on roof forms.
- Courtyards, porches, pergolas and other shaded or sheltered outdoor areas
- Red tile roofs

3. Elements of Mediterranean architecture.

- Asymmetrical shape with cross-gables and side wings
- Carved doors
- Ornate detailing including molded decoration, carved wood and stonework, or cast ornament
- Spiral columns and pilasters
- Carved stonework or cast ornaments
- Patterned tile floors and wall surfaces
- Flat roof and parapets, or a hipped roof

4. Elements of Monterey style.

- Paneled doors with sidelights
- Double-hung windows with mullions
- Ornate wood spindlework
- Projecting continuous balconies or porches on upper-stories
- Wooden verandas
- Low pitched, hipped or gabled roofs, often covered with shingles

5. Elements of the Craftsman style.

- Full- or partial-width porches
- Pedestal-like, tapered columns
- Overhanging eaves and exposed roof rafters
- Low-pitched gabled roof
- River rock exterior elements
- Horizontal wooden clapboard siding
- Smooth stucco or concrete building exterior

6. Elements of the California Bungalow house.

- An offset entryway
- A projecting bay on the façade
- Large front porch with square columns
- One or one and a half stories
- Low-pitched roof
- River rock exterior elements
- Horizontal wooden clapboard siding
- Smooth stucco or concrete building exterior



A senior housing project with a contemporary interpretation of the Queen Anne style.



A senior housing project with Queen Anne style decorative eave elements.



No single roof form may extend for more than 100 feet without incorporating a change in roof type.



A Mediterranean style multi-family building with stucco as the primary cladding material.



A multi-family building clad in brick.

7. Elements of the Victorian (Queen Anne and Eastlake) style.

- Asymmetrical facades
- Elaborate spindlework ornamentation
- Corner or curved towers
- Extensive, wrap around porches on the first floor
- Surfaces with a variety of patterning, i.e. clap-board or patterned shingles
- Protruding bay windows
- Steeply pitched roofs

8. Elements of the Streamline Moderne style.

- Horizontal building orientation
- Technological and nautical themes / references
- Smooth, rounded building corners
- White or light in color
- Long bands of windows
- Rounded edges, corner windows, and glass block walls

D. FACADE COMPOSITION

1. Wall Materials.

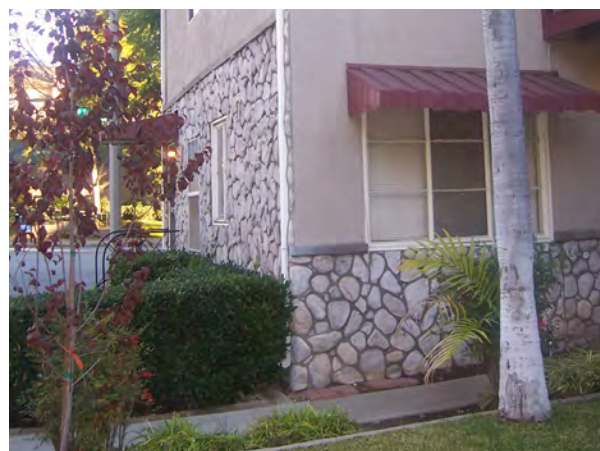
- a. *Materials.* A variety of detail is recommended, to contribute to a neighborhood character, where each building has its own, highly personalized detail and design elements. Where appropriate, combinations of surfaces and textures may be used to achieve this variety.
 - i. Primary materials are those that clad the main building walls. Materials to be used as the primary cladding include:
 - a) Stucco: Stucco, cement plaster or stucco-like finishes are acceptable finishes. Attention should be paid to detail and trim elements for a high quality installation. Highly textured surface textures are not recommended. The pattern of joints should be architecturally coordinated with the overall facade composition, and sealant colors should be coordinated with surface and other building colors.
 - b) Brick: Full size brick veneer is preferable to thin brick tile. Brick veneers should be mortared to give the appearance of structural

brick. Brick veneer applications should use wrap-around corner and bullnose pieces to minimize a veneer appearance. An anti-graffiti coating is required.

- c) Wood: Horizontal sidings such as clapboard and tongue-in-groove, vertical siding such as board and batten, and other horizontal sidings such as smaller wood shingles and shakes may be suitable. The larger, more rustic styles of shingles and shakes should not be used. Trim elements should be used, and traditional Craftsman styling such as timber detailing and exposed bracing are recommended.
- ii. Accent materials may be used as to add interest and variety at a more intimate scale, for example at porches, or at window surrounds or other architectural framing. Accent materials include stucco, brick and wood, as listed above, and also include stone and stone veneers. Stone should be used only as a base or as a special decorative material for wall panels or sills in combination with stucco or EIFS materials.
- iii. Base materials are those used along the bottoms of building walls, and can be carried to vertical portions of buildings such as columns, pilasters, or piers, to impart a sense of permanence and solidity. Primary materials are often carried to the building base, but may also include:
 - a) Precast Concrete: Textures, pigments, and special aggregates should be used to create rich surfaces. Precast concrete copings and trim are recommended for use with other materials such as poured-in-place concrete, concrete block, brick, stone, stucco and EIFS. The location of joints between castings and expansion joints should be incorporated into the facade composition. Grout and sealant colors should be coordinated with castings and other building colors. An anti-graffiti coating is required.
 - b) Poured-in-Place Concrete: Concrete walls should generally be clad with stucco or other finish materials; poured concrete may be exposed as an architectural base or a site work material. Where exposed, the location of formwork tie-holes, expansion joints and control joints should be incorporated into the facade composition. Textured form liners, pigments, stains, and special aggregates should be used to create rich surfaces. An anti-graffiti coating is recommended.



Brick veneer wraps the corner and is detailed to look like load bearing masonry.



AVOID: Stone veneer that does not wrap the corner gives away the fact that is an applied veneer.

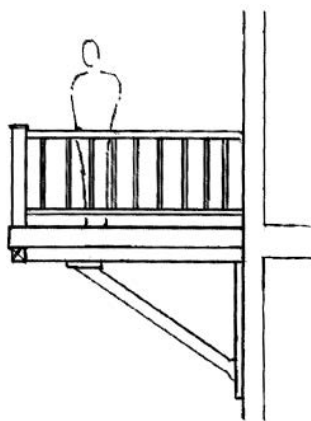


The vertical joints between the board and batten siding and stucco of this building occurs at the inside corner.

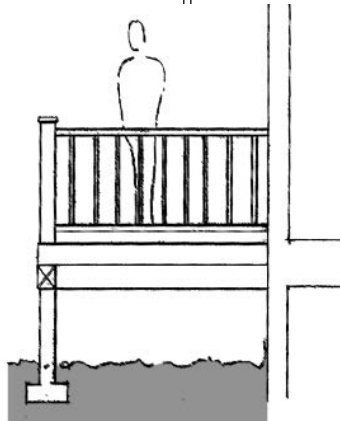


Lighter weight materials are placed above more substantial materials, in this case wood above stone

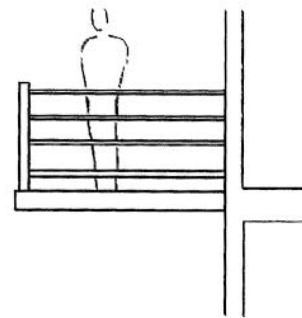
- c) **Concrete Block:** Concrete blocks of various block sizes, surface textures, and colors should be used as an architectural base or a site work material; precision concrete block walls are not recommended. Decorative treatments should be used, such as alternating courses of differing heights, different surface textures (precision face and split face) and patterns of colored blocks; and cap and trim pieces should be used. Grout colors should be coordinated with block and other building colors. An anti-graffiti coating is recommended.
- iv. Materials to avoid or to be kept to a minimum include:
 - a) Simulated finishes such as artificial stone.
 - b) Plywood siding.
 - c) EIFS (Exterior Insulation & Finish System) on exposed, ground level locations.
 - d) Reflective materials, such as mirrored glass, shiny metal, and chrome are prohibited.
- b. **Configurations.**
 - i. Two or more wall materials may be combined on one facade as follows:
 - a) If located one above the other, lighter weight materials shall be placed above more substantial materials (e.g. wood above stucco or masonry, or stucco and glass above masonry) as shown at left.



Visibly supported



Visibly supported



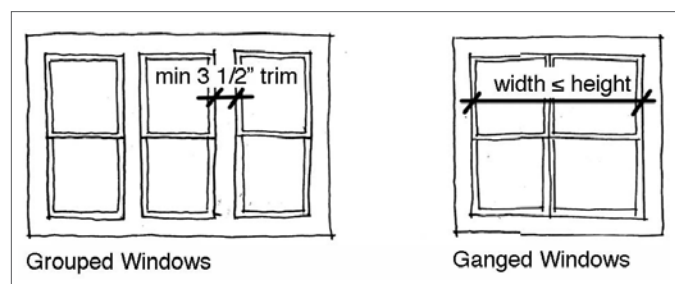
AVOID: NOT visibly supported

Visible support of existing elements.

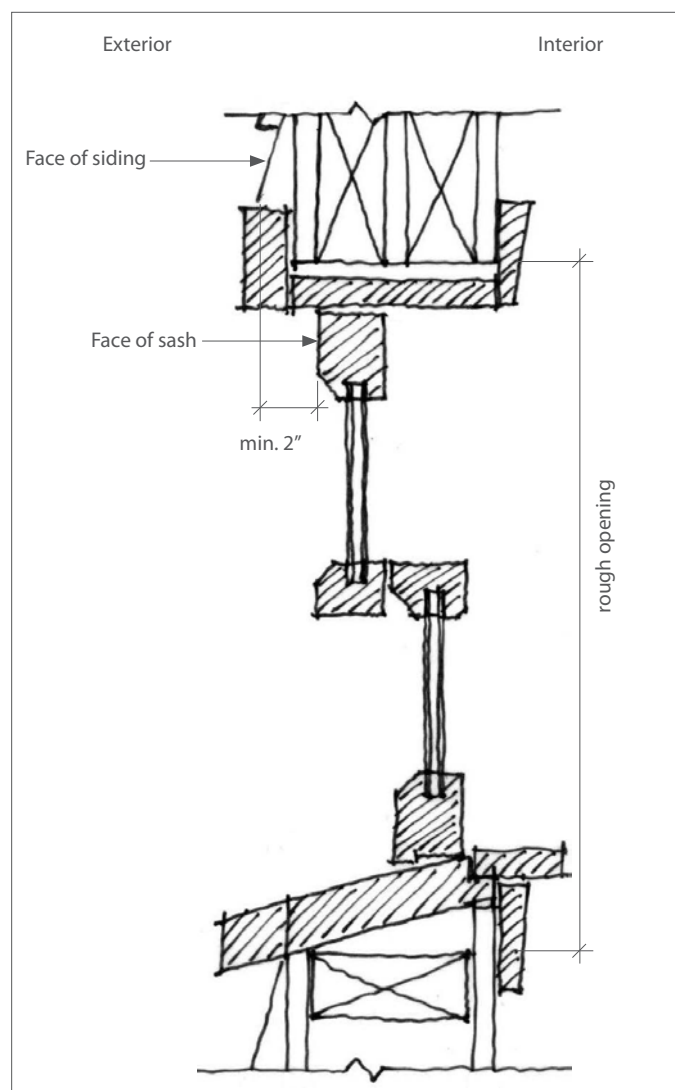
- b) Vertical joints between different materials shall occur only at inside corners.
- ii. All building elements that project from the building wall by more than 16 inches – including but not limited to decks, balconies, porch roofs and bay windows – should be visibly supported by brackets, posts, or beams that are sized at minimum six inches in nominal width or diameter as shown below.
- iii. Exterior chimneys should be finished in brick, stone, or stucco.
- iv. Walls clad in wood or cement fiber board siding should be stained or painted.
- v. The undercroft of decks and porches should be enclosed with lattice or vertical pickets.

2. Windows. Windows should be grouped so that they recognizably belong to a building module or volume, and create a recognizable composition within each unit with a clear hierarchy of major and minor windows, rather than being repeated uniformly across a wide façade with multiple components.

- a. At residential ground levels, windows should be designed and oriented so as to preserve privacy for ground floor units, and should comprise a minimum of fifteen (15%) percent of the building wall area.
- b. At upper stories, windows should comprise a minimum of fifteen (15%) percent of each floor's façade wall surface area.
- c. Buildings should include vertically proportioned façade openings; with windows that have a greater height than width (an appropriate vertical/horizontal ratio ranges from 1.5:1 to 2:1).
- d. Where window openings are paneled, for example divided with multiple groups of vertical windows, true divided light windows or sectional windows are recommended. Snap-in muntins and those sandwiched within double-paned glass should not be used.
- e. Window frames should not be set flush with walls. Glass should be inset a minimum of two (2) inches from the exterior wall and/or frame surface.
 - i. At deeply inset windows (greater than 4" from the exterior wall); the framing may be simple and relatively unarticulated. At shallower insets (2-4" from the exterior wall), projecting sills, molded surrounds, lintels and/or trim should be used to frame openings.



Window configurations.



Typical window detail.



The windows of this townhouse building are flanked by shutters.



A Craftsman-style building with windows ganged together with a mullion separating the two windows.



Doors should be simple in style, with clean lines that are appropriate to the buildings' style.

- ii. Sills and surrounds should be proportioned to relate to the window size. For windows less than 48" in width, surrounds should not exceed 6" in width. For windows greater than 48" in width, surrounds should not exceed 8" in width.
- f. Special Windows – Individual elements such as bays or dormers should be used to add interest and a domestic character to the facade. Decorative treatments on windows or balconies, such as wood or metal grilles on windows or balconies, wood balcony columns and balustrades, and simple detailed trim are recommended.
- g. Aluminum sliding windows should not be used.
- h. Clear glass is recommended. Reflective glazing should not be used. Nonreflective films, coatings, low emissivity glass, and external and internal shade devices should be used for heat and glare control.
- i. Deeply tinted glass or applied films should not be used. If tinted glazing is used, light tints and green, gray and blue hues are recommended.
- j. Fritted glass, spandrel glass and other decorative treatments are recommended to add privacy and aesthetic variety to glass where desired.

3. Doors.

Doors should match or complement the materials, design and character of the primary building; for example ornate carved doors at Mediterranean style homes, and simply styled doors with subdued ornamentation at Craftsman style residences.

- a. High quality materials such as crafted wood, stainless steel, bronze, and other ornamental metals are recommended.

4. Openings and Façade Elements.

Other design elements may be used in coordination with windows and doors, to create a consistent effect of openings across the facade wall. Openings and façade elements should be organized along the façade so that each grouping is recognizable as belonging to an individual unit or module of units.

- a. Façade elements should create an ordered composition across the building façade, to create a recognizable grouping of elements that defines each individual unit or module within the larger building.

- b. Buildings should not have large, blank or monotonous surfaces except when such façade wall areas are used in contrast to concentrated detail in other areas of the façade, as in Spanish architecture styles. Designs should include sufficient articulation, such as bay windows, entrance vestibules and dormers, to create appropriately scaled, interesting facades.
- c. Alcoves, balconies, porches or other indoor-outdoor elements should be used to provide outdoor spaces for upper story tenants, and to articulate the unit on the façade. Balconies should be designed as individual elements; run-on or continuous balconies that extend across the length of a façade should not be used except where integral to a building's architectural style such as with Monterey Style.
- d. Special architectural features should be used to create articulated, interesting facades that look custom-made for each individual building, rather than mass produced for a complex or development. These include features such as recessed windows with authentic muntins, architectural trim with substantial depth and detail, bay windows, window boxes, dormers, entry porches, et cetera.

E. ROOFS

1. **Roof Types.** Buildings should use a variety of roof forms on each building, to accentuate the fine grain of the neighborhood-scaled district and to denote individual units where possible. No single roof form may extend for more than sixty (60) feet in length, without incorporating a change in orientation, slope or roof type.
 - a. All continuous sloping roof forms (i.e., without flat horizontal portions) are recommended. These include pitched, gable, hip, and pyramidal roofs, which should be designed as follows:
 - i. Roof overhangs are recommended. Brackets and corbels (i.e., decorative supporting pieces designed to bear the weight of projected overhangs), or other expressed roof overhang supports are recommended to add richness to detailing. The spacing module of repeating supports should relate to the building's structural bay spacing.
 - ii. The soffit (i.e., the underside surface of the roof overhang) should be incorporated into the overall architectural composition with beams, coffers, light fixtures and other design articulation.



A Craftsman style building with asphalt roof shingles and trim elements conducive to the architectural style.



The sculpted parapet of this Spanish Colonial Revival style building is accented with potted plants.



A Mediterranean style building with terra cotta roof tiles.

- iii. The vertical edge of the roof should be detailed to demonstrate additional horizontal layers, step-backs, trim, and other detailing.
 - b. If used, flat roofs should always be edged with parapet walls; and softened with residential accessories such as shading elements, or trellises.
2. **Roof Materials.** Selection of roof materials should be made with consideration for the neighborhood context. Roof materials and color should be selected with consideration for views from above. Recommended roof materials include:
- a. Clay, Terra Cotta or Concrete Tile: Tile roofs are recommended wherever sloping roof forms are



A Queen Anne style inspired building with blue walls and white trim.



A white Mediterranean style building with painted base.

used. Projects should use authentic terra cotta 2-piece barrel tiles, and avoid simulated products. A double row of tiles should be used to terminate the roof at the edge of rooflines.

- b. Asphalt, Slate or Cement/Slate-type Shingles: Projects using shingles should use the highest quality commercial grade materials, and be provided with adequate trim elements.
- c. Tar and Gravel, Composition, or Elastomeric Roofs: These roof materials should be limited to flat roof locations, and should be screened from view from adjacent buildings and sites by parapet walls. They should be avoided where prominently viewable from adjacent multi-story buildings or nearby uphill areas.

3. Equipment and Screening.

- a. Roof mounted equipment such as cooling and heating equipment, antennae and receiving dishes should be completely screened by architectural enclosures that are derived from or strongly related to the building's architectural expression, or enclosed within roof volumes.
- b. In the design of screening enclosures, use dimensional increments of window spacing, mullion spacing, or structural bay spacing taken from the facade composition. Materials, architectural styles, colors and/or other elements should strongly relate the screening to the building's architecture.
- c. The location, spacing, materials, and colors of downspouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the facade and roof. Downspouts should be concealed within walls or located to harmonize with window spacing and facade composition.

F. COLOR

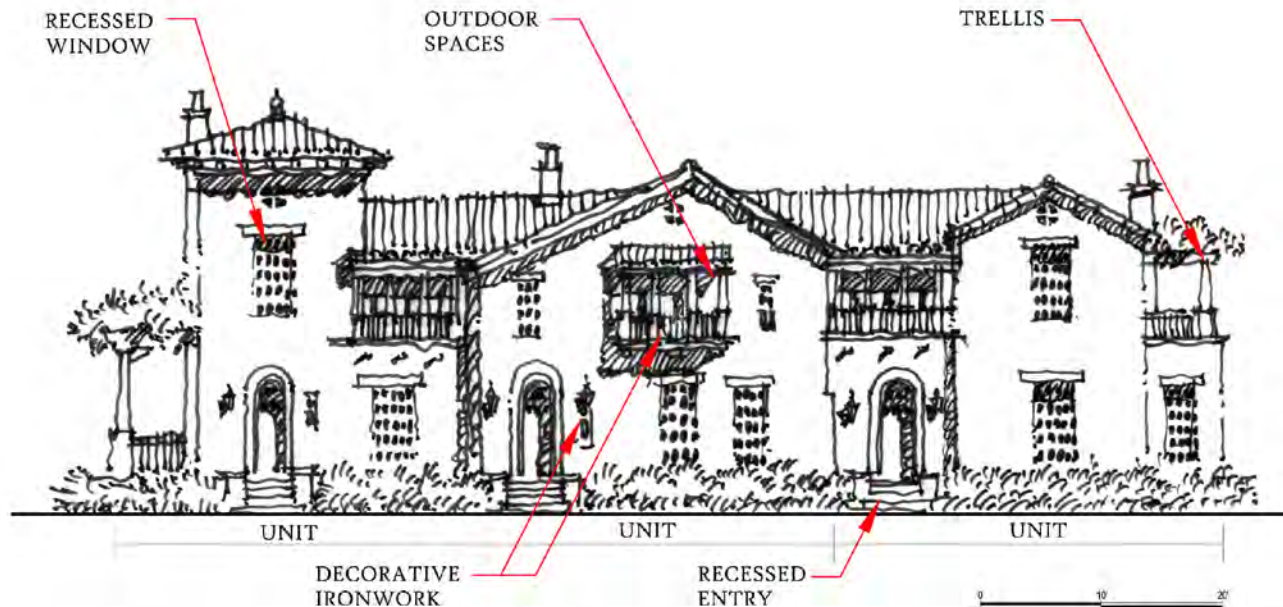
1. Consistent Color Palette. A consistent color palette is recommended, to ensure that new buildings are compatible with existing buildings. An example of the color range that falls in this palette is shown on the following page.

- a. Variety across adjacent buildings is recommended to personalize each building, and to contribute to a vibrant neighborhood character. Lighter colors ranging from white to soft cream, yellow and deep beige, are recommended at primary building walls, as shown on the color palette that follows. Dark colors like deep brown or black should not be used as primary wall colors.
 - b. Accent colors can be used to highlight special architectural features such as building bases or wainscots, windows and window frames, railing, shutters, ornament, fences, and similar features. Secondary and accent colors may be stronger, and more saturated in hue than primary colors - accents of deeper reds and dark browns are recommended, as shown on the color palette that follows. At Spanish-influenced styles, accent colors should be a darker shade against a light-colored primary building wall. Fluorescent colors should not be used.
3. For tiled roofs, red and terra cotta colors are recommended. For shingle and other roof styles, grey or earth tones are recommended. Light colored roofs may also be used to reduce solar radiation; these should be screened from view by architectural enclosures such as parapet walls or other screening treatment.

COLOR PALETTE



ARCHITECTURAL DETAILS



RECESSED WINDOWS PROVIDE SHADE AND EVOKE TRADITIONAL ARCHITECTURE



TOWERS PRESERVE TRADITIONAL ARCHITECTURAL STYLE



TRELLISES AT THE END OF BUILDINGS SOFTEN THE TRANSITION TO LOWER BUILDINGS



DETAILED IRONWORK RECALLS THE CRAFTSMANSHIP OF HISTORIC SAN FERNANDO ARCHITECTURE



ENTRANCES ARE STEPPED UP AND RECESSED TO INCREASE PRIVACY



DETAILED COLUMNS & BRACKETS ADD TO THE AUTHENTIC LOOK OF A BUILDING

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5.7. SIGNAGE - ALL DISTRICTS

A. DISTRICT ORIENTATION

A district's character is defined by the scale and intensity of its development, its uses and building architecture, and the quality of its public spaces. Building signage provides an opportunity to give visitors visual clues about the district the building is located within, while simultaneously conveying information about the businesses it advertises.

In general, sign design within each district should be unified in some ways, and unique in others. Signs throughout a district should be *unified* in that they should be compatible with district character. They should share the common themes of that District, and maintain similarities in terms of alignment, proportion, size and number of signs. Signs within a district should be *unique* in that each sign should be expressive of the individual store or establishment's identity, and appropriate to the type of activity contained within the establishment. For example, signs identifying business services should convey something very different from those advertising entertainment establishments, and may differ in terms of type, materials and color.

1. **The Maclay District.** The Maclay District is primarily a residential neighborhood, and signage for its non-residential uses should be compatible with this character. In order to maintain residential compatibility, signs in this district are limited to building-mounted and wall signs. "Neighborhood Services Overlay Areas" are intended to be pedestrian centers for their neighborhoods; therefore signage in these centers should be visible to residents who walk from nearby neighborhoods as well as to customers driving by, and follow the general character prescribed for the Downtown District (below).
2. **The Downtown District.** The Downtown District is the "center of the city" for the neighborhoods of San Fernando. This district will be the meeting place for San Fernando's community, and its sidewalks will serve as the city's living room. Thus, although the primary orientation of signage in this district should be towards the pedestrian, signage should be also be visible from vehicles. Because of the pedestrian nature of the District, signs will be seen from close view, and a high level of detail and craftsmanship should be used.



Signs in a district should be compatible with each other, yet distinctive for each individual store.



Signage should be carefully crafted with quality materials, as in this combination of wrought iron and painted wood.



Signage can be oriented to both the pedestrian and the vehicle, as demonstrated along this streetscape.



Signage should be incorporated into building architecture, like this sign located on a corner tower.



Signage should generally be located at the first floor level, especially at pedestrian-oriented districts.

3. **The Mixed-Use Corridor District.** The Mixed-Use Corridor District is intended to be a pedestrian extension of the Downtown District, and signage in this area should be pedestrian-oriented, following the general character described for the Downtown District.
4. **The Auto Commercial District.** Where free-standing signs are used, they should not be excessively auto-oriented; new pole-mounted signs and billboard advertising are not appropriate.
5. **The Workplace Flex District.** The Workplace Flex District supports the continued functioning and expansion of the City's light industrial, workshop, and large-scale commercial sectors. It also accommodates live-work uses, subject to a conditional use permit. The Workplace Flex District provides a framework for creating a more inviting pedestrian, bicycle, and vehicular connection along First Street between the Metrolink Station and Maclay Avenue's "main street" the Civic Center, and along Truman Street between the Metrolink Station and the City Center. Signage should be visible from vehicles and should also be oriented towards pedestrians and in general, should be designed as part of the building's architecture, incorporated into the building or located on prominent architectural features.

B. DESIGN GUIDELINES

1. **Architectural Compatibility.** Signs should be coordinated with building architecture, using complementary and consistent forms, shapes, materials, colors and lighting. They should relate to the primary building by using complementary and consistent forms, shapes, materials, colors and lighting. They may also reference existing building styles such as Mission, Spanish Colonial Revival, and Mediterranean architectural styles.
 - a. Within pedestrian-oriented shopping areas (i.e. the Downtown District, "Neighborhood Services Overlay Areas" and the Mixed-Use Corridor Sub-District), signs should be well-crafted and incorporate a high degree of detail, as they will be read at close range.
 - b. Along highly traveled corridors (i.e., the Workforce Flex and Maclay Districts), signage

should be incorporated into the building's architecture, and not be designed as unrelated elements attached to the building. Architectural elements such as building bays or protrusions, corner towers and oversized entrances are appropriate locations for large-scale signage.

2. **Sign Location and Placement.** The location and position of all permanent signs should be incorporated into the architectural design of the building. Placement of signs should be considered part of overall façade design of the building.
 - a. Signs in all districts should work at two scales: they should be visible to customers on foot and to those passing by in a car. Optimal viewing height from both the pedestrian and the automobile perspective is generally less than twenty (20) feet high.
 - b. Signs should typically be located at the first floor level but may be located above the second story if identifying upper story uses.
 - c. Building-mounted signs should be located within the "signable wall area" – a sign band or other portion of building above the storefront that is unbroken by windows, pilasters, detailing or other architectural elements.
 - d. Architectural elements on the building façade should be used to "frame" signs, including moldings, arches, clerestory windows, cornice lines and other features of the tenant storefront. Signage should not overlap or hide architectural elements such as columns, pilasters, cornices or other trim.
 - e. Window signs should not obscure primary views in to and out from the storefront.
 - f. Monument-type and other non-building mounted signs should be placed within a landscaped area along the building frontage, perpendicular to approaching traffic and positioned to provide clear lines of sight at intersections and driveway approaches. Sign locations should be chosen with respect to pedestrian and ADA accessibility (see *Development Standards* for the appropriate District).
3. **Design.** Sign design should be appropriate to the establishment, using font, color, and graphic images to convey a sense of what "type" of business is being advertised. The handcrafted look is encouraged, and tasteful use of materials, such as painted wood or signs cut out of metal, is recommended.



Signage should NOT overlap architectural features, in the way this sign overlaps the brick facade shown here.



Directory signs should be placed along the building frontage and out of the public right-of-way, like this sign.



Sign design should convey something about the nature and the character of the business it identifies.



Structural supports for projecting signs should be coordinated with building architecture.



Individual letters or sign panels may be mounted on the canopy above the fascia.



Adhesive "stick-on" letters should not be used.



Clerestory windows located above the storefront can provide locations for signage.

a. Wall Signs.

- i. Where individual letters are used, letters should be three dimensional, created by raised letter forms mounted to the building façade or sign panel, or by incised openings cut-out from the sign panel.
- ii. Where painted letters are allowed and used, the sign message should present a neat and aligned appearance. The services of a professional sign painter are strongly recommended.

b. Projecting Signs.

- i. Projecting signs may be attached to building walls or to architectural elements such as archways, trellises, and entry piers. All locations should provide a clear right-way for pedestrians.
- ii. Structural supports for projecting signs should be co-ordinated with the overall architecture and color scheme of the storefront. They should not appear to be "tacked on" without regard for the alignments, proportions, colors, and forms of their adjacent buildings and signs.

c. Awning and Canopy-Mounted Signs.

- i. Lettering and graphics for awning signs should be located on vertical portions of the awning, either the front fascia or the sides. Lettering should not occur on the sloped front of the awning.
- ii. Individual three-dimensional letters are recommended for canopy signs. Individual letters or sign panels may be attached to the vertical fascia of the canopy or mounted on the canopy above the fascia.

d. Window Signs.

- i. Clerestory windows located above the storefront are good locations for window signs.
- ii. Painted window signs where permitted should present a neat and aligned appearance. The services of a professional sign painter are strongly recommended.
- iii. Adhesive stick-on letters should not be used.
- iv. Signs identifying hours of operation, menus, newspaper reviews and other customer information should be framed, board-mounted or plastic laminated for a finished appearance.

e. Freestanding Signs.

- i. All freestanding signs should be low monument signs, directory signs or kiosks. New pole or pylon signs are not permitted (see Development Standards for the appropriate District).
- ii. Freestanding signs should relate to the architecture of the building or development they serve. Exterior materials, finishes, and colors should be the same or similar to those of the building or structures on site. High quality, durable materials, such as metal, stone, concrete and painted wood, should be used. Use of plastic should be minimized.

f. Signs in Public Parking Lots and Garages.

- i. Entry, directional, informational, and traffic control signs (e.g., "do not enter", "no parking", "speed limit", etc.) within parking public parking lots and garages should utilize ornamental frames, trim, bracketing, materials, colors, and/or custom typeface are recommended. Galvanized finishes should be painted.

g. Temporary Signs.

- i. Temporary signs that contribute to the liveliness of the streetscape, such as well-designed menu boards and sidewalk signs, are encouraged.
- ii. For temporary signs and banners for sales and/or special events and temporary construction signs, the services of a professional sign painter are strongly recommended for a neat and aligned appearance.

4. **Materials.** Materials should convey a high-quality appearance, and work with the overall palette of the building's architecture. Materials should be durable; materials that deteriorate quickly such as paper and light-weight cloth are not suitable for exteriors and should not be used. Acceptable materials include:

- a. Wood (carved, sandblasted, etched, et cetera). Wood should be properly sealed, primed and painted, or stained, to avoid deterioration.
- b. Metal (formed, etched, cast, engraved, et cetera). Metal that is prone to rusting should be properly primed and painted or factory coated to protect against corrosion.
- c. High-quality ornamental materials such as stone, ceramic, brass-plate and gold leaf.



Wood is a recommended material, as shown on this sign with incised letters cut into a wood sign panel.



Metal is a recommended sign material, as demonstrated by this cut metal panel sign.



Plastic panel signs like these box panels should not be used.



Individual letter “can” signs are preferable to box-panel “can” signs.



External spotlighting is a highly recommended method of sign illumination.



Sign letters should contrast with their background, like these gold letters against a brown backing.

- d. Fabric awnings, where the fabric should be selected for resistance to fading, either from sun exposure or cleaning. Lettering should be applied or silk-screened to canvas or nylon awning materials by a professional fabricator.
 - e. Custom neon tubing, when used as an accent in conjunction with other sign types. Neon should be used artistically, e.g. to highlight signage and architectural building elements, rather than as a means to attract attention by overwhelming these features.
 - f. Portable signs should be framed, board-mounted or plastic laminated to ensure durability and a high-quality appearance. Portable signs may not include stapled or taped menus.
 - g. The use of plastic panels *is discouraged* as they have a low-quality appearance. Plastic should be limited in use to translucent letters or shapes that are internally illuminated. Non-yellowing materials are recommended; polycarbonate materials subject to yellowing within five (5) years are not recommended.
5. **Lighting.** The lighting of signs should be considered as an element in a building’s overall architectural and lighting design. Signs illuminated by direct light sources are recommended.
- a. Internally illuminated box-panel “can” signs (i.e. translucent plastic sign panel with applied lettering) are not recommended. If “can” signs are to be used, the sign should be designed with light letters against a dark background; dark letters against a light background should not be used.
 - b. Where internally illuminated lighting is desired, internally illuminated individual letter “can” signs are preferable to box-panel “can” signs. Individual letters may be internally illuminated or back-lit, and should be mounted directly on the building structure.
 - c. Direct light sources are recommended. Recommended uses may include spotlighting which casts light on the sign; front-lighting from above or below with single or multiple spotlights; and backlighting fixtures where the lighting washes onto surfaces behind projecting solid or cut-out lettering to create a silhouette or “halo” effect;

- d. Light sources should be shielded to block glare from pedestrians, and residential areas and public rights-of-way; non-decorative bare bulbs should not be used. Illuminated signs and other lighting should be shut off after midnight or upon the close of business, whichever is later.
 - e. Flashing and moving lights should not be used, especially with neon or in other instances where light is an integral part of the sign.
 - f. Recommended light sources include LED, incandescent, halogen, compact fluorescent, and metal halide.
 - g. Light sources that should not be used include high-pressure sodium, low pressure sodium, and bill-board-style long tube fluorescent.
6. **Colors.** Colors of signs should relate or contribute to the overall building design. They should be chosen with regard to the primary building colors, and should relate to or contrast with the primary color to create a well-thought out building color scheme. Signs are good locations for stronger, brighter accent colors, especially in pedestrian-oriented districts like the Downtown District.
- a. Contrasting color schemes should be used to high-light the difference between the letters and the background to make the sign easier to read. Light letters on a dark background or dark letters on a light background are recommended.
 - b. Colors or color combinations that interfere with the legibility of the sign copy should be avoided. Too many colors may obscure the message of a sign.
 - c. Rich and vivid colors are acceptable where they work with the overall building color scheme. Fluorescent colors should not be used.



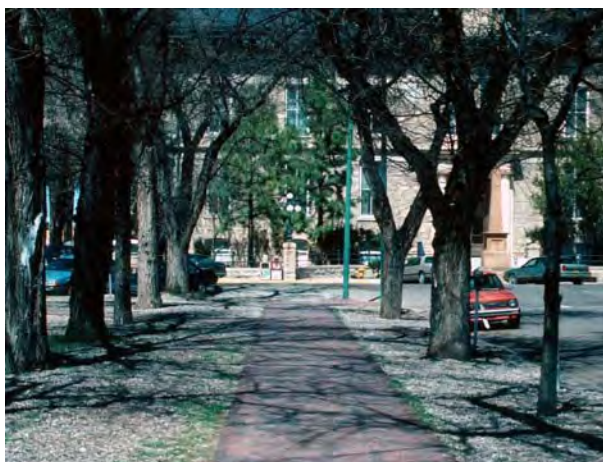
Vivid colors should be restricted in use, and coordinated with the overall building color scheme.



Grade changes should be resolved using visible pieces of architecture such as seat walls and decorative rails.



A bluestone sidewalk combined with brick.



Pavers and decomposed granite.

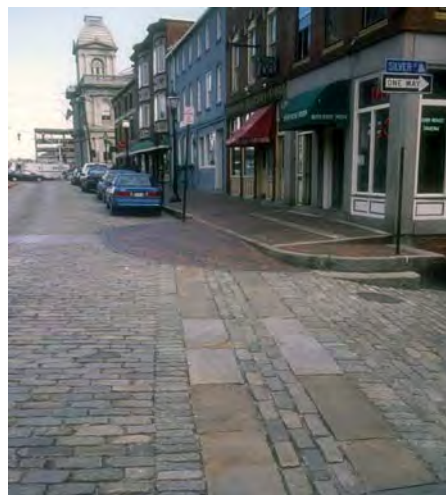
5.8. SITE IMPROVEMENTS, FURNISHINGS, LANDSCAPE, AND LIGHTING - ALL-DISTRICTS

A. SITE IMPROVEMENTS

1. **Surface Grading.** Unnecessary grading should be minimized. Where grading is unavoidable, consider the following guidelines:
 - a. Cross slopes should not exceed two (2) percent in landscaped or sidewalk areas. Optimum slope for paved areas is one point five (1.5) percent, depending on roughness of paving surface.
 - b. Follow the natural contours as much as possible, and contour slopes to blend with the existing terrain.
 - c. Large manufactured slopes should be avoided in favor of several smaller slopes.
 - d. Significant natural vegetation should be incorporated and retained into the project.
 - e. Graded slopes should be landscaped for aesthetic and slope stability purposes.
 - f. On-site water retention basins should be used.
 - g. Mounding earth to elevate buildings, or "berming" earth against the side of buildings, is not recommended.
2. **Pedestrian Surfaces.** Recommended materials for pedestrian surfaces are listed below.
 - a. Stone, such as slate or granite.
 - b. Brick pavers.
 - c. Concrete unit pavers.
 - d. Poured-in-place concrete with any of the following treatments: integral pigment color; decorative aggregate; decorative scoring or stamped pattern; or ornamental insets, such as tile. An integral color pigment or duston hardener pigment is recommended.
 - e. Decomposed granite.
3. **Driveways.** Any of the pedestrian surface materials mentioned above are recommended for driveway paving, except decomposed granite. For large areas, plain or pigmented asphalt and concrete are also acceptable. Pedestrian areas and crossings across driveways should be clearly demarcated, and may be emphasized by any of the following:

- a. Special paving.
- b. A recognizable scoring pattern.
- c. "Bands" of pavers along the crosswalk edge.
- d. Inset decorative elements.

4. **Parking Lots.** Surface parking lots should be designed as an integral feature of the overall site development plan. All parking areas should be designed with convenient safe and efficient pedestrian connections to buildings entry areas, transit stops, and to other pedestrian routes.
- a. Pedestrian systems should provide a clear route to the main building entrance and be designed to include sidewalks and walkways of a minimum five (5) foot width, separated from vehicle areas by curbing and trees.
 - b. The main pedestrian route from parking to building entrance should be easily recognizable and accessible for patrons, designated by special landscaping, such as a shaded promenade.
 - c. Pedestrian routes should be designed to enhance and connect pedestrian and transit facilities, e.g. plazas and courtyards at building entries, seating areas, shaded transit stops, public art, fountains and information kiosks.
 - d. Design of pedestrian systems should be integrated into the design of the building, connecting to building elements such as entrances, awnings, canopies and arcades.
 - e. Large expanses of uninterrupted parking should be avoided; well-distributed smaller lots and structured parking are preferable. Parking areas should be sub-divided in to small sub-lots of no more than 50 spaces each.
 - f. Sub-lots should be distinguishable and separated from each other by a tree-lined parking access road providing access to each individual sub-lots. Space-defining elements such as trellises, columns, walls, arbors, and hedges should also be used to define and enhance the appearance of lots and surroundings. These elements should be consistent in design and materials with the principal building(s) and other site features.
 - g. Landscaping for parking lots should be organized to ensure clear visibility from the street to the building's main entrance. A maximum "clear zone" of no more than 120 feet should be maintained.



Special materials on a driving surface.



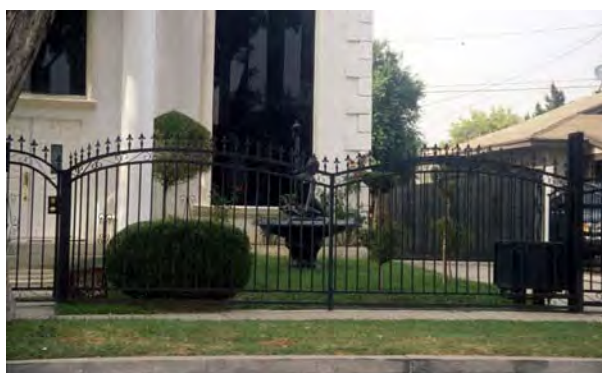
Pedestrian routes to the building entrance.



Pedestrian routes should connect to transit plazas, etc.



Trees planted at a 1:5 ratio.



Frontage fence with an open character.



Combination of iron fencing with stone piers.

- h. Trees should be planted at a ratio of one (1) tree to every four (4) spaces, to provide shade and vegetation throughout the parking area.

B. SITE FURNISHINGS

1. **Fences.** Fences should be consistent with style, materials and design of the principal building(s),
 - a. Frontage Fences:
 - i. Overall height of frontage fences (at front yards) should not exceed three (3) feet in height. Front yard fences are recommended to maintain an open character and permit visibility.
 - ii. For visual interest, a combination of thick and thin structural elements is recommended, with thicker elements for supports and/or panel divisions. Fence posts and/or support columns may be built up with additional trim, caps, finials, and/or moldings for this purpose.
 - b. Screening Fences:
 - i. Overall height of screening fences (at side and rear yards) should not exceed ten (10) feet in height.
 - ii. Screening fences located to the sides and rear of properties may be simple and relatively unornamented. However, they should be visually compatible with adjacent ornamental fence designs and adjacent building architecture. Related colors, a cap or top articulation, and related post spacing should be used at screening fences to enhance compatibility.
 - iii. Adjacent to residential properties, screening fences should maintain a character and scale appropriate to residential neighborhoods; more detailed fencing types and additional ornamentation may be required.
 - c. Materials and Colors
 - i. Fences should be built with attractive, durable materials. Wrought iron, wrought iron style metal, cast iron and wood fences are compatible with the residential character of San Fernando.
 - ii. For iron or metal fences, recommended materials include wrought iron, wrought iron style metal, cast iron, welded steel or aluminum. Metal gauges should be selected to be adequate for resisting bending and denting from casual impacts or petty vandalism. Metal fences should be mounted

on a low masonry wall, and/or between masonry piers. Galvanizing pretreatment beneath recommended paint (a “duplex” system) is recommended for maximum finish life and rust resistance of steel. A powder coat system is also acceptable, though it will generally not be as durable as the recommended wet paint system. A UV-protectant clear coat over paint is recommended for prevention of fading of dark or fugitive colors.

- iii. For painted wood picket fences, a protective coating should be applied. White and light colors are recommended.
- iv. Chain link fencing, corrugated metal fencing and “tennis windscreens” are not permitted.

2. **Walls.** Wall elements should be designed to strongly relate to the architectural style and materials of the principal building(s), and be divided into regular modules that relate to the architectural scale of the principal building(s). Creativity and variety in design is encouraged.

a. Frontage Walls:

- i. Overall height of frontage walls (at front yards) should not exceed three (3) feet in height. These may occur as garden walls, planter walls, seat walls, or low retaining walls.
- ii. Wall openings, material change, or design elements should be used to break up long expanses of uninterrupted fences and walls. Wall expanses should be broken at a minimum of every forty (40) feet. Support piers, pilaster or posts can be emphasized at regular intervals.
- iii. Walls should generally have a cap and base treatment. A distinctive cap of different width, material or texture should occur within the top 8”.
- iv. Entrances and pedestrian “gateways” should be announced by pilasters, trellises, special landscaping, public art or other special features.

b. Screening Walls:

- i. Overall height of screening walls (at side and rear yards) should not exceed eight (8) feet in height.
- ii. Design elements should be used to break up long expanses of uninterrupted walls, both horizontally and vertically. Walls over six (6) feet in height should include design



Wrought and painted iron as fence materials.



Wall openings can be exaggerated to add interest.



Front walls should have a cap treatment.



Plain block walls should NOT be used.



Masonry piers should be used to break up long distances of fencing.



Piers and posts should work with the overall architectural composition.

elements such as textured concrete block, interlocking "diamond" blocks, formed concrete with reveals, or similar materials to relieve surface monotony.

- iii. Mechanical equipment, trash and recycling bins, and meters should be provided with architectural enclosures or fencing, sited in unobtrusive locations, and screened by landscaping. Colors and finishes of mechanical enclosures and equipment should be coordinated with colors and finishes of streetlights, fencing and other painted metal surfaces to be used on site, or with the associated building's material and color scheme.

c. Materials and Colors

- i. Walls should be built with attractive, durable materials. Recommended wall materials include precast concrete, textured concrete block, or formed concrete with reveals, stucco, stone and brick.
- ii. Exposed block walls may be constructed with a combination of varied height block courses and/ or varied block face colors and textures (e.g., a combination of split-face and precision-face blocks).
- iii. Plain gray precision-face concrete block walls are not recommended. Design treatments and finishes previously described should be applied to these walls for improved visual compatibility with building architecture.
- iv. An anti-graffiti coating is recommended for exposed wall surfaces

3. Piers. Pier and Bollard Design

- v. Piers are recommended to have a base, shaft and cap composition. They may provide a termination to a run of fencing, be used instead of fence posts, or be freestanding landscape elements. Larger piers may be specially designed for gateway or other special locations, and these may incorporate ornamental plaques or signs identifying the building or business; public art such as panels or sculptural elements; and /or light fixtures. Piers may also be topped by ornamental light fixtures, roof caps, and/or ornamental finials.
- vi. Masonry piers should be a minimum of eighteen (18) inches per side or diameter at spacings greater than twelve (12) feet; a minimum of twelve (12) inches per side or diameter at spacings of twelve (12) feet or

less. Metal posts should be a minimum of four (4) inches per side or diameter.

vii. The maximum spacing of masonry piers should be thirty (30) feet on center for piers with fencing; eight (8) feet on center for freestanding piers.

viii. Piers should be at the same height or up to eighteen (18) inches higher than adjacent fencing, excluding luminaires or finials.

d. **Materials and Colors**

i. Piers and posts should be constructed of the same or a compatible material as the principal building(s). Support post or pier materials may differ from fence materials; e.g. metal fence panels combined with masonry piers.

ii. Recommended pier materials include integrally colored or decoratively treated cast-in-place concrete, stucco-faced concrete or concrete block, decoratively treated concrete block, precast concrete, brick (colors other than red), terra cotta, and stone. Precast caps and trim may be combined with other materials. An anti-graffiti protective coating is recommended.

iii. Bollards are recommended to be cast iron, cast aluminum, and precast concrete. An anti-graffiti protective coating is recommended for precast concrete.

4. **Site Furnishings and Equipment.** Pedestrian furnishings and amenities should be provided where possible.

a. Seating, freestanding planters, ornamental trash and recycling receptacles, drinking fountains, bollards, information kiosks, transit shelters and bicycle racks are recommended for publicly accessible landscape and hardscape areas, especially public gathering areas. Low walls or wide planter walls are recommended for the creation of seating opportunities without appearing to be empty when not used.

b. Newspaper vending and distribution racks (boxes) should be located in designated areas configured to accommodate them and make them visible and accessible to pedestrians; for example, spaces at street corners "bulbs" are appropriate. Racks should not be permitted to proliferate indiscriminately and create visual blight and pedestrian congestion. Selection of rack equipment that creates ganged mounting and enables aesthetic treatment to relate to streetscape design is strongly recommended.



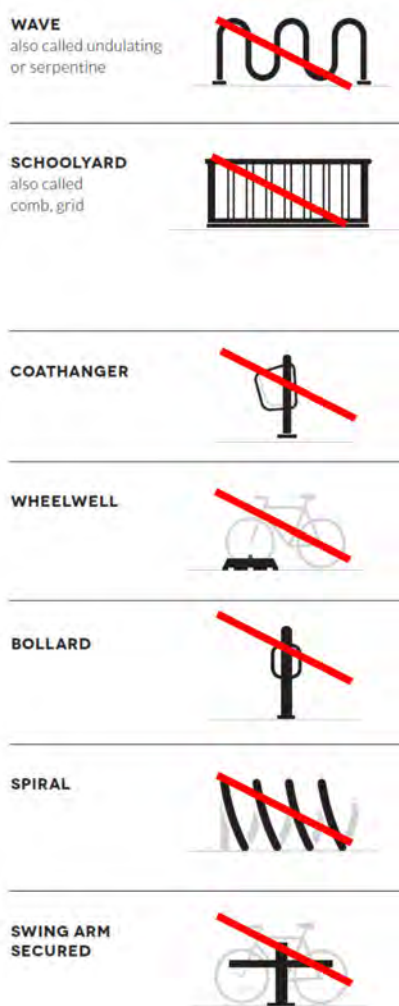
Low walls can be used as planters or for seating.



Newspaper distribution as part of the streetscape.



Preferred bicycle racks styles.



Bicycle racks styles to be avoided.

- c. The design, materials and colors of manufactured furnishings should be coordinated with the principal building(s) and/or other site and streetscape furnishings. Design and selection of furnishings should attempt to reinforce visual relationships to create a “family of objects” within the immediate project vicinity. This should in turn reinforce District character.
- d. Components should be made of durable high quality materials such as painted fabricated steel, painted cast iron, painted cast aluminum, and integrally colored precast concrete. Masonry finishes should be treated with an anti-graffiti coating. Metal surfaces should be coated with highly durable finishes, such as aliphatic polyurethane enamel. An ultraviolet protectant clear coating is strongly recommended for dark or fugitive colors.

4. Bicycle Racks.

- a. Bicycle racks should strike a balance between economy (in both fabrication and maintenance), aesthetics, security and ease of use, and should meet the following functional criteria:
 - i. Support bicycles at two points of contact (preventing fallen bicycles).
 - ii. Allow locking of bicycle frames and wheels with U-locks.
 - iii. Offer a user friendly design.
 - iv. Minimize maintenance costs (galvanized finish resists corrosion).
 - v. Do not require lifting of the bicycle.
 - vi. Provide secure mounting.
 - vii. Offer visibility to pedestrians with a minimum height of 31 inches.
 - viii. Endorsed by the Association of Pedestrian and Bicycle Professionals.
- b. Materials and configurations.
 - i. Stainless steel, cast ductile pipe, and hot-dip galvanized steel pipe are encouraged.
 - ii. Powder coating, although superior to simple paint, is discouraged because it does not sufficiently inhibit rust and the powder coat finish cannot practically be re-applied.
 - iii. Bicycle racks should be mounted to concrete surfaces or subsurfaces and not asphalt.
 - iv. Preferred bicycle rack styles include: inverted U, Post; post and ring; wheelwell secure. Custom-designed art rack designs are also encouraged, provided they meet

the above functional, material, and general configuration criteria.

- v. Rack styles that should be avoided include wave; schoolyard; coathanger, wheelwell, bollard, spiral, and swing arm secured.

C. OPEN SPACE, LANDSCAPE AND PLANT MATERIALS

1. Open Space.

- a. Common open space should be accessible to all related buildings or units. Open space should remain unlocked during daylight hours.
- b. Open spaces should be designed to take into consideration spatial enclosure, and be defined by buildings or landscape elements on a minimum of two sides. Development of open space shall include an enhanced pedestrian system that connects to adjacent public streets and sidewalks via interior walk-ways. Ornamental gates, trellises, lighting, plant materials, etc., should be used to create a sequence for pedestrians along this system; for example, an ornamental gate at the sidewalk leading to a passage lined with columns, then arrival at a courtyard.
- c. Open space areas should contain both landscaped areas and hardscape areas. A mix of both treatments will encourage social interaction, allowing for recreation and play within green spaces while providing alternative gathering areas in the form of plazas or courts. It will also ensure access for people of all abilities to and through open spaces.
 - i. Common landscaped green and/or garden space should comprise between seventy percent (70%) and eighty percent (80%) of the common outdoor area. The space should be centrally located to serve all related buildings or units. The space should be rectilinear with no side less than fifteen (15) feet clear (with additional space allowance for buffer landscaping as required). Space should be seventy five percent (75%) enclosed by buildings, low walls, low fences, or linear buffer landscaping (e.g., hedges or rows of trees) and not be bordered by streets or surface parking areas on more than one side.
 - ii. Common hardscape space should comprise between twenty percent (20%) and thirty percent (30%) of common outdoor area. Common roof deck space may count to-



A common courtyard for residential units.



Visible pedestrian pathways leading to open spaces.



Palm trees permit visibility to establishments.



Evergreens can be used for screening.



Well-shielded downward-focused lighting should be used to direct light away from the sky.

wards this provision. Material selected for hardscape areas should be both functional and attractive; i.e., unit pavers or gravel. Hardscape space shall be connected directly to landscaped areas by stairs, walks, and/or ramps where necessary.

2. **Plant Materials.** Plantings should be used to create an attractive and harmonious character, and contribute to a cohesive design for the street. Planted and landscaped areas should have a simple palette of plant species.

- a. **Street Trees:**

- i. Street trees should be planted in aligned rows centered within planting strips between sidewalks and curbs where available. Alternatively, they may be planted in tree wells within the sidewalk at the back of curb (at integral curb, gutter and sidewalk installations) to create a buffer between pedestrians and automobiles.
- ii. Regular spacing and consistency should be used to reinforce a strong street identity and corridor structure, typically along the length of a street corridor within a District. Where a street tree pattern and species have been established, infill projects should provide matching materials and layout.
- iii. Larger species and more visible spatial configurations should be used at larger, more important streets and plazas. Where street image perceived from both motorist and pedestrian views is important, the scale of planting treatments should follow suit, e.g., rows of tall palm trees to shape the motorist experience, and an understory planting of smaller shade trees in between the larger trees for the scale and comfort of pedestrians.
- iv. The minimum installed size of new street trees should be a twenty-four (24) inch box size. Tree wells should be landscaped with drought tolerant shrubs and/or decomposed granite, subject to City review for streetscape continuity.

- b. **Tree Types and Species:**

- i. To ensure visibility to retail establishments, trees with open branching structures and canopies, at maturity, that grow above storefronts and signage are recommended.
- ii. Trees and plants at other locations should be selected and placed to reflect both ornamental and functional characteristics.

- iii. Selected species should be drought and wind tolerant and minimize litter and other maintenance problems.
- iv. A qualified arborist or licensed landscape architect should be consulted for final selections and installation recommendations based on site soils, drainage, and microclimate.
- v. Both seasonal and year-round flowering shrubs and trees should be used where they can be most appreciated - adjacent to walks and recreational areas, or as a frame for building entrances and stairs.
- vi. Evergreen shrubs and trees should be used for screening along rear property lines (not directly adjacent to residences), around trash/recycling areas and mechanical equipment, and to obscure grillework and fencing associated with subsurface parking garages.

D. LIGHTING

1. Design.

- a. In order to restrict the emission of undesirable illuminating light rays up into night sky, all exterior lighting shall be cut-off fixtures. Indirect illumination is recommended, and may be achieved by concealing light features beneath shields or screens, or by recessing them into building walls or overhangs.
- b. Street lighting should be chosen with care, and should add to the aesthetic of the street. Lighting design should be consistent with streetscape character.
- c. Other pedestrian-oriented areas, including walkways and paths, plazas, parking lots, and parking structures should be illuminated to provide clear views both to and within the site.
- d. Area lights, especially at parking lots, are encouraged to be greater in number, lower in height and lower in light level, as opposed to fewer in number, higher in height and higher in light level.

2. Materials and Color.

- a. The color and finish of exposed metal surfaces of on-site light fixtures and poles should be compatible with building architecture. Color and finish of lighting metalwork should match that of other site furnishings, and/or of the building's metalwork or trim work.



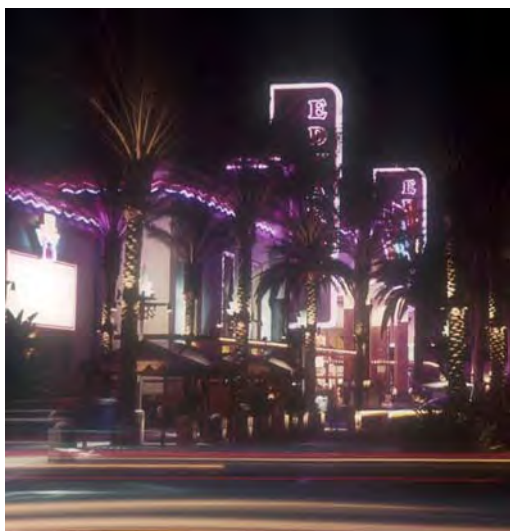
Lighting fixtures and mounting should be designed as a part of building architecture.



Prismatic refractor globes are recommended to enhance efficiency and reduce glare.



Mounting height should be related to the pedestrian scale.



Facade lighting for nighttime effects.

- b. Recommended paint finishes for metal include:
 - i. Galvanizing beneath paint (a “duplex” system) is recommended for maximum finish life and rust resistance of steel.
 - ii. A UV-protectant clear coat over paint is recommended for prevention of fading of dark or fugitive colors.

3. Luminaire Type.

- a. Fixtures should use a reflector and/or a refractor system for efficient distribution of light and reduction of glare.
- b. Sharp cut-off type fixtures are recommended, to prevent light from being emitted above the horizontal relative to the light source. Small decorative “glow” elements are permitted to emit light above the horizontal. Alternatively or in addition, fixtures should use a refractive prismatic diffuser globe to direct light downward and focused in a pattern as desired.
- c. Recommended globes include clear borosilicate prismatic glass globes; clear acrylic globes with optical diffusing (prismatic) patterns; translucent clear (frosted) or white acrylic globes. Polycarbonate globes are not recommended. Clear, smooth surface finish acrylic or polycarbonate globes are not recommended as they tend to show scratches and wear after several years.
- d. House side shields and internal reflector caps should be used to block light from illuminating residential windows.
- e. For pedestrian-oriented area lighting, energy efficient sources with warm white color and good color rendition are recommended. Recommended lamp types include:
 - i. Color-corrected metal halide [two thousand nine hundred (2900) to three thousand two hundred (3200) degrees Kelvin] are acceptable.
 - ii. Color-corrected fluorescent [two thousand seven hundred (2700) to three thousand two hundred (3200) degrees Kelvin] are acceptable.
 - iii. For loading areas and other non-pedestrian intensive areas, high pressure sodium, low pressure sodium, or metal halide may be acceptable for efficient lighting.
- f. For accent lighting, LED, halogen incandescent and standard incandescent are also recommended.

- g. Standard mercury vapor, high-pressure sodium, low pressure sodium, and cool white fluorescent are not recommended.

4. **Poles And Mounting Height.** In general, light sources should be kept low to maintain pedestrian scale and prevent spill light from impacting adjacent properties.

- a. Mounting height of light sources for area illumination (such as parking lots and yards) should be a maximum of eighteen (18) feet, measured from the finished grade.
- b. For pole-mounted lighting at pedestrian plazas, walk-ways, and entry areas, a pedestrian-height fixture is recommended - twelve (12) to fifteen (15) feet in height from grade to light source. Thirteen (13) feet is optimal.
- c. Bollard mounted lighting and step-lighting is also recommended for low-level illumination of walkways and landscaped areas.

5. **Uplighting.** Uplighting should be carefully sited and shielded to prevent spill light from visibility by pedestrians, motorists, and nearby residential dwelling windows.

- a. All decorative uplighting, including building facade uplighting, roof "wash" lighting, and landscape uplighting, should be operated on timers that turn off illumination after 12 midnight nightly.
- b. At lighted areas adjacent to single family homes, a combination of careful placement, mounting height and luminaire shields should be used to protect residences from glare.
- c. Illumination levels of facade uplighting, roof wash lighting and landscape uplighting should use lower brightness levels where illuminated facades, roofs and landscaping face residential buildings, except across wider streets or boulevards with landscaped medians and street trees.

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CHAPTER SIX: CAPITAL IMPROVEMENTS



Truman Street



First Street



Truman Street Improved



First Street Improved

This chapter describes the capital improvements that are integral to the envisioned future of the San Fernando Corridors. Hand in hand with the private investment that the community desires to assist in the revitalization of the corridors, public investments such as streetscapes, gateway features, and architectural landmarks are fundamental to achieving the stated goals.

Capital improvements set the stage for revitalization. The benefits behind improvements such as new streetscapes, the construction of civic and public open space, and the enhancement of transportation and other city infrastructure are several. First, by creating signs of investment in areas that have not received private investment for some time, capital improvements “break the ice” for new investment. In this sense, the City of San Fernando takes the lead by serving to “prime the pump” for new investment, attracting the interest of prospective new investors. Second, within a given city district, improvements recondition the physical space within the public rights of way to provide the type of environment in which desired land uses will best perform. Therefore, new capital investments serve to “set the stage” for new investment by creating ideal places for such development to occur. Finally, by building support from both the private sector and the public community, capital improvements can act as a starting point to generate the momentum needed to revitalize the corridors. The investment that the City makes in its public realm is the physical evidence indicating the City’s intentions and in that way capital improvements add value to the community by making evident to prospective investors the City’s commitment to revitalization.

In the particular case of San Fernando, the process of preparing this specific plan for the Maclay, Truman, San Fernando Road, and First Street Corridors in many ways presents an opportunity to reverse the current underutilization, disinvestment, and lack of amenity in the corridors planning area. Currently, the corridors are aesthetically unappealing and do not provide the comforts that attract pedestrians to the uses which line them. New street improvements are an opportunity to make the city’s most visible streets attractive to pedestrians and supportive of a mix of urban land uses, turning them back into comfortable and habitable “pieces of the city.” Capital improvements will beautify the corridors so that they better represent the family-oriented and small town identity that is fundamental to the San Fernando community, while maintaining the necessary traffic flows and keeping practicality in mind.

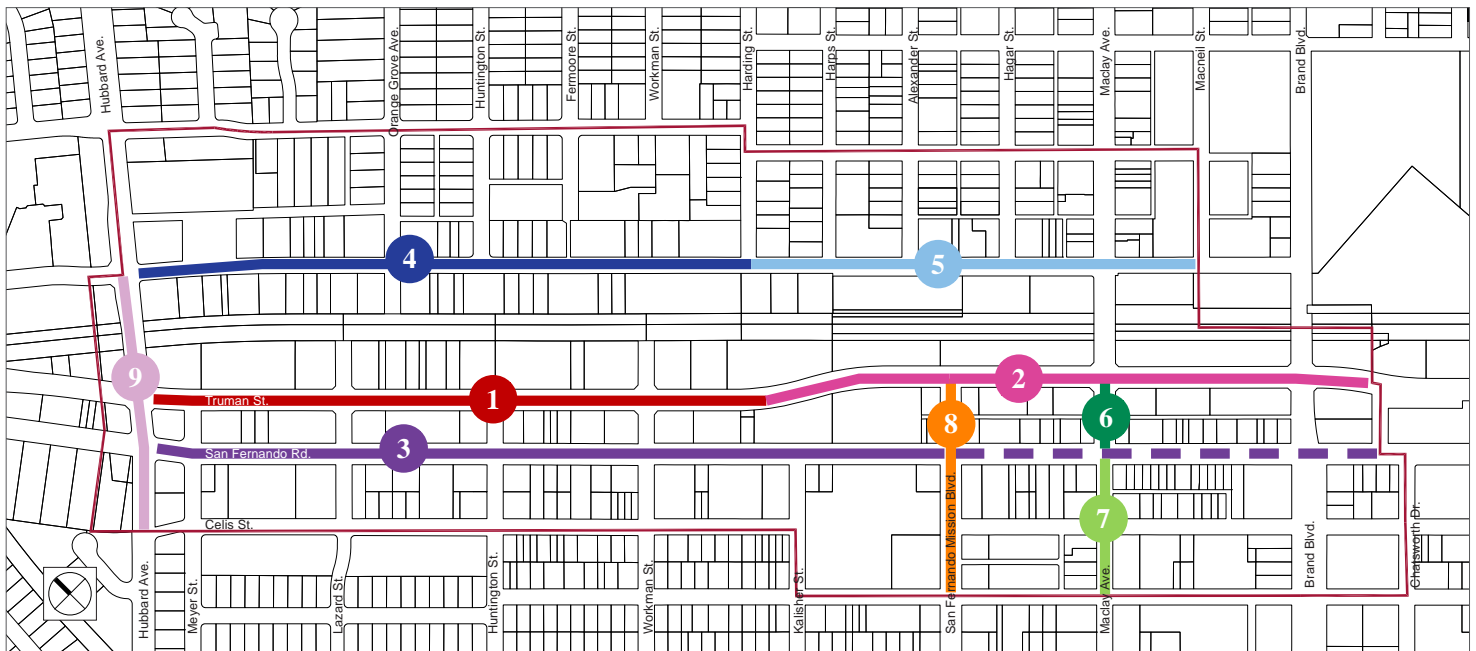
The following recommendations result from active participation on the part of community members and City staff. They are designed to work in collaboration with the proposed district formation in order to “set the stage” for the preferred revitalization strategies embedded within this specific plan. These recommendations are conceptual design efforts. Future investments in streetscapes, landmarks, gateways, or other types of capital improvements will require further “design development” considering relevant budgetary constraints and subject to thorough engineering and environmental review.

STREETS

The streetscape improvements recommended for the Maclay, Truman, San Fernando Road, and First Street Corridors are designed to stimulate near-term investment. The recommended improvements are structured to “set the stage” for new investment by signaling to the investment community that the City is serious about its commitment to change. They will also provide an attractive and supportive environment for envisioned land uses and building types.

These streetscape improvements may be accomplished in two ways. Where possible they may be advanced by the City in order to create a supportive environment for appropriate development in each segment, and to instigate more immediate change in the specific plan area. However, in areas where the City has not already fully completed all of the planned streetscape improvements pursuant to this specific plan, such street and sidewalk improvements will be required of new development, to be provided by each developer along his or her property frontage as development occurs.

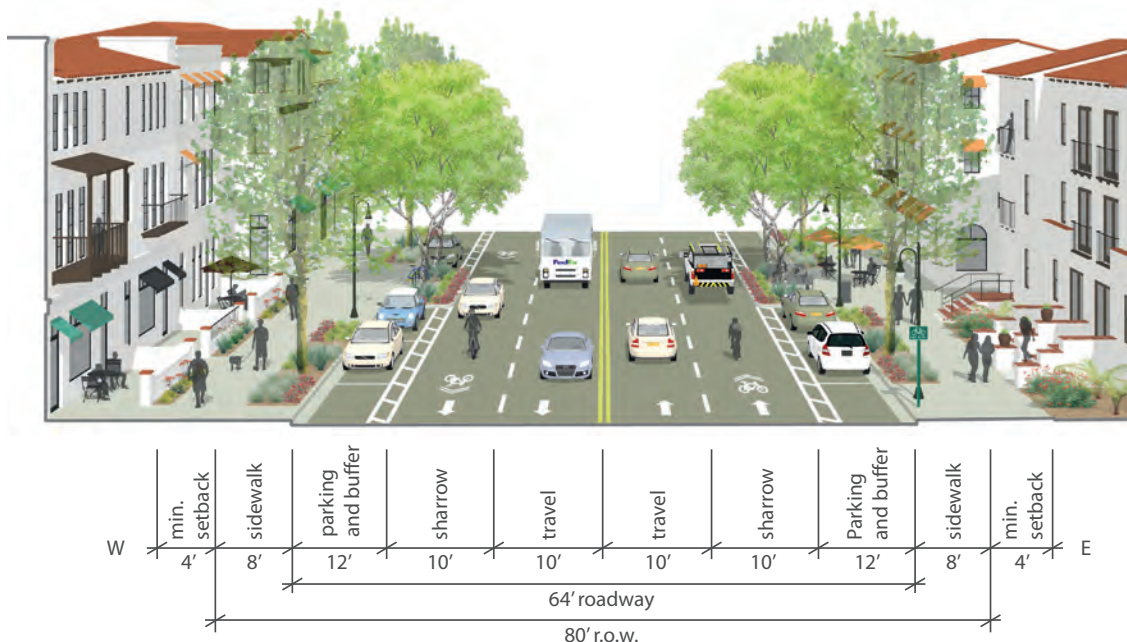
The streets that are recommended for improvement as part of this plan are shown in Figure 6.1 and the recommended improvements are shown on pages 162-173.

FIGURE 6.1 - STREET NETWORK**Legend**

- | | | |
|-----------------------------|------------------------------------|------------------------------|
| 1 Truman Street | 4 First Street West of Harding St. | 7 Maclay Avenue 2 |
| 2 Truman Street in Downtown | 5 First Street East of Harding St. | 8 San Fernando Mission Blvd. |
| 3 San Fernando Road | 6 Maclay Avenue 1 | 9 Hubbard Avenue |

1 TRUMAN STREET: ALTERNATIVE 1

HUBBARD AVENUE TO SAN FERNANDO MISSION BOULEVARD

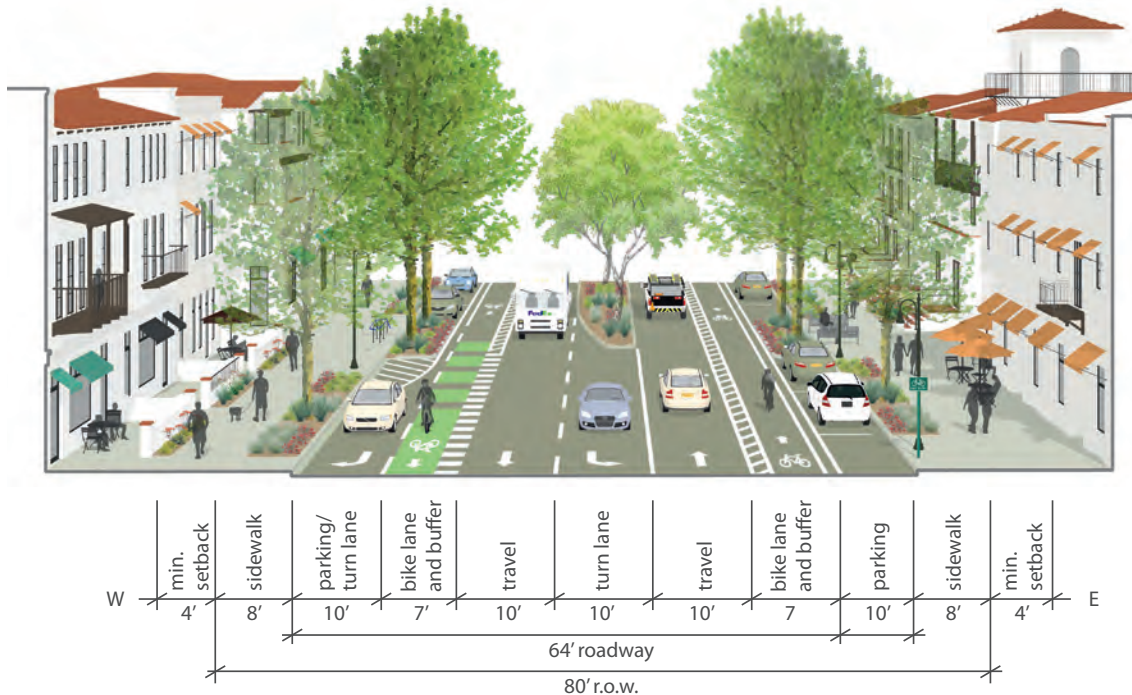


Improvements to Truman Street between Hubbard Avenue and San Fernando Mission Boulevard consist primarily of streetscape improvements – introducing street trees and street lights – and re-striping the travel lanes so that they are narrower to encourage vehicular traffic to slow down. On-street parallel parking is preserved to provide convenient parking in front of businesses and residences, as well as to provide a buffer between vehicular traffic and the sidewalk. Improvements occur within the existing right-of-way and curb-to-curb widths. Improvements must incorporate the following:

- A. Travel lanes: two (2) ten (10) foot wide travel lanes in each direction with the curb side lane marked as a sharrow (a shared bicycle and traffic lane). Left turn lanes are provided as needed.
- B. Street parking/buffer: eight (8) foot wide parallel parking lanes and four (4) foot wide buffers along both sides. Where bus stops occur, whether local or bus rapid transit (BRT), parallel parking is prohibited to provide a place for buses to turn out. Bus stops are designed per the appropriate transit provider requirements. Parking lanes are also converted to right turn lanes at key intersections.

- C. Sidewalks: Sidewalks are widened from their existing eight (8) foot width to a minimum of twelve (12) feet by setting back buildings and dedicating the setback area to sidewalk.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately 32 ft. on center.
 - 1. Alternate: street trees are placed in in-street tree planters in between every 2 parallel parking spaces.
- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

1 TRUMAN STREET ALTERNATIVE 2: ONE LANE EACH DIRECTION WITH BIKE LANES MEYER TO SAN FERNANDO MISSION BOULEVARD



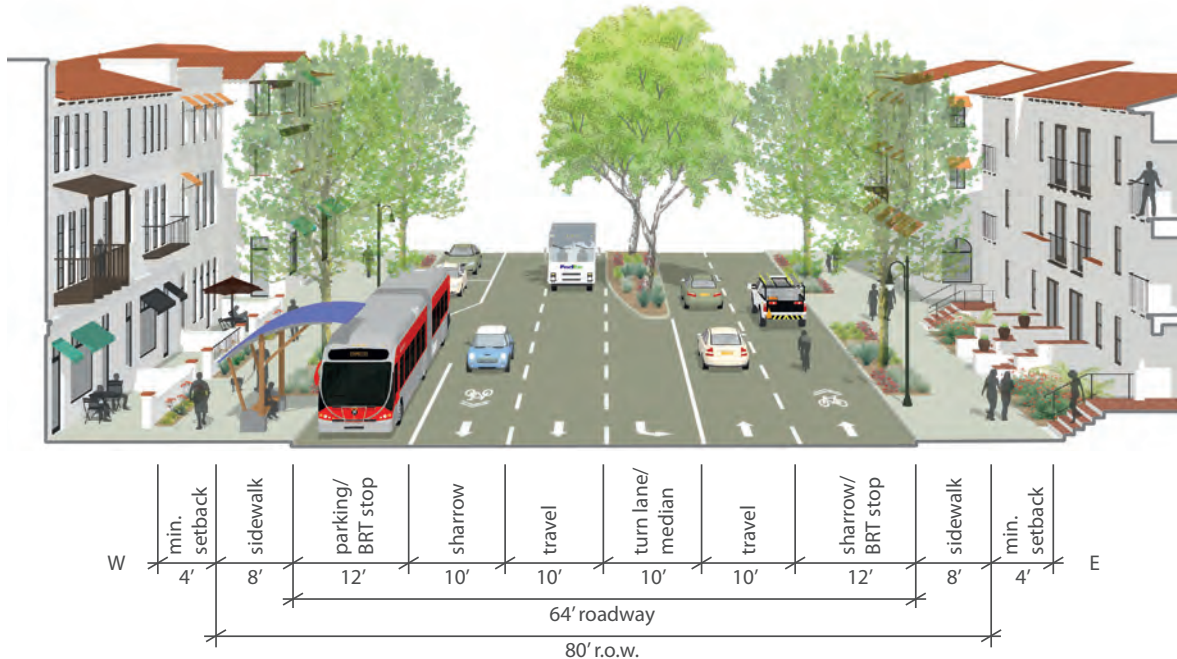
Since traffic loads along Truman Street between San Fernando Mission Boulevard and Hubbard Avenue, an alternative configuration could consist of one lane and a Class II bike lane in each direction separated by a center turn lane or landscaped median. Improvements occur within the existing right-of-way and curb-to-curb widths. Improvements must incorporate the following:

- A. Travel lanes: one (1) lane in each direction separated by a center turn lane or a planted center median. The travel lanes, turn lane, and/or median are all ten (10) feet wide.
- B. Bike lanes: a seven (7) foot wide buffered bike lane is introduced in each direction.
- C. Street parking: eight (8) foot wide parallel parking lanes and four (4) foot wide buffer along both sides. Where bus stops occur, whether local or bus rapid transit (BRT), parallel parking is prohibited to provide a place for buses to turn out. Bus stops are designed per the appropriate transit provider requirements.

- D. Sidewalks: Sidewalks are widened from their existing eight (8) foot width to a minimum of twelve (12) feet by setting back buildings and dedicating the setback area to sidewalk.
- E. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.
- F. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

2 TRUMAN STREET IN DOWNTOWN: WITH CENTER TURN LANE

SAN FERNANDO MISSION BOULEVARD TO BRAND BOULEVARD

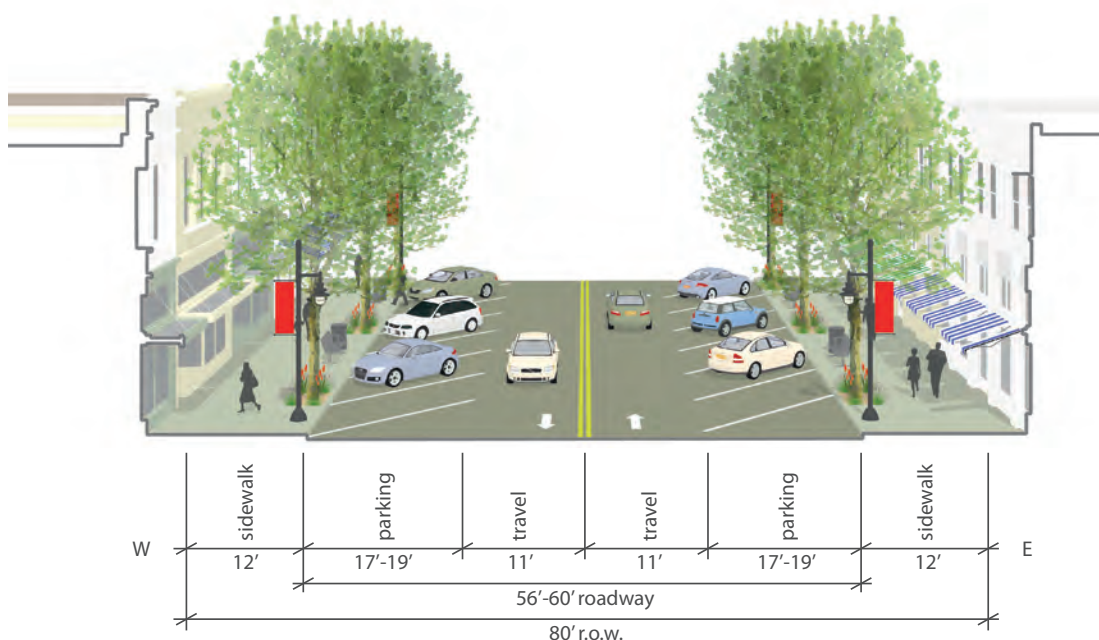


Improvements to Truman Street between San Fernando Mission Boulevard and Brand Boulevard consist primarily of streetscape improvements – introducing street trees and street lights – and re-striping the travel lanes so that they are narrower to encourage vehicular traffic to slow down. On-street parallel parking is introduced along the south side of the street to provide convenient parking in front of businesses and residences, as well as to provide a buffer between vehicular traffic and the sidewalk. Improvements occur within the existing right-of-way and curb-to-curb widths. Improvements must incorporate the following:

A. Travel lanes: two (2) lanes in each direction with a shared center turn lane. In the northbound direction, the curb side lane is twelve (12) feet wide and the median side lane is ten (10) feet wide. In the southbound direction, both travel lanes are ten (10) feet wide. The curb side lanes are marked as sharrows, a shared bicycle and traffic lane. Left turn and right turn lanes are provided as needed.

- B. Street parking: Twelve (12) foot wide parallel parking lane along the south side of the street. Where bus stops occur, whether local or bus rapid transit (BRT), parallel parking is prohibited to provide a place for buses to turn out. Bus stops are designed per the appropriate transit provider requirements.
- C. Sidewalks: Sidewalks are widened from their existing eight (8) foot width to a minimum of twelve (12) feet by setting back buildings and dedicating the setback area to sidewalk.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.
- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

3 SAN FERNANDO ROAD ALTERNATIVE 1



Improvements to San Fernando Road occur within the existing right-of-way and curb-to-curb widths. Improvements must incorporate the following:

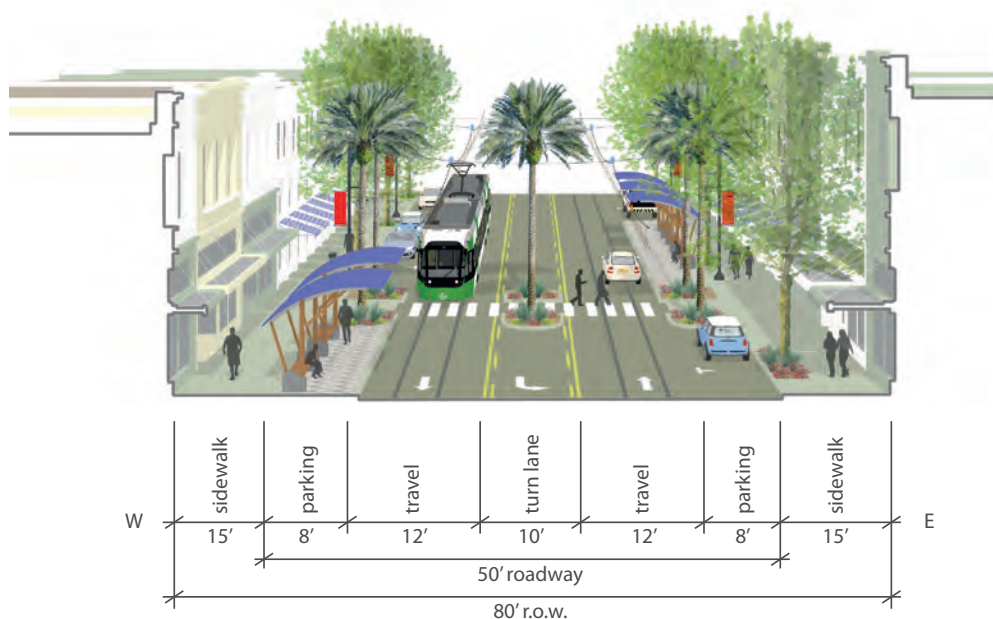
- A. Travel lanes: one (1) travel lane in each direction. Left turn and right turn lanes are provided as needed.
- B. Street parking: Angled parking along both sides of the street to provide convenient parking in front of businesses and residences as well as to provide a buffer between vehicular traffic and the sidewalk. **Reverse angled parking is permitted, subject to review by the Public Works Director**
- C. Sidewalks: minimum twelve (12) foot wide, level, paved sidewalks. Where sidewalk is narrower than twelve (12) feet, adjacent building shall be setback appropriately with the setback area dedicated to sidewalk
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.

E. Street Lights:

1. Between Brand Boulevard and San Fernando Mission Boulevard: New double-head, pedestrian-scale lights shall be installed to be consistent with tree planting, at approximately thirty-two (32) feet on center along the public right-of-way.
2. Between San Fernando Mission Boulevard and Hubbard Avenue: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center.

Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

3 SAN FERNANDO ROAD ALTERNATIVE 2: WITH METRO TRAM



This San Fernando Road alternative accommodates Metro's East San Fernando Valley Transit Corridor Tram alternative and consist of narrowing the curb to curb width in order to create a narrower roadway and provide wider sidewalks. Of all the improvements to the streets within the Corridors Specific Plan area, this is the only one that entails moving the curbs from their current position and it would only be possible if the Tram alternative is introduced. Improvements must incorporate the following:

- A. Travel lanes: one (1) travel lane in each direction with shared center turn lane. Right turn lanes are provided as needed.
- B. Street parking: Parallel parking along both sides of the street to provide convenient parking in front of businesses and residences as well as to provide a buffer between vehicular traffic and the sidewalk. Curb bulb-outs and no parallel parking are provided at the Tram stops.

- C. Sidewalks: minimum fifteen (15) foot wide, level, paved sidewalks.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.
- E. Street lights:
 1. Between Brand Boulevard and San Fernando Mission Boulevard: New double-head, pedestrian-scale lights shall be installed to be consistent with tree planting, at approximately thirty-two (32) feet on center along the public right-of-way.
 2. Between San Fernando Mission Boulevard and Hubbard Avenue: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center.

Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

4 FIRST STREET WEST OF HARDING STREET

HUBBARD AVENUE TO HARDING STREET



Improvements to First Street between Hubbard Avenue and Harding Street are intended to create a more inviting pedestrian, bicycle, and vehicular connection between the Metrolink Station and Maclay Avenue's "main street" and the Civic Center. Improvements occur within the existing right-of-way and curb-to-curb widths and consist primarily of the introduction of street trees and street lights. Improvements must incorporate the following:

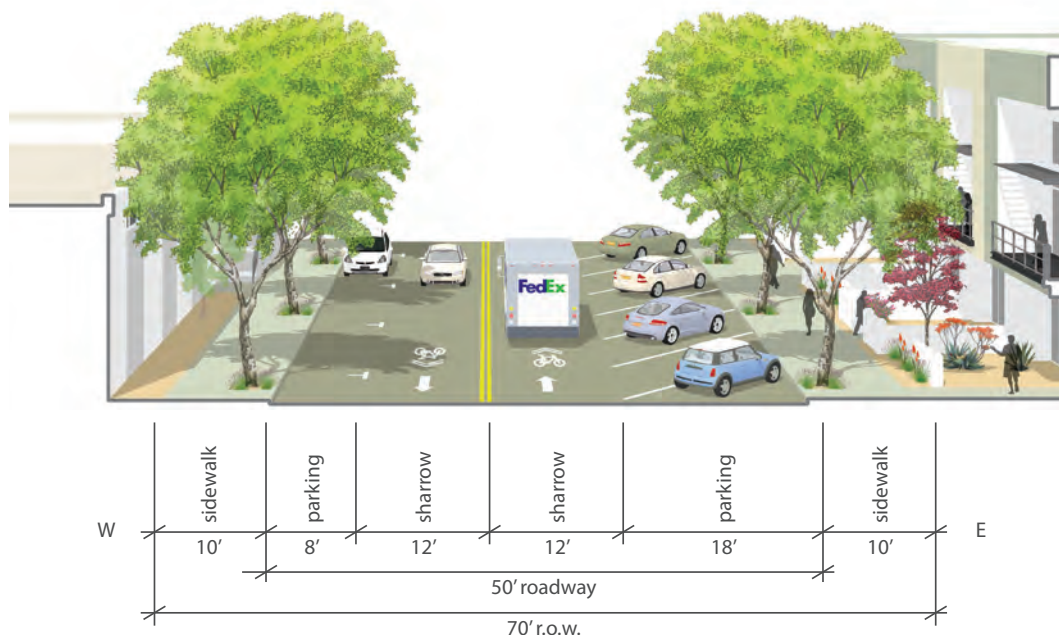
- A. Travel lanes: one travel lane in each direction, marked as sharrows, a shared bicycle and traffic lane. Left turn and right turn lanes are provided as needed.
- B. Street parking: Parallel parking along both sides of the street to provide convenient parking in front of businesses and residences as well as to provide a buffer between vehicular traffic and the sidewalk.

- C. Sidewalks: minimum twelve (12) feet wide, level, paved sidewalks. Where sidewalk is narrower than 12 feet, adjacent building shall be setback appropriately with the setback area dedicated to sidewalk
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately 32 ft. on center.
- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center.

Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

5 FIRST STREET EAST OF HARDING STREET ALTERNATIVE 1

HARDING STREET TO BRAND BOULEVARD



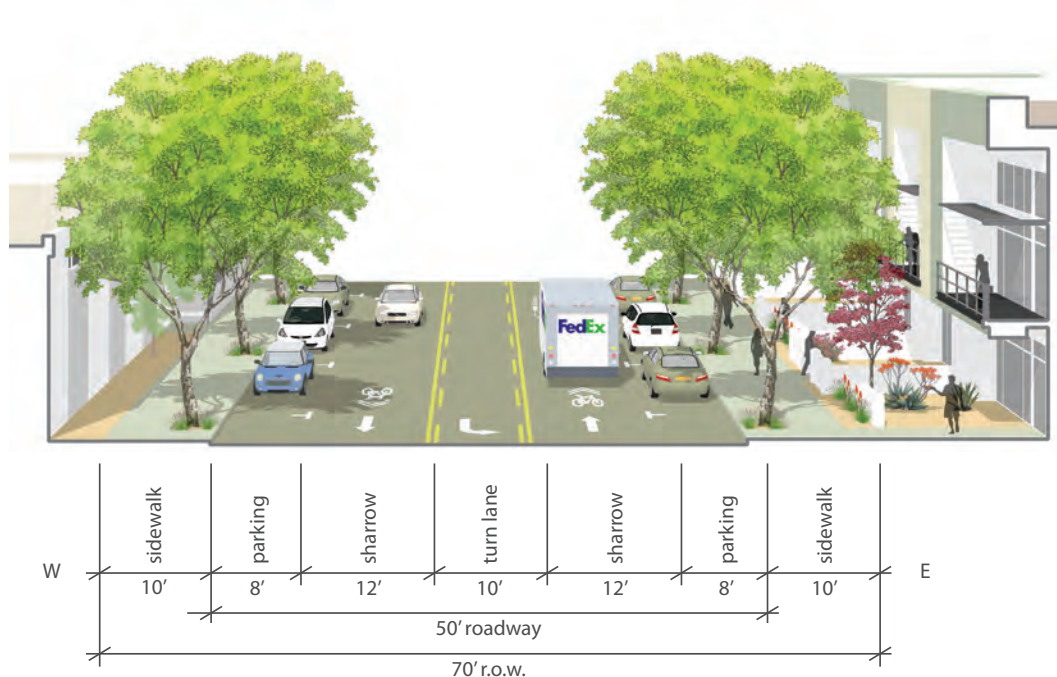
Improvements to First Street between Harding Street and Brand Boulevard are intended to create a more inviting pedestrian, bicycle, and vehicular connection between the Metrolink Station and Maclay Avenue's "main street" and the Civic Center, as well as to make more efficient use of First Street's excessively wide curb to curb width by introducing angled parking along its north side. Improvements occur within the existing right-of-way and curb-to-curb widths and consist of re-striping and introducing street trees and street lights and must incorporate the following:

- A. Travel lanes: one (1) travel lane in each direction, marked as a sharrow, a shared bicycle and traffic lane. Left turn and right turn lanes are provided as needed.
- B. Street parking: Parallel parking along the south side of the street and angled parking along the north side of the street. **Reverse angled parking is permitted, subject to review by the Public Works Director**

- C. Sidewalks: minimum twelve (12) feet wide, level, paved sidewalks. Where sidewalk is narrower than twelve (12) feet, adjacent building shall be setback appropriately with the setback area dedicated to sidewalk
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.
 - 1. Alternate: street trees are placed in in-street tree planters in between every 2 parallel parking spaces.
- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

5 FIRST STREET EAST OF HARDING STREET ALTERNATIVE 2

HARDING STREET TO BRAND BOULEVARD



This alternative way of improving First Street between Harding Street and Brand Boulevard, like the first takes advantage of First Street's excessive width, but by introducing a center left turn lane for east bound traffic. Also, like the rest of the First Street improvements, it is intended to create a more inviting pedestrian, bicycle, and vehicular connection between the Metrolink Station and Maclay Avenue's "main street" and the Civic Center. Improvements occur within the existing right-of-way and curb-to-curb widths and consist of re-striping and introducing street trees and street lights and must incorporate the following:

- A. Travel lanes: one (1) travel lane in each direction, with a shared center turn lane. Travel lanes are marked as sharrow, a shared bicycle and traffic lane. Left turn and right turn lanes are provided as needed.
- B. Street parking: Parallel along both sides of the street.
- C. Sidewalks: minimum twelve (12) feet wide, level, paved sidewalks. Where sidewalk is narrower than twelve (12) feet, adjacent building shall be setback appropriately with the setback area dedicated to sidewalk

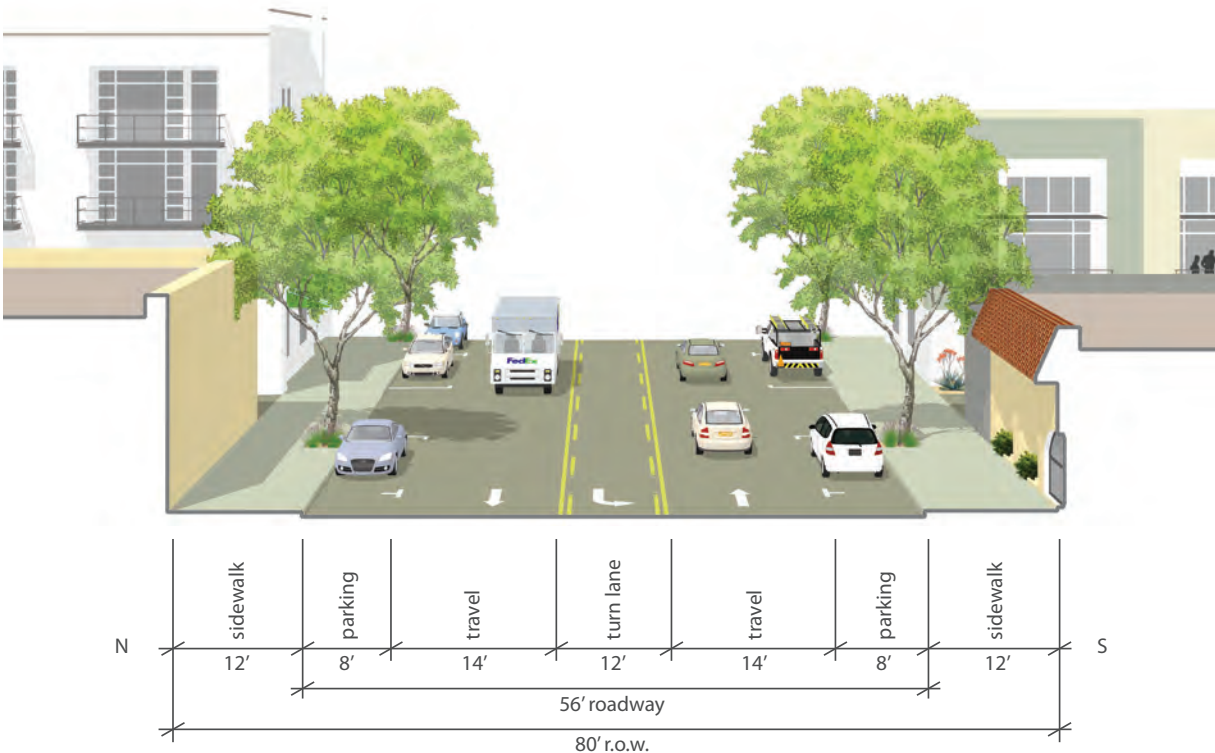
D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.

- 1. Alternate: street trees are placed in in-street tree planters in between every 2 parallel parking spaces.

E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

6 MACLAY AVENUE 1

TRUMAN AVENUE TO SAN FERNANDO ROAD



Improvements to MacLAY Avenue between Truman Streets and San Fernando Road occur within the existing right-of-way and curb-to-curb widths and consist of re-striping the travel lanes in order to introduce convenient on-street parking in front of Downtown stores and restaurants as well as to provide a buffer between vehicular traffic and the sidewalk. Streetscape improvements include the introduction of street trees and street lights. Improvements must incorporate the following:

- A. Travel lanes: one (1) travel lane in each direction with a center shared left turn lane.
- B. Street parking: parallel parking along both sides of the street.
- C. Sidewalks: minimum twelve (12) feet wide, level, paved sidewalks, preserving existing sidewalk widths.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.

- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, MacLAY District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

7 MACLAY AVENUE 2

SAN FERNANDO ROAD TO PICO STREET

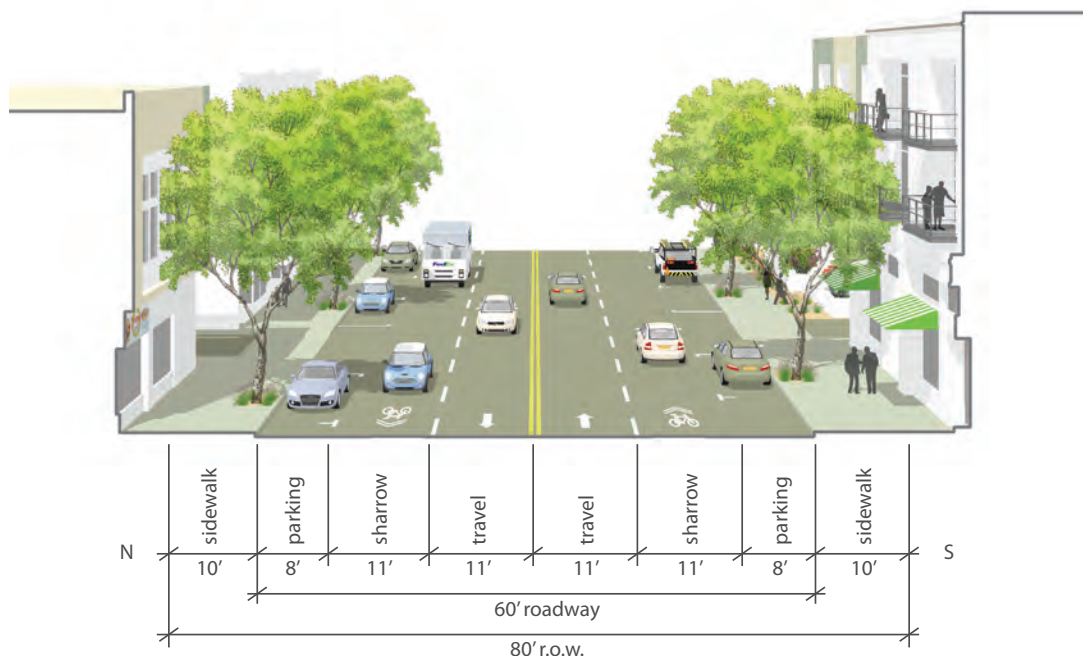


Improvements to MacLAY Avenue between San Fernando Road and Pico Street within the existing right-of-way and curb-to-curb widths and consist of re-striping the travel lanes in order to introduce convenient on-street parking in front of Downtown stores and restaurants as well as to provide a buffer between vehicular traffic and the sidewalk. Streetscape improvements include the introduction of street trees and street lights. Improvements must incorporate the following:

- A. Travel lanes: one (1) travel lane in each direction with left turn lanes.
- B. Street parking: parallel parking along both sides of the street.
- C. Sidewalks: minimum ten (10) feet wide, level, paved sidewalks, preserving existing sidewalk widths.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.

- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, MacLAY District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

8 SAN FERNANDO MISSION BOULEVARD



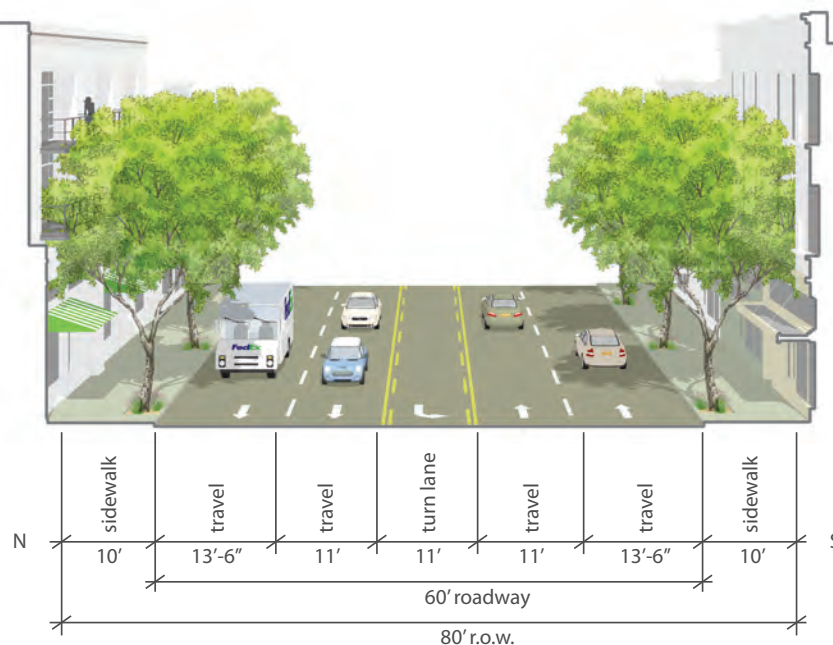
Improvements to San Fernando Mission Boulevard Road occur within the existing right-of-way and curb-to-curb widths and consist of re-stripping the travel lanes in order to introduce convenient on-street parking in front of Downtown stores and restaurants as well as to provide a buffer between vehicular traffic and the sidewalk. Streetscape improvements include the introduction of street trees and street lights. Improvements must incorporate the following:

- A. Travel lanes: two (2) travel lanes in each direction with the curb side lane marked as a sharrow, left turn lanes where needed.
- B. Street parking: parallel parking along both sides of the street.
- C. Sidewalks: minimum ten (10) feet wide, level, paved sidewalks, preserving existing sidewalk widths.
- D. Street trees: Large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center.

- E. Street lights: New decorative pedestrian-scale lights shall be installed to be consistent with tree planting, approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.

9

HUBBARD AVENUE



Improvements to Hubbard Avenue consist solely of streetscape improvements to improve the pedestrian experience to the Metrolink Station. Improvements must incorporate the following:

- A. Travel lanes: no change. Keep two (2) travel lanes in each direction with, left turn and right turn lanes.
- B. Street parking: none.
- C. Sidewalks: keep existing minimum ten (10) feet wide, level, paved sidewalks, preserving existing sidewalk widths.
- D. Street trees: large open habit trees are placed at the back of curb, at a spacing of approximately thirty-two (32) feet on center. Lights shall comply with Section 6.4 *Lighting* of the Development Standards of the Downtown District, Maclay District, and Workplace Flex District and Section 7.4 *Lighting* of the General Neighborhood District.
- E. Improved crosswalks at Truman Street, San Fernando Road, and First Street to provide safe and easy access to the Sylmar/San Fernando Metrolink Station.

LANDMARKS

Landmarks such as the “landmark columns” illustrated on page 173 and/or other prominent architectural features should be used to distinguish and unify the Downtown District, particularly in front of parking lots. Regularly spaced landmarks should be used to “stitch” together streets within the downtown. In a potential extension beyond the specific plan’s boundaries, extending the use of the landmark columns along Second Street between Maclay Avenue and Macneil Street will help complete the perception of the Downtown District as an extension of the Civic Center.

In addition to defining the Downtown District, landmark columns would assist in strengthening the visual and pedestrian character of the heart of the city. During the community workshop process, workshop participants remarked that along the south side of Truman Street, at the back of the Mall’s public parking lots, the pedestrian environment lacks sufficient enclosure. The north side of Truman Street in this stretch is a similarly unfriendly environment for pedestrians. Installation of landmark columns within the public right of way at the back of sidewalk in these areas will have a strong influence to reverse this feeling of exposure. Landmarks should be designed to accommodate public art work that is either temporary or permanent. Design elements may further include opportunities for street lighting. Uplighting the landmark columns would substantially enhance their visibility and overall aesthetic impact during the evening hours.

Landmark columns should be installed at a spacing of no more than one hundred twenty (120) feet on center to ensure strong district definition. Larger versions of the landmark column can be used at primary intersections to further define entry to the Downtown District. Refer to “Capital Improvements – Landmark Column” illustration on page 173 for an illustration of one possible way to locate the landmark columns.

As an alternative or interim measure, some of the same effect may be achieved at less cost through the use of architecturally elaborated street light standards in the Downtown District. Distinctive fixtures such as changeable banners, in combination with other elements such as uplighting and/or decorative lighting of street trees or elaborated street light standards would also define and enhance the district. Continuation of a downtown design theme with wayfinding signs on monument-type bases and street furniture that is limited to the Downtown District would further reinforce the coherence of the district.

GATEWAYS

Gateways play the important role of announcing entrances to the city. When done effectively they convey a sense of arrival and in their form and character communicate something about the community’s character. They are the first welcoming image presented to potential investors and visitors, and they instill a sense of pride and belonging to returning residents. Physically distinguishing the City of San Fernando from surrounding areas will make an impression on residents and visitors alike regarding the sense of pride and ownership with which the citizens of San Fernando regard their city.

Gateway elements should be constructed within the public right of way as follows:

1. At the city’s northern boundary, use a combination of high-quality architectural prominence and landscape architecture to introduce the neighborhood character of the Maclay District. Gateway monuments and landmark architecture at the intersection of Maclay Avenue and Eighth Street should relay high-quality design and put forth an appealing pedestrian environment.
2. Define the entrance to San Fernando at its border to the west at Sylmar using large scale prominent architectural features that transmit a message of urbanity and history far in excess of what the experience of San Fernando Road west of the city boundary has to offer as shown on the graphic to the right.

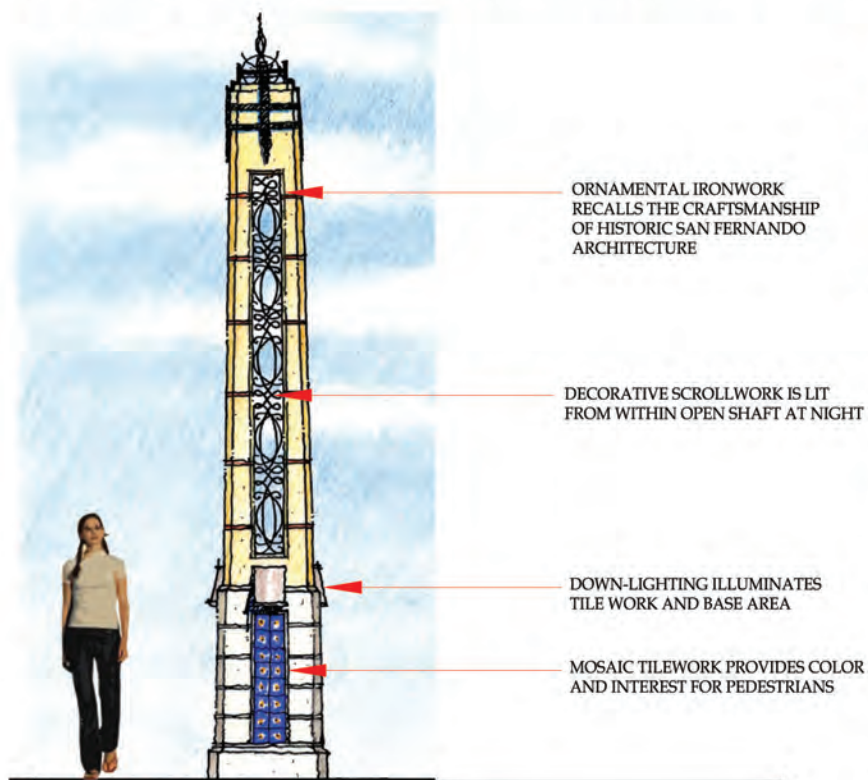
It should be noted that the gateway landmark treatment shown on page 192 is only a conceptual illustration of one such treatment for this location. The design of such a gateway landmark could easily be modified to reflect a preference by the City of San Fernando for Mission style architectural and landscape elements at this location similar to the existing gateway landmark treatment at the city’s eastern boundary, as was discussed by the City Council in approving the specific plan as a whole.

3. At the city’s eastern boundary with Pacoima, build upon the existing landscaped gateway and Cesar Chavez Memorial via the introduction of wayfinding signage that distinguishes the San Fernando and Truman Corridors. Signage should alert visitors to the location of the Downtown District, the San Fernando Mall, the Civic Center and Police Station, as well as other areas of interest. Consider the introduction of architectural landmark columns or other prominent features to complement the existing landscaped gateway.

CAPITAL IMPROVEMENTS - LANDMARK COLUMNS



View looking west along Truman Street towards San Fernando Mission Boulevard



Existing view looking west along Truman Street



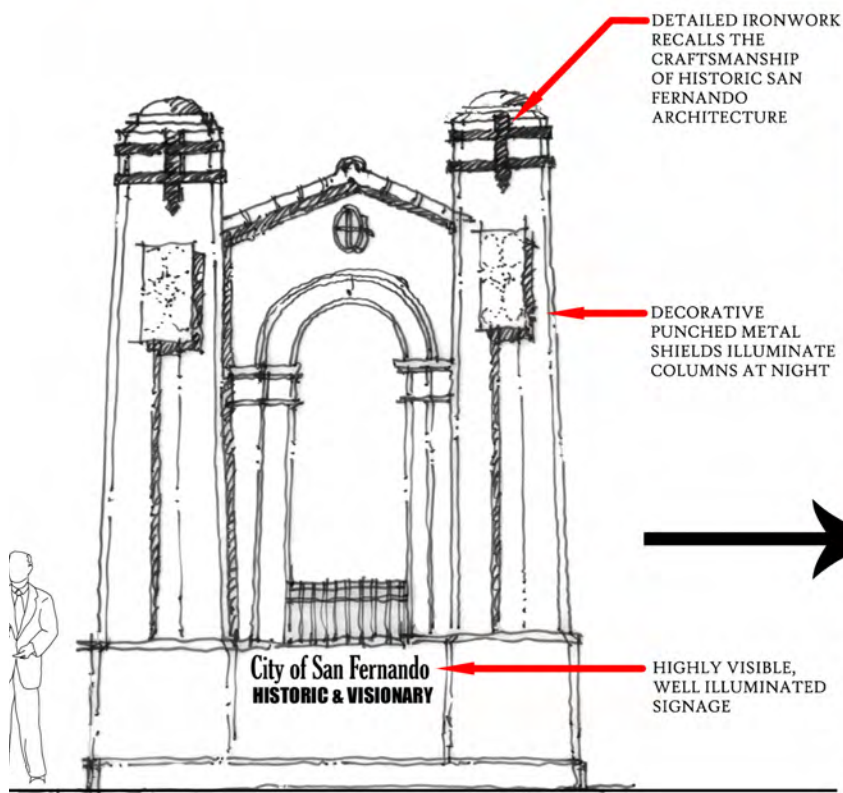
Proposed landmark locations within the downtown core

Landmark columns "stitch" together streets within the downtown core, bridging the gap between the San Fernando mall and the historic downtown and civic center.

CAPITAL IMPROVEMENTS - GATEWAY LANDMARKS



View of proposed City Gateway looking east along San Fernando Road



Existing view looking east along San Fernando Road



Location of proposed City Gateway

A strong architectural gateway provides a welcoming civic image and the overall style recalls architectural elements of historic San Fernando buildings

CHAPTER SEVEN: UTILITIES & INFRASTRUCTURE PLAN



This chapter covers the objectives of the San Fernando Corridors Specific Plan with respect to utilities infrastructure, and the general policies that will apply to new development in the specific plan area in this regard. It then provides a detailed description of existing utility infrastructure in place throughout the specific plan area, and identifies locations where improvements to this infrastructure are planned. These planned improvements are shown in Figure 7.1.

UTILITY INFRASTRUCTURE OBJECTIVES AND GENERAL POLICIES

A prime objective of the San Fernando Corridors Specific Plan is to cause the revitalization of the corridors planning area by encouraging new investments in infill development, particularly on underutilized parcels. Use of the existing utility infrastructure in this already urbanized area will facilitate such investment, and new development occurring pursuant to this specific plan will be accommodated by the existing utility infrastructure. Although modifications and upgrading of existing utility facilities may be necessary in conjunction with more intensive infill development at certain locations in the planning area, it is anticipated that the cost of such improvements will be provided for through the City of San Fernando's standard "capital facilities fees" that are charged to all new development on a proportionate basis.

The following are general policies relevant to the provision of water, sewer and storm drainage infrastructure to new infill development within the Specific Plan area.

1. Financing the cost of necessary utility improvements is the responsibility of the benefited properties. New development in the specific plan area will contribute to the cost of incremental upgrading of the utility system's capacity where and when necessary through payment of the City's standard "capital facilities fees" that are charged to all new development on a proportionate basis. The cost of providing or upgrading on-site utilities to an individual property will be borne by the applicant for new development of the site, and on-site improvement costs serving more than one property will be shared proportionately by the benefiting developments based on project demand and/ or discharge.
2. Installation, operation and maintenance of utilities should not adversely affect significant natural resources. Where such impacts are unavoidable, they shall be mitigated.

3. New development should provide for the efficient use of water through the use of natural drainage where feasible, drought tolerant landscaping and recycling. Public facilities and private and common open space shall be designed and landscaped to minimize water consumption.
4. Existing cast iron water main pipes should be replaced with ductile iron pipes over time through the City's capital improvements program, so as to improve the durability and to maintain the safety of the community's potable water system as a whole.
5. Development in the specific plan area shall not result in flows of storm water that diminish the prior quality of receiving waters, nor shall such development create an overall increase in storm water flows.

Specific requirements pertaining to utility construction and landscape improvements should be considered for each individual project prior to construction.

WATER SUPPLY SYSTEM

The City of San Fernando's water supplies consists of imported water from the Metropolitan Water District (MWD) and groundwater produced from the Sylmar Groundwater Basin ("Basin"). Groundwater flow in the Basin is generally from the Santa Susana Mountains and the San Gabriel Mountains in the north. The Basin is replenished naturally by percolation from precipitation and by stream flow and subsurface inflows from the Santa Susana Mountains and the San Gabriel Mountains. The total storage in the Basin is estimated to be approximately 310,000 acre-feet (AF), with a natural safe yield estimated to be approximately 6,810 acre-feet-per-year (AFY). This Basin has been adjudicated since 1984, of which the Cities of San Fernando and Los Angeles were granted equal share of the safe yield of the Basin, as determined by the Upper Los Angeles River Area (ULARA) Watermaster. The City's current allotted draw from the basin is 3,405 acre-feet per year, which represents approximately 94 percent of the City's total existing water demand.

The City maintains four active wells for groundwater extraction, which have a combined storage capacity of 8.9 million gallons. Between the years 2005 and 2009, these four wells produced an average of 3,292 AF of groundwater. The City, which became a member agency of MWD in 1971, receives imported water supplies from MWD to supplement its groundwater supplies on an as-needed basis.

Annual water use in the City between 2005 and 2009 has ranged from about 3,395 AFY to 3,575 AFY, with an average of 3,631 AFY. While the City typically is able to meet 100 percent of its demand from groundwater wells and maintain low levels of imported water purchases, projected water supply availability based on the City's adjudicated groundwater rights and preferential rights is 0.10% of MWD's annual supplies.

The City's current allotted draw from the basin is 3,405 acre-feet per year, which represents approximately 94 percent of the City's total existing water demand. Although the City's groundwater rights are fixed at 3,405 AFY, the City's overall water supply reliability is expected to remain consistent or improve slightly due to limited population growth and various conservation efforts.

Maclay Avenue Water

The existing water service line serving Maclay Avenue properties from Eighth Street to Seventh Street is an 8 inch diameter cast iron (CI) pipe which was put into service in 1975. Pursuant to Utilities Policy 4 above, it should be replaced with a new 8 inch ductile iron (DI) pipe. The existing 10 inch CI line running from Seventh Street to Fifth Street should also be replaced with a 12 inch DI pipe. The parallel 6 inch CI line running from Glenoaks Boulevard to Defoe Street should be abandoned and all services and fire hydrants should be reconnected to the newly installed 12 inch DI main line. Along Maclay Avenue from Fifth Street to First Street, there is currently no water main line installed as properties in this area are served by 8 inch DI pipes in the adjacent alleys parallel to Maclay Avenue. Based on expected future peak domestic and fire flow demands it is recommended that a new 12 inch DI main line should be installed in this area.

Truman Street Water

The existing water line serving Truman Street across the width of the city from its western to its eastern boundary is a 12 inch diameter DI pipe. It is anticipated that this existing water main will be sufficient to provide service to development pursuant to the specific plan.

San Fernando Road Water

The existing 8 inch diameter CI water line serving properties along San Fernando Road from Hubbard Street to Lazard Street is undersized for serving planned development in this part of the corridor, and should be replaced with a new 12 inch diameter DI pipe. From Lazard Street to Kalisher Street, the existing 12 inch DI line is adequate to meet the needs of development and land uses planned there. However from Kalisher Street to Wolfskill Street, the existing 8 inch CI water main

should be replaced with a new 12 inch DI pipe. The existing 8 inch DI pipe that runs from Wolfskill Street to the eastern boundary of the city is adequate for the development and land uses planned for that area.

SANITARY SEWER SYSTEM

Sewer lines in the City are maintained by the City's Department of Public Works, Sewer Maintenance Division. The treatment and disposal of effluent is currently provided under contract with the City of Los Angeles through the Hyperion Treatment system that includes the Hyperion Treatment Plant, the Donald C. Tillman Water Reclamation Plant (DCTWRP), and the Los Angeles-Glendale Water Reclamation Plant. The treatment capacity of the entire system is approximately 550 million gallons per day (mgd). The system currently has an average utilization of 362 mgd..

Maclay Avenue Sewer

Properties along Maclay Avenue currently receive sanitary sewer service through an assortment of 8 inch diameter vitrified clay pipes. However, from Eighth Street to Knox Street along Maclay Avenue, there is no sewer main line installed in Maclay Avenue as the adjacent properties currently receive service from sewer lines located in the alleys parallel to Maclay Avenue. Installation of a new vitrified clay pipe sewer line in this area may be required in conjunction with new infill development of the adjacent properties.

Properties along Maclay Avenue between Knox Street and Seventh Street are being served by two parallel 8 inch diameter clay pipes on the east and the west side of the street right-of-way, respectively. Properties along Maclay Avenue from Seventh Street to Lucas Street are being served on the east side of the street by an 8 inch clay pipe in the street, and on the west side by an 8 inch line that is located in the alley parallel to the west of Maclay Avenue. The area from Lucas Street to Glenoaks Boulevard is being served on the east side of the street by an 8 inch clay pipe that runs parallel to Maclay Avenue in an alley to the east, and an 8 inch clay pipe in the street that serves the west side of the street. Each of these lines is expected to adequately accommodate the proposed development and land uses in this part of the specific plan area.

From Glenoaks Boulevard to Fifth Street, properties on the east side of the street are being served by an 8 inch diameter clay pipe, and properties on the west side of the street are served by the line that runs parallel to Maclay Avenue in the alley to the west until Degarmo Street where it transitions to the street right of way along the west side of the street. Properties from Fifth

Street to First Street are served by 8 inch clay pipes located in the alleys along both sides of Maclay Avenue. No sewer main line replacements or up-grades are anticipated in order to serve expected new development and land uses in this part of the corridors planning area.

Truman Street Sewer

Properties along Truman Street from the western boundary of the city to Workman Street are served by an 8 inch diameter vitrified clay pipe that extends down the centerline of Truman Street. The area from Workman Street to San Fernando Mission Boulevard is served by two parallel 8 inch clay pipes, one on either side of the street beneath the existing sidewalks. From San Fernando Mission Boulevard to Brand Boulevard, the adjacent properties are served by a 10 inch clay pipe running through the alley between San Fernando and Truman Street. Each of these lines is expected to adequately accommodate the proposed development and land uses in this part of the Specific Plan area.

At Brand Boulevard, the main line increases in size to a 15 inch clay pipe that extends to the east until reaching Kittridge Street, where it continues east in an easement through the property located at 753 San Fernando Road. At Wolfskill Street, the 15 inch line continues south to Celis Street. No sewer main line replacements or up-grades are anticipated in order to serve new development and land uses in this part of the corridors planning area.

San Fernando Road Sewer

Properties located on the north side of San Fernando Road from the western boundary of the city to San Fernando Mission Boulevard are served by an 8 inch diameter vitrified clay pipe in Truman Street. Properties on the north side of San Fernando Road from San Fernando Mission Boulevard to the eastern boundary of the city are served by an 8 inch clay pipe that extends down the alley mid-block between Truman Street and San Fernando Road. Each of these lines is expected to adequately accommodate the proposed development and land uses in this part of the specific plan area.

Properties located on the south side of San Fernando Road from the western boundary of the city to Maclay Avenue are being served by a 15 inch diameter vitrified clay pipe in San Fernando Road. Properties on the south side of this street from Maclay Avenue to Brand Boulevard are served by an 8 inch clay pipe in the alley running mid-block between San Fernando Road and Celis Street. Properties on the south side of San Fernando Road between Brand Boulevard and the eastern boundary of the city are served from the sewer line in Celis Street.

No sewer main line replacements or up-grades are anticipated in order to serve expected new development and land uses in this part of the corridors planning area.

STORM DRAINAGE SYSTEM

Land within the specific plan area is generally paved or otherwise covered with impervious surfaces. As a result, no additional storm water infrastructure is anticipated to be required to accommodate storm water runoff from new development pursuant to the specific plan. Nevertheless, new development will be required to comply with Federal Clean Water Act requirements, and to obtain a National Pollutant Discharge Elimination System (NPDES) permit from the Los Angeles Regional Water Quality Control Board. The future development and land uses will also be required to comply with the City's storm water management guidelines

Maclay Avenue Storm Drains

There are no storm drain deficiencies found along Maclay Avenue between First Street and Eighth Street. Concrete gutters exist on both sides of the street for its entire length. There is a 33 inch diameter reinforced concrete pipe running south between Seventh Street and Glenoaks Boulevard with inlets at Seventh Street, Lucas Street and Glenoaks Boulevard on both sides. Storm water between Eighth Street and Glenoaks Boulevard is conveyed to Glenoaks and enters a 75 inch diameter Los Angeles County Flood Control (LACFC) trunk line that terminates at the Pacoima Wash. Storm water that accumulates on the west side of Maclay Avenue between Glenoaks Boulevard and Fourth Street enters a catch basin at Fourth Street. The remaining water between Fourth St and First Street enters a catch basin at First Street. Water on the east side between Glenoaks Blvd and First Street also enters a catch basin at First St. The water is carried to an 83 inch diameter LACFC trunk line that runs underneath First Street to the Pacoima Wash.

There is no history of localized storm drainage problems along this street. New infill development is not expected to generate significant additional amounts of storm water runoff since most surfaces are already paved or otherwise developed with impervious surfaces. Thus no storm drain system improvements are needed to serve anticipated future infill development in this part of the corridors planning area.

Truman Street Storm Drains

There are no storm drain deficiencies found along Truman Street. The street contains a series of concrete gutters and drain inlets extending the entire street length to convey storm water to various local trunk lines. The failure of sidewalk culvert drains at the intersections of

Lazard Street, Maclay Avenue, Brand Boulevard, and Wolfskill Street would result in inconsequential street flooding which is not anticipated to threaten structures. There is a city-owned 2' x 4' undersized reinforced concrete storm drain pipe at the intersection of Workman Street that runs south ultimately draining to the East Canyon Channel. The failure of this pipe would prove inconsequential at Truman Street.

There is no history of localized storm drainage problems along this street. New infill development in this vicinity is not expected to generate significant additional amounts of storm water runoff since most surfaces are already paved or otherwise developed with impervious surfaces. Thus no storm drain system improvements are needed to serve anticipated future infill development in this part of the corridors planning area.

San Fernando Road Storm Drains

There are several storm drain deficiencies present along San Fernando Road. There are no gutters installed on either side of the street between San Fernando Mission Boulevard and the western boundary of the city. A two foot concrete gutter is integral in limiting the spread of water on the pavement, and should be installed in conjunction with future street improvements in this area. The failure of sidewalk culvert drains at the intersections of Huntington Street, Kalisher Street, San Fernando Mission Boulevard, Brand Boulevard, and Wolfskill Street, would result in inconsequential flooding. However, the failure of the culvert at San Fernando Road and Maclay Avenue will cause moderate to severe flooding of the downtown mall pedestrian shopping district. The finished floor elevations of many of the commercial buildings on San Fernando Road between San Fernando Mission Boulevard and Maclay Avenue are below the street's centerline elevation. .

Existing problems with storm drainage in this part of the corridor area will be corrected over time through storm drainage improvements to be required in conjunction with new infill development in this area, as necessary to minimize any property loss from flooding and to enhance community safety.

STORM RUNOFF POLLUTION CONTROL

The majority of the planning area is currently paved and/or covered with impervious surfaces, which leads to the accumulation of debris, leaves, soils, oil, grease, chemicals, air contaminant residue and other pollutants within off-street parking lots. Since such pollutants may enter the storm drain system during periods of rainfall, future infill development will be required to implement storm water pollution control measures and to obtain storm water runoff permits pursuant to the National Pol-

lutant Discharge Elimination System (NPDES) requirements. Given the developed character of the planning area, there will not be a significant net increase in the amount of quality of storm water runoff.

Policies related specifically to the management of storm runoff in general and pollution control in particular with respect to new development within the corridors planning area include the following:

1. Treatment of storm flows will be required to reduce or eliminate the particulate matter washed into the storm drain system in order to obtain a storm water discharge permit in accordance with NPDES requirements.
2. Prior to issuance of an occupancy permit, a storm water management plan utilizing best management Practices to control or reduce the discharge of pollutants to the maximum extent practicable shall be prepared and approved by the Public Works Director.
3. Future development must demonstrate compliance to the pertinent NPDES requirements concerning industrial wastewater discharges prior to issuance of the occupancy permits.

CHAPTER EIGHT: IMPLEMENTATION



This chapter is an overview of recommendations for the implementation of corridor revitalization as set forth in this specific plan. The actions to be taken to attain revitalization are broadly divided into policy tools, capital improvements, and assistance programs. Within an action plan that will serve as a roadmap to revitalization, individual actions are prioritized by importance into a recommended sequence of implementation.

The action plan begins with short-term efforts over the next three years. At the end of this short-term phase, the City should review and update the long-term implementation actions that follow, giving priority to subsequent actions according to conditions on the corridors at that time.

ACTION CATEGORIES

Implementation of the San Fernando Corridors Specific Plan requires coordinated action by the City in several related but distinct areas of activity. The primary categories of intervention are:

Policy Tools

The policies and regulations of the plan are the “nuts and bolts” of the vision for corridor revitalization, especially with regard to harnessing private investment to serve as the primary engine for change. They provide the City with its strongest mechanisms to shape private investment towards the vision established by the community. The development standards contained within this plan will ensure that site configurations, uses, and intensities of developments and their resulting activity will achieve the urban design and revitalization goals for the corridors. The design guidelines will lay out the criteria for the character of architecture and site design that will fit San Fernando and will serve as a framework for design review by City staff. By providing explicit directions to investors as to “how we build here in San Fernando,” they lay out a clear path towards more rapid approvals and thus give incentive to project designs built according to the community’s vision.

Capital Improvements

Capital improvements set the stage for revitalization. The primary benefits behind improvement projects for streets, public open spaces, and transportation infrastructure are twofold: first, they can catalyze new private investment in a particular district by demonstrating to outside investors and residents alike that the City has a strong commitment to change. Second, they create a re-configured neighborhood setting tailored to support the desired types of developments and give them a greater likelihood of success. Residents also benefit from the greater livability and community pride that comes from

an attractive public realm. For a detailed description of the proposed capital improvements, refer to the Chapter 6, Capital Improvements.

Assistance Programs

Assistance programs aid existing and new businesses along the corridors. By providing informational, design, logistical, financial or other types of support to businesses, the City can provide incentives to private investors to fulfill aspects of specific plan recommendations. Informational assistance can be as simple as educating private investment about the possible opportunity sites along the corridor, or providing developers with a clear and simple process for approvals. It can also include assistance and education on business practices and help in getting access to government or non-governmental organization programs. Design assistance may come in the form of programs to provide storefront, signage, or window display design or educational services to existing businesses. Logistical support can include assisting the relocation of businesses to more suitable sites within the city and the recruitment of desired business types into a district from the outside. Financial assistance can take on many forms, including grants and grant application assistance, revolving loan funds, and tax increment financing.

POLICY IMPLEMENTATION

As provided for under state enabling legislation, the San Fernando City Council has adopted this Specific Plan as an ordinance of the City. This was done in conjunction with a corresponding amendment to the San Fernando General Plan and land use map, and an amendment of the City’s zoning code and zoning map to reference this specific plan. This procedure ensures consistency between this specific plan and the City’s general plan, and allows the land use regulations, development standards and design guidelines of this specific plan to directly govern new development within the specific plan area just as the City’s zoning code does in other areas of the community.

An environmental impact report (EIR), as authorized by the California Environmental Quality Act (CEQA), was prepared to assess and address the potential environmental impacts of the San Fernando Corridors Specific Plan. The significant impacts identified were noise during construction, the unearthing of subsurface cultural resources during construction, and decreases in intersection performance due to automobile traffic. All these impacts can be mitigated with the appropriate mitigation measures. In conjunction with the approval of a negative declaration of environmental impact, the City has also prepared a mitigation monitoring program as required by Public Resources Code Section 21081.6, to

ensure compliance during project implementation. The adopted program will apply to changes made to the project or conditions of project approval in order to mitigate or avoid any significant effect on the environment.

In order to ensure that the policies, standards, and design guidelines contained within Chapters 4, 5, and 6 are used most effectively, the City should take steps to ensure successful internal administration for the specific plan. The staff responsible for its administration should fully understand the document, its vision and its policies, particularly as they pertain to the review and approval of projects.

Per the City's zoning code, site plans shall continue to be reviewed by the Community Development Director or his/her authorized staff for conformity with this specific plan. Only in specific or unique cases where a proposed project could have a major impact on the public realm and/or surrounding projects, will projects be subject to commission review and approval. In these instances, the site plan shall be submitted to the commission and the items in question shall be placed on the agenda. The commission may approve or disapprove with conditions on the site plan.

Action Plan

It is important to structure an implementation strategy that will start and maintain the momentum of private investment interest and garner public support. In most cases, this means achieving measurable success through short-term achievements that occur within an initial three year window. If visible measures of success are not available by this time, the momentum of the process may falter and hinder achievement of future project goals. With this in mind, it is essential to use the *Policy Tools*, *Capital Improvements*, and *Assistance Programs* referred to above in the order that makes the most sense for the unique conditions of the City.

This section lists the actions that should be taken to achieve revitalization in the approximate sequence that they should occur. The list is divided into two parts: short-term actions, to be completed within the first three years after adoption of the San Fernando Corridors Specific Plan, and longer-term actions, to be revisited after the first three-year window of the Plan.

Short-Term Actions

- Implement the policy tools of the specific plan. Establish staffing resources and procedures to support consistent and thorough review procedures. Clearly communicate the role of the specific plan and its development standards and guidelines to the investment community. This may be achieved

through press releases, seminars, and other venues.

- Establish clear leadership and lines of responsibility for the implementation of revitalization. Revitalization strategies are by nature complex and multifaceted; challenges usually overlap departmental categories and can often lead to diffused or conflicting responses. Successful revitalization efforts inevitably have a champion at a departmental leadership level; those that do not have a low chance of success. We recommend that a staff member be assigned under such a "champion" as a full or part-time coordinator of the revitalization effort. It may be appropriate to train or recruit this person to acquire training or have the experience of a downtown coordinator, which is a position that a number of California cities have established.
- Focus committee and commission review and approval on the design standards and guidelines, in order to enable designated City staff to perform typical development review applications. Conserve committee and commission purview for special review of public and community facilities and conditional use applications.
- Set up specific financing plans for major capital improvements required to support development along the corridors. Continue to apply for grants and other funding sources for capital improvements for corridor improvements, as the City has successfully done for Maclay Avenue.
- Implement capital improvements to stimulate investment and create supportive district settings. Begin with:
 1. Streetscape improvements within the Downtown District. Priority should be given to the section of Truman Street between Mission and Brand Boulevards.
 2. Streetscape improvements along San Fernando Road within the Mixed-Use Corridor District. Improvements should be made first to the segment between San Fernando Mission Boulevard to Huntington Street, followed by the section from Huntington Street to the city's western border.
 3. Streetscape improvements along Truman Street from Mission Boulevard to western border along the Mixed-Use Corridor District.
 4. A city gateway feature at the northwestern city boundary on Truman/San Fernando (outside the city boundary – to be negotiated with the

City of Los Angeles, as was similarly done for the south-eastern boundary). Part of the design should include attractive signage or markers to assist visitors to choose between Truman Street or San Fernando Road – by indicating which city attractions are accessible from each road.

- Identify opportunity sites for infill and development. Acquire and assemble parcels to create viable opportunity sites where possible. Market these sites to developers to incite interest in new large-scale projects, particularly housing development.
- Proactively recruit the kinds of businesses that will contribute the most to the community to the Downtown District. Use inducements such as low interest loans and grants to entice new establishments to locate within the downtown. Assist businesses to relocate to more appropriate spaces within the city.
- Work with downtown businesses and organizations like the Northeast San Fernando Valley Chamber of Commerce to encourage “after 5:00” business hours throughout the downtown. Promote “special event” evenings, in cooperation with civic events or entertainment, to initiate later operating hours on certain nights.
- Implement a signage assistance program as a grant program to existing businesses to provide incentives for rapid and highly visible improvement and change in the Downtown District. Set up the program to provide grants to pay for design, fabrication and installation of improved signage for existing businesses. Business owners/operators whose applications were approved would be consulted by a City-selected sign design/ fabrication/ installation company, to develop new sign designs from the owner/operator’s input and according to the standards included in the specific plan.
- Consider adopting a mural ordinance to allow the installation of murals on buildings within the Specific Plan area. Murals will nurture creative and artistic expression in the public realm and contribute to downtown San Fernando’s distinctive sense of place. Provisions of the ordinance should include mural placement, content, and installation requirements, as well as contract requirements specifying the lifetime of the mural (after which the mural can be painted over) and maintenance responsibilities should the mural be damaged or need touch-up.

Long-Term Actions

- Design and implement improved public street-scape and paseo connections between the Civic Center and the Downtown.

- Implement a corridor signage and way-finding program to help commuters, visitors, and residents navigate the corridors in a legible way, marking interest points and major destinations. Insure that directions to public parking facilities are well-marked.
- Look for upcoming site opportunities to create a public plaza space in the Downtown District as a gathering place for community and special events.
- Establish a program to replace existing low-performance street lights with higher quality lighting. For example, replace sodium street lighting along the San Fernando Mall with high quality warm white, pedestrian-scale lights. Long-life induction lighting is recommended for low maintenance and energy efficiency.
- Improve public parking lots serving the San Fernando Mall at Truman Street. Improve pedestrian access and area lighting for lots. Consider the pedestrian arcade concept along the backs of the shops as advocated by the 1985 Downtown Master Plan, and explore opportunities to create mid-block paseo connections from these rear parking lots to the San Fernando Mall (San Fernando Road).
- As parking demand increases, develop a parking strategy for coordination of shared parking (to maximize the efficiency of existing surface lots, whether public or private) and eventual targeting of sites for a municipal park-once, parking structure.

FINANCING

Private Investment

New development on privately owned land within the specific plan area will generally be financed by developers with conventional funding from private lending institutions. The intent of the specific plan provisions operating in concert is to create strong incentives for widespread private sector investment in the corridors without recourse to the limited resources of public financial assistance.

However, it should be noted that other sources of regulatory and or financial assistance may also be available to development projects through existing legislation or through programs from other agencies at the regional and state level. For example, pursuant to California Government Code Section 65915, proposals for residential or mixed use development that include a sufficient number of dwelling units reserved for occupancy by low to moderate income residents may qualify for a residential density bonus above the maximum density otherwise permitted under this specific plan, among other possible regulatory concessions.

Public Investment

Public investment within the specific plan area will generally be limited to development of public improvements on publicly owned land such as the public right-of-way. Such improvements are described in detail in the Capital Improvements, Circulation and Utility Infrastructure Chapters of this specific plan.

It is intended that such improvements be financed through a combination of grant and revenue sources dedicated for such public improvements. For example, most of the street improvements outlined in this specific plan for Maclay Avenue in the Downtown District and in the Maclay District will be financed by federal transportation funds received through the Los Angeles County Metropolitan Transportation Authority, and by state gas tax funds for such capital improvements through the City's capital improvements program. Such a combination of dedicated funding sources will also be sought by the City for improvements per this specific plan to San Fernando Road and Truman Street in the Downtown District and in the Mixed-Use Corridor District. Where private development occurs on property fronting street segments in advance of such public streetscape improvements, the private development will generally be responsible for such improvements as determined through the development approval process.

As discussed in the Utilities Infrastructure Chapter of this specific plan, incremental improvements as necessary to public utilities infrastructure, including the water supply system, the sanitary sewer system and the storm drainage system, will be provided through the City's capital improvements program. Such improvements are financed by payment of the City's standard "capital facilities fee" that is charged to all new development on a proportionate basis.

Implementation of this specific plan is not expected to have any negative fiscal impact on the City of San Fernando's general fund. Moreover, private development pursuant to the specific plan and subsequent reassessment of increased property values is expected to augment property tax revenues to the City.

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IV.**LAND USE ELEMENT****INTRODUCTION AND BACKGROUND****General Plan Legislation**

The adoption of a General Plan by cities and counties became a legal requirement 33 years ago in 1955. At that time, a General Plan consisted only of a Land Use Element and a Circulation Element. In 1971, the Legislature created one of the most important features of State planning law by requiring that zoning and subdivision approvals be consistent with the General Plan.

Government Code Section 65302 (a) defines the Land Use Element as follows:

A Land Use Element which designates the proposed general distribution and general location and extent of the uses of the land for housing, business, industry, open space, including agriculture, natural resources, recreation, and enjoyment of scenic beauty, education, public buildings and grounds, solid and liquid waste disposal facilities, and other categories of public and private uses of land. The Land Use Element shall include a statement of the standards of population density and building density recommended for the various districts and other territory covered by the plan. The Land Use Element shall also identify areas covered by the plan which are subject to flooding and shall be reviewed annually with respect to such areas.

Purpose and Function

The purpose of the Land Use Element is to establish a pattern for compatible land uses to reflect existing conditions and to guide future development. By law, the element must set clear standards for the density of population and the intensity of development for each proposed land use category. The Land Use Element which has the broadest scope of the seven required elements, provides a composite discussion of the issues which are addressed in the other elements through text, diagrams and a land use map.

The General Plan, particularly the Land Use Element, governs the future direction of a local jurisdiction. Prior to 1971, the General Plan was considered just a guideline; however, because of

the consistency requirement enacted in 1971, the General Plan has assumed greater importance. Consistency with the adopted General Plan is now mandatory for all land use approvals.

In addition to the seven mandated elements of the General Plan, other optional elements may be included to address areas of concern that, in the judgement of the City, relate to the physical development of the community. Once an optional element has been adopted, it is as important and legally binding as a mandatory element.

Section 65300.5 of the Government Code requires that the General Plan be consistent throughout (i.e., 'internally consistent'). The assumptions and projections used in the Housing Element, for instance, must be consistent with those used in the Land Use Element and the Open Space Element. Local policies must not conflict with Statewide policies pertaining to housing, open space and environmental quality.

Land use categories designated by the General Plan must be defined in terms of population density and building intensity. Allowable uses for each district must be indicated. Density ranges must be given for residential designations to facilitate implementation through zoning districts. Zoning laws regulate the geographic allocation and allowed uses of land. Finally, the Zoning Ordinance must conform to the adopted General Plan. It is one of the implementation measures used to achieve the goals and objectives of the General Plan.

Several zoning designations may be encompassed within one General Plan land use category. Therefore, the zoning of a property may change without requiring a change in the land use designation. The General Plan has a long term perspective. It must be structured enough to provide overall direction with adequate flexibility to accommodate changing conditions.

ISSUES AND OPPORTUNITIES

The former San Fernando General Plan contained 12 land use designations. The 12 designations were as follows: 1) the Residential land use designations included Low Density Residential (LDR), Medium Density Residential (MDR), and High Density Residential (HDR); 2) the Commercial land use designations Included Central Business District (CBD), Neighborhood Shopping, Highway Related, Automotive Sales and Services, and Multi-Use; 3) the Industrial land use designations included light Industrial and Industrial; and 4) Other land use designations Include Public/Quasi-Public, and Neighborhood Park/Landscaping.

A major focus of the General Plan Update and the efforts of the Citizens Advisory Committee was to reexamine the appropriateness of these land use designations and how they have been applied to different areas of the City.

Upon review of available data and existing development conditions, the following issue areas emerged as warranting more detailed analysis from a land use planning standpoint:

1. Central Business District
2. San Fernando -Truman Area
3. Kalisher Street
4. Mission Boulevard
5. Airport Site
6. Glenoaks Boulevard
7. Medium Density/Neighborhood Commercial
8. Recreation Park Surplus Land
9. Brand Boulevard High Density
10. Multi-Use on Maclay Avenue

Each of these issues are described in Part A of the Technical Appendix. Background data are presented for each issue in the following terms:

1. Description of the Land Use Issue
2. Existing Conditions and Surrounding Land Uses
3. Current Land Use and Zoning Designations
4. Identification of Problems and Opportunities
5. Alternative Courses of Action

Exhibit ~~IV-III~~IV-I shows the location of the various areas. A synopsis of land use issues follows.

Central Business District

There is no zoning designation to implement the Central Business District land use designation. More specific guidance is needed to realize the economic potential of CBD and to create a distinct visual identity for this area. The San Fernando Corridors Specific Plan (SP-5) provides new zoning categories that encourage the concentration of various retail business and civic uses that promote pedestrian activity and encourage economic development at a scale suitable for local-serving commercial uses. These zoning categories are described in more detail in the “Implementation Measures” Section below.

San Fernando -Truman Area

In this portion of the City, the existing Land Use Element designation is not consistent with the current zoning. The land use designation is Industrial and the zoning categories are M-2, C-2, and R-2. The San Fernando Corridors Specific Plan (SP-5) introduces new zoning categories that replace the underlying industrial land use designations and zoning categories with a mix of residential, office, retail, and service uses. The intent of the updated zoning is to encourage reinvestment in this area, and to allow a mix of new jobs producing businesses, good quality infill housing, and local-serving retail and restaurant uses within comfortable walking distance of transit. For the parcels located between Truman Street and the railroad tracks, light industrial uses are still allowed and, per the direction of the Flex-Use Corridor Overlay, may be converted to the mix of use types permitted in the remainder of the San Fernando – Truman Area. These zoning categories are described in more detail in the “Implementation Measures” Section below.

Kalisher Street

The primary land use issues associated with this area are incompatibility of existing uses and land use intensity. Kalisher Street is designated Multi-Use Commercial on the General Plan and the surrounding neighborhood is designated Medium Density Residential. Kalisher Street contains a mixture of residential, commercial and quasi-public uses.

Mission Boulevard

The study area is presently designated Multi-Use Commercial on the General Plan. The area contains both commercial and residential uses. The issue is whether to retain the Multi-Use designation or adopt a Commercial designation.

Airport Site

The issue regarding this study area is the choice of an appropriate designation for the former San Fernando Airport, located in the northeastern portion of the City.

Multi-Use on Maclay Avenue

The issue is whether to retain the Multi-Use designation or change the designation to commercial, which reflects existing development. The San Fernando Corridors Specific Plan (SP-5) introduces the Maclay District zoning category to properties located on both sides of North Maclay Avenue, beginning north of Fourth Street and extending to the northerly boundary line of the City. The Maclay District promotes the creation of new housing opportunities, while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. The Maclay District includes the Neighborhood Services overlay, which promotes mixed-use development at and near the intersections of Maclay Avenue with Glenoaks Boulevard, and with Eighth Street. The Maclay District zoning category is described in more detail in the "Implementation Measures" Section below.

Glenoaks Boulevard-Medium Density/Neighborhood Commercial

While this area is designated Medium Density and Neighborhood commercial on the General Plan, development has remained primarily low density residential. The San Fernando Corridors Specific Plan (SP-5) introduces the Maclay District zoning category to the parcels located on both sides of North Maclay Avenue, beginning north of Fourth Street and extending to the northerly boundary line of the City. The Maclay District includes the Neighborhood Services Overlay, which promotes mixed-use development at and near the intersections of Maclay Avenue with Glenoaks Boulevard, and with Eighth Street. The Maclay District and accompanying Neighborhood Services Overlay apply to the parcels at and near the intersection of Glenoaks Boulevard and Maclay Avenue. The Maclay District zoning category and Neighborhood Services Overlay are described in more detail in the "Implementation Measures" Section below.

Recreation Park Surplus Land

The issue is whether to retain this property as a park site on the revised General Plan.

Brand Boulevard High Density Residential

Within this area, the zoning is not consistent with the General Plan designation. The study area is shown on

the General Plan as High Density Residential. Zoning is R-1, Single Family Residential.

~~Multi-Use on Maclay Avenue~~

~~The issue is whether to retain the Multi-Use designation or change the designation to commercial, which reflects existing development. The San Fernando Corridors Specific Plan (SP-5) introduces the Maclay District zoning category to properties located on both sides of North Maclay Avenue, beginning north of Fourth Street and extending to the northerly boundary line of the City. The Maclay District promotes the creation of new housing opportunities, while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. The Maclay District includes the Neighborhood Services overlay, which promotes mixed-use development at and near the intersections of Maclay Avenue with Gleneaks Boulevard, and with Eighth Street. The Maclay District zoning category is described in more detail in the "Implementation Measures" Section below.~~

Goals and Objectives

Goals:

- To retain the small town character of San Fernando.
- To promote economic viability of commercial areas.
- To maintain an identity that is distinct from surrounding communities.

Objectives:

- To conserve single family neighborhoods.
- To attract new commercial activities, particularly within the downtown area.
- To revitalize the City's commercial corridors so they better represent the downtown's mixed-use heritage and small-town character of the City.
- To allow and encourage greater mixed-use development in appropriate locations.
- To enable the creation of a walkable, mixed-use, multi-modal environment that accommodates retail, office, light industrial, and residential uses within walking distance of both the Sylmar/San Fernando Metrolink Station, and Downtown San Fernando.
- To facilitate the transition of the Maclay Avenue, Truman Street, San Fernando Road, and First Street corridors into multi-modal streets that complement the land uses and development pattern planned for the corridors through implementation of the specific plan.

Implementation Measures

Leading up to the adoption of the General Plan in 1987, possible land use alternatives for the various issue areas were reviewed by the General Plan Citizens Advisory Committee at several meetings. The Committee meetings included a workshop held at the Santa Rosa Church to solicit comments from residents and business owners in the Kalisher Street area. The Planning Commission held several hearings to receive public testimony and to consider the range of land use alternatives for each area under study. The Planning Commission recommendations were then submitted to the City Council.

In the fall of 2002, the City of San Fernando began a planning effort to revitalize the Maclay, Truman and San Fernando corridors. While past planning processes had examined independent components of streetscape design and individual development sites, never before had the City undertaken a process that brought together the various components of city planning – street configuration and design, streetscape design, land use, development standards and design guidelines – into a single planning and policy document. As a demonstration of their commitment to the revitalization of these corridors, the City chose to use one of its most powerful policy tools, a specific plan, to guide the restructuring of the three corridors. After an extensive community outreach process that included three public workshops, the Specific Plan was reviewed by the Planning Commission and adopted by the City Council in January 2005.

In June 2012, the Los Angeles Metropolitan Transit Authority (Metro) Board released an application for Round 3 of the TOD Planning Grant Program. The TOD program encourages cities along major transit corridors such as the Metrolink Railway to make regulatory changes necessary to facilitate infill and transit oriented development projects that are consistent with regional transportation plans. In February 2013, the Metro Board awarded the City a grant to prepare an amendment to the San Fernando Corridors Specific Plan (SP-4), along with an associated General Plan amendment, Zoning Ordinance amendment, and Environmental Impact Report (EIR). Through an extensive community process that included four public workshops, four meetings with a Development Advisory Committee (DAC), and reviews by the Planning Commission, the City Council adopted the updated Specific Plan (SP 5), the associated Environmental Impact Report (EIR), and the necessary General Plan amendments and Zoning Ordinance amendments in December 2017. The 2017 Specific Plan (SP-5) replaced the 2005 Specific Plan (SP-4) in its entirety.

The recommended land use designations for the issue areas are identified below.

Central Business District

Change the CBD boundaries to the following:

- North - Railroad Tracks
- West - Mission Boulevard
- East - Chatsworth Drive
- South - Celis Street

The Corridors Specific Plan (SP-5) introduced the Downtown District, which encompasses the Central Business District land use designation, encourages the concentration of various retail business and civic uses that promote pedestrian activity. Uses encouraged within the Downtown District include smaller specialized retail shops, restaurants, entertainment venues, galleries, personal services, business services, and, within certain areas, upper floor residential uses.

San Fernando - Truman

~~From the railroad track to Truman Street, Industrial except for the corner parcel bounded by the railroad track to the north, Hubbard Avenue to the west, Meyer Street to the east and Truman Street to the south. That parcel and the area from Truman Street to San Fernando Road will be commercial.~~The Corridors Specific Plan (SP-5) introduced the Mixed-Use Corridor District, which encompasses and replaces the underlying industrial land use designations and zoning categories, encouraging the development of a mix of use types, ranging from residential and office uses to retail stores and services, at a lower intensity and scale than the Downtown District.

Kalisher Street

Kalisher Street will be commercial from Pico to Coronel, then medium density residential from the south side of Coronel to the City limit.

Mission Boulevard

This area will be commercial from Pico to Mott, then medium density residential from the south side of Mott to the City limit.

Airport Site

The northern third of the property will be commercial. The remainder will be industrial.

Maclay Avenue

Change of designation to commercial. The San Fernando Corridors Specific Plan (SP-5) introduced the Maclay District zoning category to the parcels located on both sides of North Maclay Avenue between Fourth Street and the northerly boundary line of the City. The Maclay District promotes the creation of new housing opportunities, while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. Examples of permitted uses include residential and commercial uses that are compatible with residential development. The parcels located on the northwest, northeast, and southeast corners of Glenoaks Boulevard and Maclay Avenue are also subject to the San Fernando Corridors Specific Plan's Neighborhood Service Overlay, which promotes mixed-use development that includes upper floor residential and work-live uses in conjunction with first floor commercial uses.

Glenoaks Boulevard

The area bounded by Pioneer Park to the north, Harding to the west, Alexander to the east and Glenoaks to the south will be medium density. The area bounded by Lucas to the north, Brand to the east, Glenoaks to the south and an alley to the west will also be medium density. The remainder of that study area will be low density residential. The Maclay District includes the Neighborhood Services Overlay, which promotes mixed-use development at and near the intersections of Maclay Avenue with Glenoaks Boulevard, and with Eighth Street. The Maclay District applies to the parcels at and near the intersection of Glenoaks Boulevard and Maclay Avenue, while the Neighborhood Services Overlay applies to the parcels located on the northwest, northeast, and southeast corners of Glenoaks Boulevard and Maclay Avenue.

Residential Park

Retain Park land use designation.

Brand Boulevard

This area will be office professional from Celis to the north side of Kewen, the low density from the south side of Kewen to O'Melveny. The portion of Brand Boulevard between Celis Street and Pico Street is governed by the Downtown District of the San Fernando Corridors Specific Plan (SP-5). The Downtown

District encourages the concentration of various retail business and civic uses that promote pedestrian activity. Uses encouraged within the Downtown District include smaller specialized retail shops, restaurants, entertainment venues, galleries, personal services, business services, and, within certain areas, upper floor residential uses.

Maclay Avenue

~~Change of designation to commercial. The San Fernando Corridors Specific Plan (SP-5) introduced the Maclay District zoning category to the parcels located at and near the intersection of Gleneaks Boulevard and Maclay Avenue. The Maclay District promotes the creation of new housing opportunities, while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. Examples of permitted uses include residential and commercial uses that are compatible with residential development. The parcels located on the northwest, northeast, and southeast corners of Gleneaks Boulevard and Maclay Avenue are also subject to the San Fernando Corridors Specific Plan's Neighborhood Service Overlay, which promotes mixed use development that includes upper floor residential and work-live uses in conjunction with first floor commercial uses.~~

CURRENT LAND USE DESIGNATIONS

The San Fernando General Plan contains 13 land use designations. The 13 designations, which are defined in Chart IV-1, are listed below:

Residential

1. Low Density Residential (LDR)
2. Medium Density Residential (MDR)
3. High Density Residential (HDR)

Commercial

4. Central Business District (CBD)
5. Neighborhood Shopping (COM)
6. Highway Related Commercial (COM)
7. Automotive Sales and Services (COM)
8. Multi-Use (MU)

Industrial

9. Light Industrial (IND)

10. Heavy Industrial (IND)

Other

- 11. Public/Quasi-Public (PUB)
- 12. Neighborhood Park/Landscaping (PRK)
- 13. San Fernando Corridors Specific Plan (SP-5) - including the Maclay District, the Downtown District, the Mixed-Use Corridor District, the Automobile Commercial District, the Workplace Flex District, and the General Neighborhood District

Chart IV-I lists the intent and maximum intensity allowed in each land use designation. The complete Land Use Element Map is set forth as Exhibit ~~IV-IV~~IV-II.

SAN FERNANDO CORRIDORS SPECIFIC PLAN LAND USE DESIGNATION (~~SP-4~~SP-5)

The San Fernando Corridors Specific Plan (Corridors Specific Plan) was originally adopted by the City ~~on 2004~~ in 2005 and revised in 2017. The Corridors Specific Plan is intended to implement development strategies for the revitalization of the City's primary commercial corridors, namely Truman Street, San Fernando Road, Maclay Avenue, and First Street. The Plan applies to approximately 150 acres of formerly residential-, commercial-, and industrial-zoned properties, which are identified on the Land Use Element Map by the San Fernando Corridors Specific Plan (~~SP-4~~SP-5) land use designation. The purpose of the Corridors Specific Plan is to transform the City's downtown and adjacent supporting districts into attractive, livable, and economically vital places, while preserving existing residential neighborhoods.

The Corridors Specific Plan contains a detailed statement of the community's vision for the future of the corridor areas, as well as the revitalization strategy and urban design principles to be used in achieving that vision. The Corridors Specific Plan includes specific land use regulations, development standards and design guidelines applicable to new development in the corridor areas, as well as a program of public improvements for the streets and sidewalks in the corridor areas, so as to improve their function and appearance. The development standards and design guidelines of the Corridors Specific Plan have been tailored to each District and Sub-District in order to require high quality design and architecture that is consistent with the proposed uses allowed within the Corridors Specific Plan Area.

The Corridors Specific Plan contains the following ~~three~~six Districts:

1. Maclay District
2. Downtown District
3. Mixed-Use Corridor District
4. Automobile Commercial District
5. Workplace Flex District
6. General Neighborhood District

~~The six Districts are shown in Exhibit IV-III, A general description of the Maclay Avenue, Downtown and Truman/San Fernando these Districts, including the location and size of the district, permitted land uses, the intent for the desired character and intensity, and any applicable overlays as well as an explanation of the types of land uses permitted within each District, is as follows:~~

1. The Maclay Avenue District:

~~Location: This~~ Covering approximately 33 acres, the Maclay District includes properties located on both sides of North Maclay Avenue, beginning north of Fourth Street and extending to the northerly boundary line of the City, approximately 100 feet north of Eighth Street, as shown in Exhibit IV-III. The Maclay District encompasses the majority of the Maclay Street area, identified in Exhibit IV-I, and discussed earlier in this Chapter.

~~Prior Designation:~~ Prior to the adoption of the Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the Commercial (COM) land use designation.

~~Permitted Uses: Residential and commercial uses that are compatible with residential development.~~

~~Intent/Character: The Maclay District – This District will serve as a neighborhood spine for the community and will become a residential-oriented corridor as a means to promote the creation of new housing opportunities (i.e., condominium and/or town home opportunities), while, at the~~

same time, maintaining the integrity of the existing adjacent residential neighborhoods. Examples of permitted uses within the Maclay District include residential and commercial uses that are compatible with residential development.

Overlays: The Maclay District includes the Neighborhood Services Overlay, which applies to the parcels at and near ~~At the intersections of Maclay Avenue with Glenoaks Boulevard, and with Eighth Street, Neighborhood Service Overlay Areas.~~ The Neighborhood Services Overlay will encourage the clustered development of ~~pedestrian-oriented, locally-serving convenience uses that will encourage pedestrian activity.~~

2. The Downtown District:

Location: Covering approximately 32 acres, ~~This~~the Downtown District includes properties located on both sides of North Maclay Avenue between ~~Truman Street and~~ Fourth Street ~~and the railroad right-of-way, and properties located on the south side of Truman Street and on both sides of San Fernando Road, between San Fernando Mission Boulevard and South Brand Boulevard~~and the whole block containing City Hall bounded by Macneil Street. Moving southward, it includes all properties bounded by the railroad, Chatsworth Drive, San Fernando Mission Boulevard, and Pico Street. ~~The Downtown District contains two Sub-Districts: City Center and the San Fernando Mall.~~ The Downtown District encompasses the majority of the Central Business District area, identified in Exhibit IV-I, and discussed earlier in this Chapter.

Prior Designation: Prior to the adoption of the Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the Commercial (COM) and Central Business District (CBD) land use designations. ~~The Downtown District, together with the Maclay Avenue District, encompass all of the Maclay Street area discussed earlier in this Chapter and identified on Exhibit IV-II.~~

Permitted Uses: Uses that generate the most pedestrian activity, including smaller specialized retail shops, restaurants, entertainment venues, galleries, personal services, and business services. Complementary uses such as the Civic Center also support the planned development of the downtown area.

Intent/Character: The Downtown District is established for the purpose of creating a lively “center of the city” where the community of San Fernando comes together. — This District is intended to facilitate development that will serve as a focal point of activity for the City. — This the Downtown District will promote the concentration of various retail business and civic activities as a means to facilitate pedestrian activity. The uses that are encouraged within the Downtown District, and those that generate the most pedestrian activity, include smaller specialized retail shops, restaurants, entertainment venues, galleries, personal services, and business services. Complementary nearby uses such as the Civic Center will also support the planned development of the downtown area. Buildings are required to be located directly at the back of sidewalk with an active storefront expression, to create activity and interest along the streetfront. Residential uses are allowed subject to a conditional use permit on upper floors of buildings within the Downtown Residential Overlay (see below).

Overlays: The Downtown District contains one Overlay:

- Downtown Residential Overlay. This Overlay applies to select parcels near the San Fernando Mall. It allows for mixed-use development that includes upper floor residential and work-live uses in conjunction with first floor commercial uses, which helps create a transition area between the new infill residential development and the lower scale character of the San Fernando Mall. Residential is only allowed on upper floors, subject to a conditional use permit. In addition, the allowed height and permitted density within the Downtown Residential Overlay is higher than is allowed in the base Downtown District.

3. The Mixed-Use Corridor District:

Location: Covering approximately 25 acres, the Mixed-Use Corridor District is generally bounded by Truman Street to the north, San Fernando Mission Boulevard to the east, Celis and Pico Street to the south, and Hubbard Avenue to the west. It also includes the parcels along Hubbard Avenue between Truman Street and the railroad and one parcel north of Truman Street at San Fernando Mission Boulevard. The Mixed-use Corridor District encompasses the San Fernando - Truman area, identified in Exhibit IV-I, and discussed earlier in this Chapter.

Prior Designation: Prior to the adoption of the Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the Commercial (COM) land use

designation.

Permitted Uses: Mix of use types, ranging from residential and office uses to retail stores and services, at a lower intensity and scale than the Downtown District.

Intent/Character: The Mixed-Use Corridor District is a pedestrian-oriented, mixed-use neighborhood located between the San Fernando Mall and the Sylmar/San Fernando Station. Buildings may be mixed-use or single-use. Commercial ground floors face the sidewalk with inviting shopfront frontages, particularly along San Fernando Road. Residential buildings are set back behind small setbacks with ground floor units accessed through lobbies, stoops, or porches.

4. The Auto Commercial District:

Location: Covering approximately 13 acres, the Auto Commercial District is located east of the San Fernando Mall and is generally bounded by San Fernando Mission Boulevard to the west, Celis Street to the south, the City boundary to the east and the railroad to the north.

Prior Designation: Prior to the adoption of the Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the Commercial (COM) land use designation.

Permitted Uses: Auto-related uses, auto dealerships, office, and retail.

Intent/Character: The Auto Commercial District is a flexible district devoted to automobile-related uses, particularly auto dealerships, while also accommodating office and retail uses. Front setbacks for auto –oriented uses are larger than in the Specific Plan’s more pedestrian-oriented districts and are landscaped. However, new office and retail uses are located close to and accessed directly from the sidewalk.

~~3. The Truman/San Fernando District: This District includes properties located on both sides of an Fernando Road and Truman Street, from the City's westerly boundary line along Hubbard~~

~~Street to the City's easterly boundary line along Fox Street, excluding the properties located within the Downtown District. This District also includes properties located on both sides of Cells Street, between San Fernando Mission Boulevard and South Brand Boulevard; properties located on the south side of Celis Street, between Chatsworth Drive and Fox Street; and properties located on the north side of Pico Street, between Kalisher Street and South Brand Boulevard.~~

~~The Truman/San Fernando District contains four Sub-Districts: Support Commercial, Workplace Commercial, Mixed-Use Transition, and Auto Commercial. Prior to the adoption of the Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the Commercial (COM), Industrial (IND) and Central Business District (CBD) land use designations. The Truman/San Fernando District, together with the Downtown District, encompass the majority of the properties which were previously identified on the Land Use Element Map by the Central Business District (CBD) land use designation, except for those properties identified by the CBD land use designation which are located between Pico Street to the north, Hollister Street to the south, Kalisher Street to the west, and Chatsworth Drive to the east. In addition, this District encompasses all of the San Fernando-Truman Area discussed earlier in this Chapter and identified on Exhibit IV-II.~~

5. The Workplace Flex District

Location: Covering approximately 30 acres, the Workplace Flex District is established along First Street between Hubbard Avenue and Hagar Street and along the north side of Truman Street between Kalisher Street and Meyer Street. The Workplace Flex contains approximately 30 acres of net developable area and the portion south of the railroad right of way encompasses the northern portion of San Fernando Truman area, identified in Exhibit IV-I, and discussed earlier in this Chapter.

Prior Designation: Prior to the adoption of the San Fernando Corridors Specific Plan in 2005, properties within the Workplace Flex District were designated Industrial (IND). The property along the east side of First Street between Harps Street and Alexander Street was designated Specific Plan (SP-2).

Permitted Uses: Light industrial, workshop, and large-scale commercial.

Intent/Character: The Workplace Flex District is established for the purpose of providing a cohesive district that support the commercial and industrial uses of the city while providing appropriate areas for limited live-work uses and limited retail. The Workplace Flex District also provides a framework for creating a more inviting pedestrian, bicycle, and vehicular connection along First Street between the Metrolink Station and Maclay Avenue's "main street," the Civic Center, as well as along Truman Street between the Metrolink Station and Downtown.

Overlays: The Workplace Flex District has one special overlay zone: the Mixed-Use Corridor Overlay.

- The Mixed-Use Corridor Overlay. The Mixed-Use Corridor Overlay applies to the parcels designated Workplace Flex along the north side of Truman Street. It permits the light industrial uses permitted by the underlying Workplace Flex designation, while also allowing the residential, office, retail, and service uses allowed under the Mixed-Use Corridor designation.

6. The General Neighborhood District

Location: Covering approximately 18 acres of developable area, the General Neighborhood District is established along the south side of Second Street between Hubbard Avenue and the alley between Hagar Street and Maclay Avenue.

Prior Designation: Prior to the adoption of the San Fernando Corridors Specific Plan, the properties within this District were identified on the Land Use Element Map by the High Density Residential (HDR) land use designation.

Permitted Uses: Multi-family apartment and condominiums buildings and neighborhood compatible residential uses.

Intent/Character: The General Neighborhood District is established to accommodate multi-family housing near Downtown and the Metrolink Station and to ensure that new housing along the south side of Second Street provides a suitable transition to the low-rise, single-family

residential neighborhoods across the street to the north. New multi-family buildings facing Second Street will have two-story volumes at the front of the lot with massing that is in character with the single-family houses across the street. Higher massing – up to four stories – is allowed at the center and rear of the lot.

~~The Corridors Specific Plan contains a detailed statement of the community's vision for the future of the corridor areas, as well as the revitalization strategy and urban design principles to be used in achieving that vision. The Corridors Specific Plan includes specific land use regulations, development standards and design guidelines applicable to new development in the corridor areas, as well as a program of public improvements for the streets and sidewalks in the corridor areas, so as to improve their function and appearance. The development standards and design guidelines of the Corridors Specific Plan have been tailored to each District and Sub-District in order to require high quality design and architecture that is consistent with the proposed uses allowed within the Corridors Specific Plan Area.~~

~~A general description of the Maclay Avenue, Downtown and Truman/San Fernando Districts, as well as an explanation of the types of land uses permitted within each District, is as follows:~~

~~The Maclay District This District will serve as a neighborhood spine for the community and will become a residential-oriented corridor as a means to promote the creation of new housing opportunities (i.e., condominium and/or town home opportunities), while, at the same time, maintaining the integrity of the existing adjacent residential neighborhoods. Examples of permitted uses within the Maclay District include residential and commercial uses that are compatible with residential development. At the intersections of Maclay Avenue with Gleneaks Boulevard, and with Eighth Street, Neighborhood Service Overlay Areas will encourage the clustered development of locally serving convenience uses that will encourage pedestrian activity.~~

~~The Downtown District This District is intended to facilitate development that will serve as a focal point of activity for the City. This District will promote the concentration of various retail business and civic activities as a means to facilitate pedestrian activity. The uses that are encouraged within the Downtown District, and those that generate the most pedestrian activity,~~

~~include smaller specialized retail shops, restaurants, entertainment venues, galleries, personal services, and business services. Complementary nearby uses such as the Civic Center will also support the planned development of the downtown area.~~

~~The Truman/San Fernando District - At the northwest end of this District, the Support Commercial Sub-District is intended to provide land use policies that support the expansion of the City's industrial and large-scale commercial sectors.~~

~~Along the southwest edge of this District, the Workplace Commercial Sub-District will accommodate growth in the health and professional services sectors. East of that sub-district, the Mixed-Use Transition Sub-District allows for the mixed-use development that includes residential and work-live uses in conjunction with first floor commercial uses, which helps create a transition area between the new infill residential development and the revitalization efforts proposed for the San Fernando Mall. In addition, the Specific Plan land use designation provides for continued expansion of the automobile sales area by establishing the Auto Commercial Sub-District east of the San Fernando Mall.~~

INTERNAL CONSISTENCY

The Land Use Element is a synthesis of the other General Plan elements. It designates open areas identified in the Open Space/Conservation/Parks Element. The selection of land uses is also influenced by technical data contained in the Noise, Safety and Circulation Elements. The range of residential designations reflects the needs identified in the Housing Element.

CHART IV-I

CITY OF SAN FERNANDO CURRENT LAND USE DESIGNATIONS AND IMPLEMENTING ZONES

LAND USE DESIGNATION	INTENT OF LAND USE DESIGNATION	IMPLEMENTING ZONES
RESIDENTIAL		
Low Density Residential	Single family dwellings at a density of 0-6 dwelling units per acre.	R-1
Medium Density Residential	Single family dwellings, duplexes, and multiple-family dwellings at a density of 6-17 dwelling units per acre.	R-2, SP-1, SP-3
High Density Residential	A diversity of multiple-family housing types at a density of 17-43 dwelling units per acre.	R-3, RPO, SP-3
COMMERCIAL		
Central Business District	A major retail area of community-wide significance.	C-2
Neighborhood Shopping	Commercial uses which provide for the immediate shopping needs of nearby residents.	C-1
Highway Related Commercial	Commercial development located along major arterials.	No Specific Zone
Automotive Sales and Services	To consolidate automobile sales and service facilities while protecting them from the encroachment of incompatible uses.	No Specific Zone
Multi-Use	Areas that lend themselves to more than one type of development, facilitating a mixture of commercial, office and residential uses.	SP-2
INDUSTRIAL		
Light Industrial	Provides for the operation of light manufacturing uses and related services.	M-1
Heavy Industrial	To provide for a variety of industrial activities under development standards designed to limit impacts on surrounding land uses.	M-2
Public/Quasi-Public	Provides the necessary infrastructure to maintain a quality living environment. Such facilities include school and the civic center.	No Specific Zone
Neighborhood Park/Landscaping	Defines active and passive recreational facilities.	No Specific Zone

LAND USE DESIGNATION	INTENT OF LAND USE DESIGNATION	IMPLEMENTING ZONE
<u>SAN FERNANDO</u> <u>CORRIDORS</u> <u>SPECIFIC PLAN</u>		

	Within the Maclay Avenue District: For residential development, a minimum density of 12 dwelling units per acre and a maximum density of 36 units per acre. For non-residential development, an FAR of 1.0. For mixed-use development, a maximum FAR of 1.5.	SP-4SP-5
	Within the Downtown District: For all development, a maximum FAR of 3.0. For mixed-use development <u>within the Downtown Residential Overlay</u> , a maximum FAR of 3.5. <u>For residential development within the Downtown Residential Overlay, the minimum density is 24 dwelling units per acre and maximum density is 50 units per acre.</u>	SP-4SP-5
	Within the Mixed-Use Corridor District: <u>For all development, a maximum FAR of 3.0. For all residential development the minimum density is 24 dwelling units per acre and maximum density is 37 units per acre.</u>	SP-5
	Within the Auto Commercial District: <u>For all development, a maximum FAR of 3.0.</u>	SP-5
	Within the Truman/San Fernando Districts For residential development, a minimum density of 24 dwelling units per acre and a maximum density of 45 dwelling units per acre. For non-residential development, a FAR of 2.0. For mixed-use development, a maximum FAR of 2.5.	SP-4
	<u>Within the General Neighborhood District:</u> <u>For all residential development, the maximum density is 43 units per acre.</u>	SP-5
	<u>Within the Workplace Flex District:</u> <u>For all non-residential development, the maximum FAR is 2.0.</u> <u>For all live-work development, the maximum density is 18 units per acre.</u>	SP-5

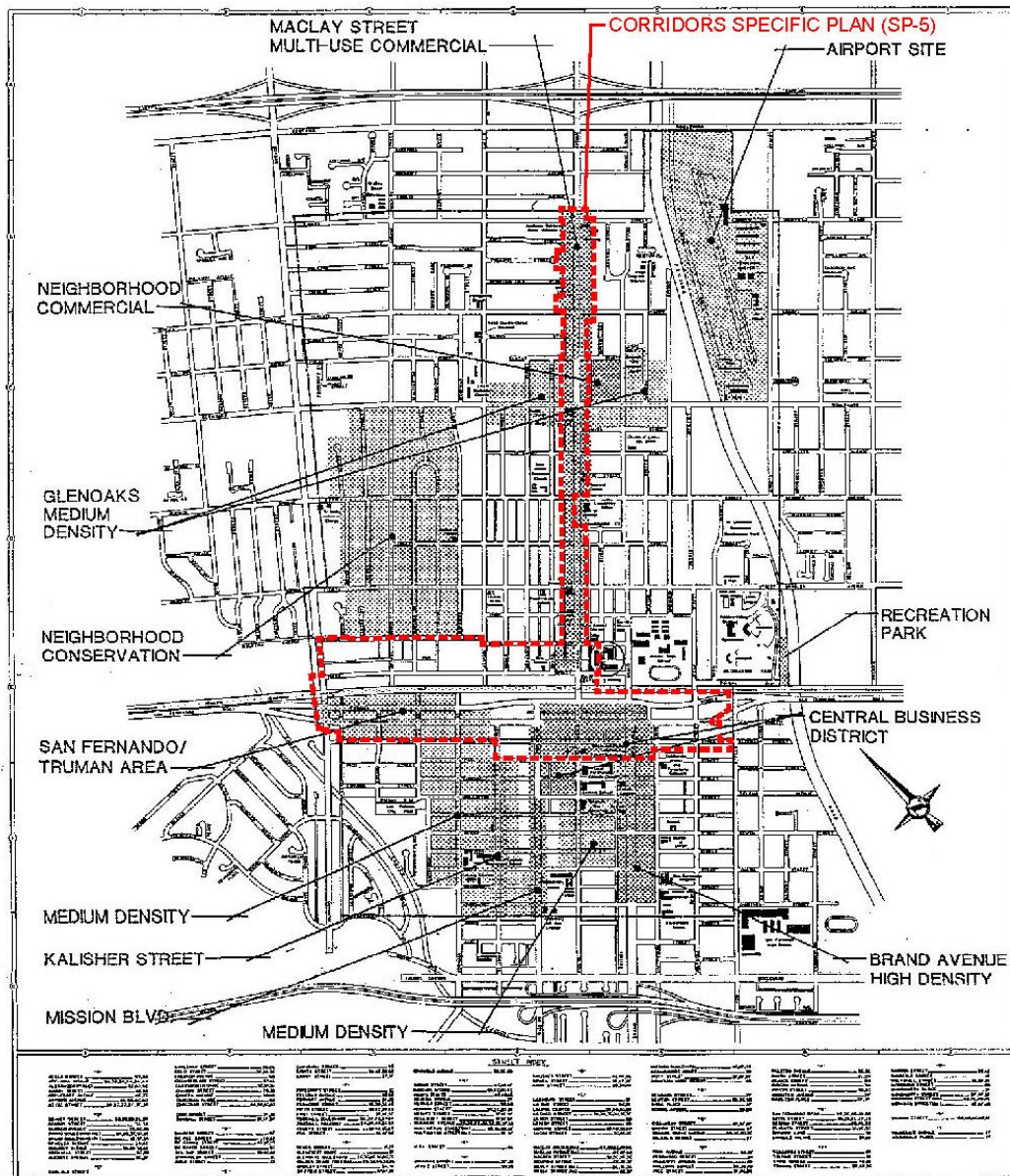
EXHIBIT IV-I

Note: The issues associated with the areas discussed in the Issues and Opportunities Section of this Chapter that overlap with the San Fernando Corridors Specific Plan

(SP-5) have been resolved by the Corridors Specific Plan.

"THE MISSION CITY"
SAN FERNANDO
CALIFORNIA

INCORPORATED AUGUST 31, 1951



Land Use Issue Areas
SAN FERNANDO GENERAL PLAN

EXHIBIT IV-1

EXHIBIT IV-II

PROPOSED GENERAL PLAN DESIGNATIONS FOR SP-5

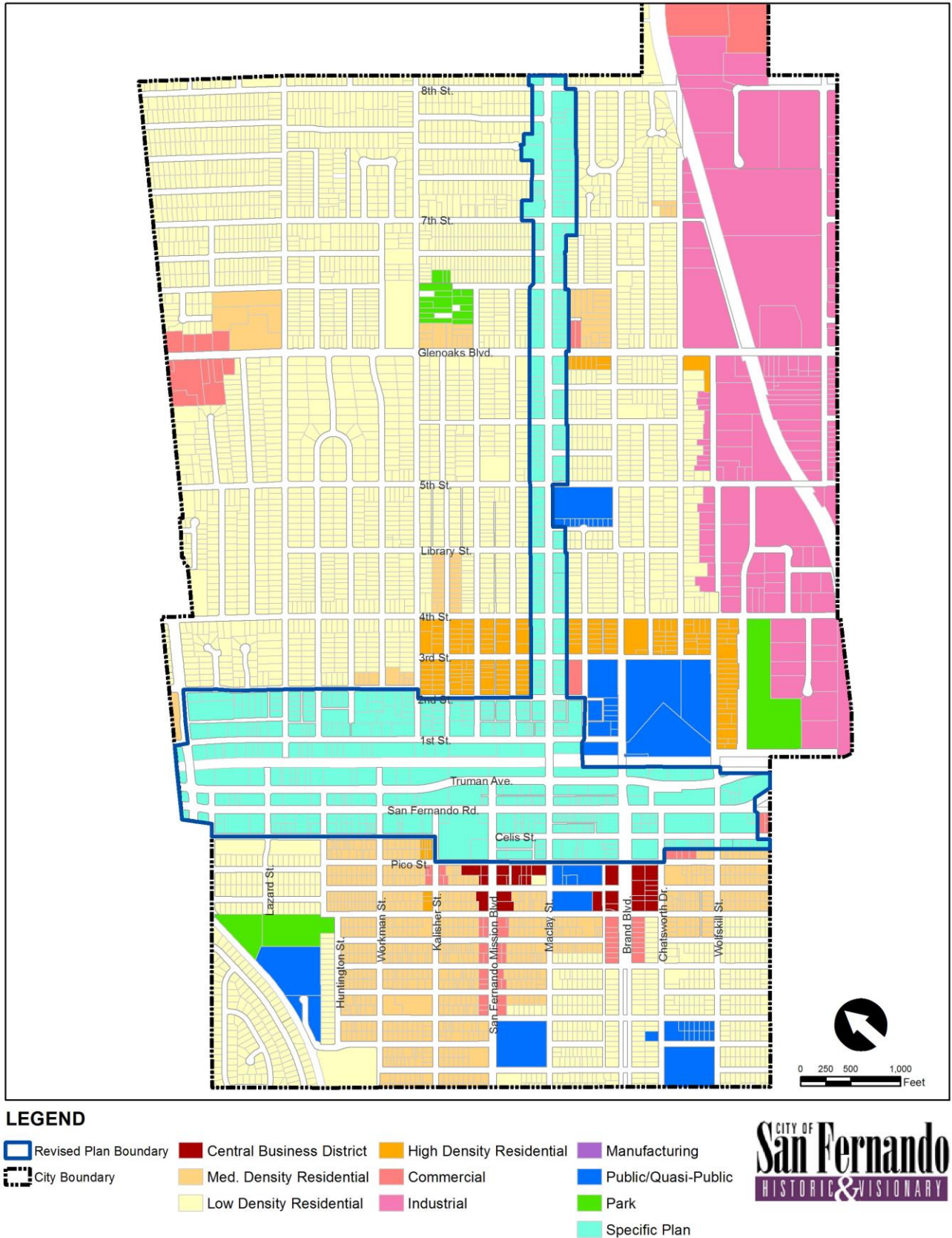
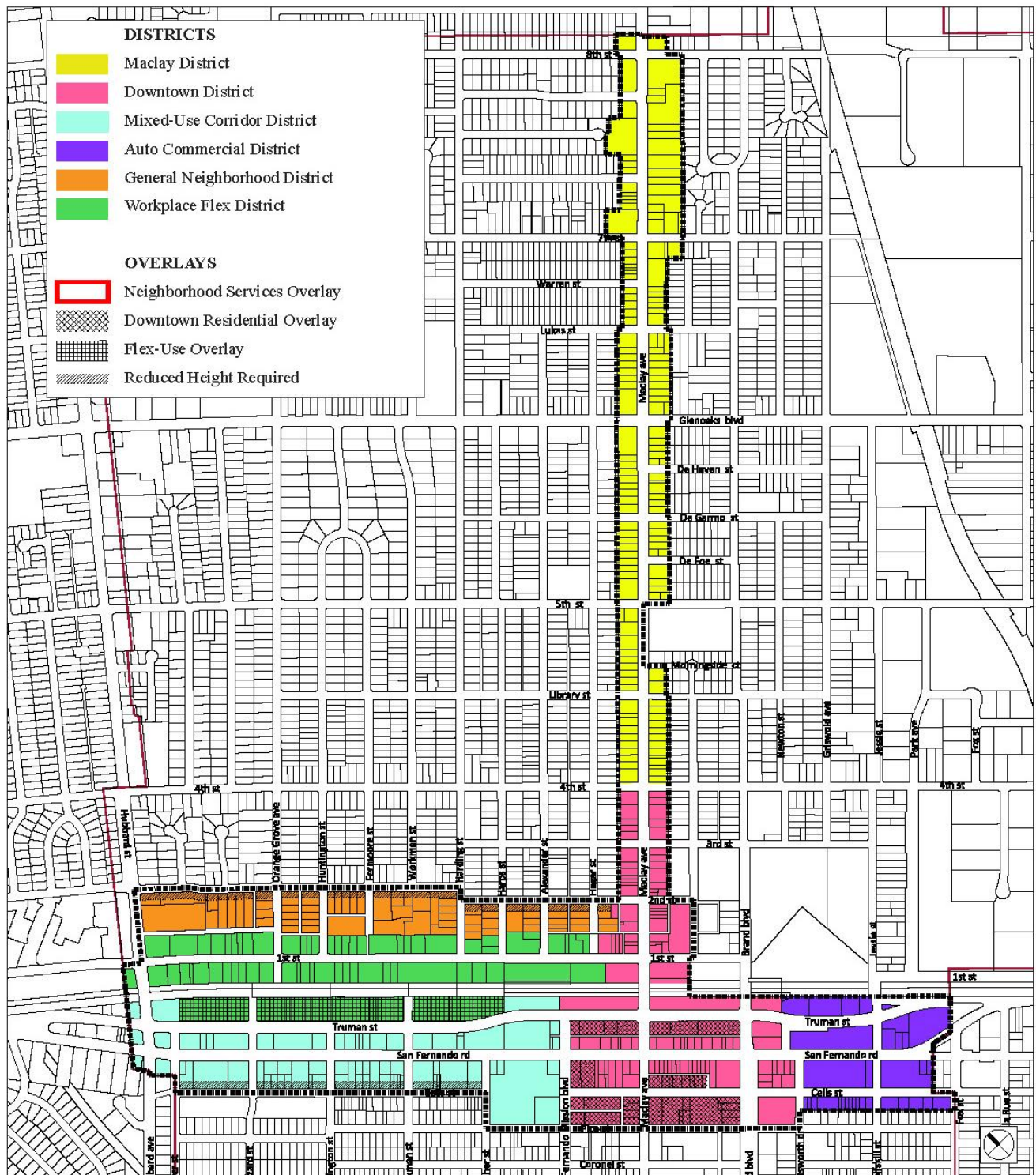


EXHIBIT IV-III

CORRIDORS SPECIFIC PLAN (SP-5) CITY DISTRICTS AND OVERLAYS





CITY OF SAN FERNANDO 2013-2021 HOUSING ELEMENT

DRAFT

January 2014

**City of San Fernando
COMMUNITY DEVELOPMENT DEPARTMENT
117 MACNEIL STREET
SAN FERNANDO, CA 91340**

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I. INTRODUCTION

A. COMMUNITY CONTEXT

The City of San Fernando is located in the northeast section of the San Fernando Valley at the southern foot of the San Gabriel Mountains. This compact community of 2.4 square miles is completely surrounded by the City of Los Angeles, including the nearby communities of Sylmar, Mission Hills and Pacoima.

San Fernando has a rich history, which can still be observed in the built environment. The City's roots go back to 1874 when Charles Maclay laid out a speculative township map for "the first city of the valley," leading to the City's incorporation in 1911. The City's early development is closely related with ranching, the citrus industry, and the nearby San Fernando Mission. Many San Fernando neighborhoods were originally developed in the early twentieth century, with current land uses still reflecting the City's first zoning ordinance adopted in 1929. The City has since developed as a predominately single-family community, with approximately 80 percent of the City's 6,500 housing units consisting of single-family homes, and 55 percent of the City's households owning their homes.

The City has suffered from two major natural disasters, the 1971 Sylmar Earthquake and the 1994 Northridge Earthquake. These earthquakes caused substantial damage to the housing stock, as well as sewers, streets, and other buildings. As a result of these disasters, combined with the age of the City's housing stock, San Fernando had historically placed a large focus on redevelopment activities, including rehabilitation and production of replacement housing. The City has also placed a renewed focus on neighborhood preservation through a combination of code enforcement, rental housing inspection, community involvement, and rehabilitation assistance. However, the State of California's decision to eliminate redevelopment agencies statewide has significantly impacted San Fernando's ability to continue programs that maintain, preserve, and enhance its residential neighborhoods.

While San Fernando has experienced only limited residential development over the past several decades, the City's population has continued to grow and change. More than 90 percent of the City's 23,645 residents are of Latino origin, and 29 percent of the population is under 18 years of age. The Latino population includes both recent immigrants as well as many long time San Fernando families. The provision of adequate affordable housing, including larger rental units and first-time homeownership opportunities for younger growing families, is thus an important issue facing San Fernando.

With adoption of the San Fernando Corridors Specific Plan in January 2005, and its subsequent amendment in 2017, the City has established a well-defined plan for the revitalization of San Fernando's three primary corridors - North Maclay Avenue, Truman Street and San Fernando Road - while providing significant additional opportunities for residential development. The City is evaluating mixed-use residential development and other infill residential opportunities that have the potential to produce hundreds of additional units

on publicly and/or privately owned parcels and underutilized parking lot sites that are located within the City's downtown area and are part of the greater San Fernando Corridors Specific Plan area. Furthermore, the San Fernando Corridors Specific Plan envisions the transition of North Maclay Avenue into a residentially-focused corridor, and provides density bonuses for the integration of residential dwelling units within mixed-use developments within the City's Downtown and Civic Center areas which include the San Fernando Road and Truman Street corridors.

B. ROLE OF THE HOUSING ELEMENT

State law recognizes the vital role local governments play in the availability, adequacy and affordability of housing. Every jurisdiction in California is required to adopt a comprehensive, long-term general plan to guide its physical development; the housing element being one of the seven mandated elements of the General Plan. Housing element law mandates that local governments adequately plan to meet the existing and projected housing needs of all economic segments of the community. The law recognizes that in order for the private market to adequately address housing needs and demand, local governments must adopt land use plans and regulatory systems that provide opportunities for, and do not unduly constrain housing development. As a result, State housing policy rests largely upon the effective implementation of local general plans and in particular, local housing elements. Housing Element statutes also requires the State Department of Housing and Community Development (HCD) to review local housing elements for compliance with State law and to report their findings to the local government.

San Fernando's Housing Element covers an eight-year planning period from October 15, 2013 to October 15, 2021. California's housing element law also requires that each city and county develop local housing programs to meet its "fair share" of existing and future housing needs for all income groups. The Southern California Association of Governments (SCAG) is responsible for developing and assigning these regional needs, or "RHNA", to southern California jurisdictions. The RHNA planning period for this Housing Element cycle is an eight-year plan extending from January 1, 2014, through October 31, 2021.

This Housing Element identifies strategies and programs that focus on: 1) preserving and improving housing and neighborhoods; 2) providing adequate housing sites; 3) assisting in the provision of affordable housing; 4) removing governmental and other constraints to housing investment; and 5) promoting fair and equal housing opportunities.

The City's Housing Element consists of the following major components:

- An analysis of the City's demographic, household and housing characteristics and related housing needs (Section II);
- A review of potential market, governmental, and infrastructure constraints to meeting San Fernando's identified housing needs (Section III);

- An evaluation of residential sites, financial and administrative resources available to address the City's housing goals (Section IV); and,
- The Housing Plan for addressing the City's identified housing needs, constraints and resources; including housing goals, policies and programs (Section V).

C. DATA SOURCES

In preparing the Housing Element, various sources of information are consulted. The U.S. Census Bureau's decennial Census and period American Community Survey (ACS) provides the basis for population and household characteristics. Several data sources are used to supplement U.S. Census Bureau data, including:

- SCAG's 2012 Regional Transportation Plan (RTP) Growth Forecast and 2008 Regional Integrated Forecast provides population, housing and employment projections;
- State of California Employment Development Department labor force participation and unemployment rate estimates;
- Household income data by type of household is derived from the Comprehensive Housing Affordability Strategy (CHAS) prepared by HUD;
- Housing market information updated through internet listings;
- Housing conditions information is obtained from recent field surveys conducted by the San Fernando Community Development Department's Building and Safety Division;
- State of California Department of Developmental Services reports data for residents with developmental disabilities who are assisted at the North Los Angeles County Regional Center;
- Los Angeles Homeless Services Authority (LAHSA) counts of unsheltered homeless individuals;
- SCAG's 2014-2021 Regional Housing Needs Assessment (RHNA) provides information on existing and projected housing needs;
- Lending patterns for home purchase and home improvement loans are provided through the Home Mortgage Disclosure Act (HMDA) database; and,
- Information on San Fernando's land uses and development standards are derived from the City's Zoning Ordinance, and the San Fernando Corridors Specific Plan.

D. PUBLIC PARTICIPATION

Opportunities for residents to provide input on housing issues and recommend strategies are critical to the development of appropriate and effective programs to address San Fernando's housing needs. The City undertook a public outreach program to involve all community stakeholders (e.g., residents, property owners, developers, affordable housing advocates, etc.) early on in the development of its Housing Element, including:

- On Saturday, September 14, 2013, the City conducted the first of two community workshops on the Element update at the San Fernando Regional Pool Facility. The workshop was attended by seven members of the public.
- On Saturday, September 28, 2013, the second community workshop was held at Las Palmas Park and was attended by ten members of the public.
- On Tuesday October 15, 2013, the City conducted a Planning and Preservation Commission study session to review the Draft Housing Element and to receive additional public input.

Notification of the two community workshops was published in the local newspaper and Spanish translation was provided at all the meetings. In addition, notices of public workshops were sent to housing professionals and agencies and organizations serving the City's lower income populations and those with special needs (see Appendix A). A summary of the public comments received is also included in Appendix A.

Upon completion of the Draft Housing Element, the document is placed in public locations throughout the community, including City Hall, the Los Angeles County public library, and the two community centers at Recreation Park and Las Palmas Park. In addition, the Draft Housing Element is placed on the City's website at www.sfcity.org/housingelement. The Draft is also sent to the State Department of Housing and Community Development (HCD) for review and comment. Upon receipt of input from HCD, public hearings will be held before the Planning and Preservation Commission and City Council during adoption of the Housing Element.

E. RELATIONSHIP TO OTHER GENERAL PLAN ELEMENTS

The San Fernando General Plan is comprised of the following eight elements: Land Use; Circulation; Housing; Conservation; Open Space; Safety; Noise and Historic Preservation. As part of the update of the Housing Element, the other elements of the General Plan were reviewed to ensure consistency with the policies set forth in those elements.

As required by State law, internal consistency is required among the various elements of the General Plan, including the Housing Element. The City will maintain consistency between

the Housing Element and the other General Plan elements so that policies introduced in one element are consistent with other elements. Whenever any element of the General Plan is amended in the future, the Housing Element will be reviewed and modified, if necessary, to ensure continued consistency between elements.

State law also requires that the Conservation Element (AB 162, enacted 2009) and Safety Element (SB 1241, enacted 2012) include an analysis and policies regarding flood hazard and management information upon revisions to the Housing Element. Although these revisions are not related to Housing Element law, the City will ensure compliance with this requirement by reviewing its Conservation and Safety Elements.

II. HOUSING NEEDS ASSESSMENT

This section of the Housing Element discusses the characteristics San Fernando's population and housing stock, and consists of the following sections: A) Demographic Profile; B) Household Profile; C) Housing Stock Characteristics; and, D) Regional Housing Needs.

A. DEMOGRAPHIC PROFILE

Demographic changes, such as population growth or changes in age, can affect the type and amount of housing that is needed in a community. This section addresses population, age, race and ethnicity of San Fernando residents.

1. Population Growth and Trends

Table 1 presents population growth trends in San Fernando, and compares this growth to the City of Los Angeles and the entire County of Los Angeles. This table illustrates the high levels of population growth experienced during the 1980s, with growth levels in San Fernando surpassing both the City of Los Angeles and countywide averages. During the 1990s, population growth slowed dramatically throughout the region, reflective of the impacts of the economic recession during the first half of the decade. According to the U.S. Census Bureau, San Fernando added less than 100 people to its population in the last decade with a population of 23,645 in 2010.

Table 1: Regional Population Growth Trends 1980-2010

Jurisdiction	1980	1990	2000	2010	Percent Change		
					1980-1990	1990-2000	2000-2010
San Fernando	17,731	22,580	23,564	23,645	27%	4%	<1%
City of Los Angeles	2,966,850	3,485,398	3,694,742	3,792,621	18%	6%	3%
County of Los Angeles	7,477,503	8,863,164	9,519,338	9,818,605	19%	7%	3%

Source: U.S. Census 1980, 1990, 2000, and 2010.

According to the Southern California Association of Government's (SCAG) 2012 Regional Transportation Plan (RTP) Growth Forecast, the population of San Fernando is expected to grow to 25,500 by 2035, an eight-percent increase from the 2010 population. SCAG's population projections translate to an annual growth rate of approximately 0.2 percent, which matches the annual population growth the City experienced over the most recent two decades (1990-2010).

2. Age Characteristics

Housing need is often affected by the age characteristics of residents in the community. Different age groups have different lifestyles, income levels, and family types that influence housing needs. These housing choices evolve over time, and it is important to examine the changes in the age structure of San Fernando residents in order to identify potential impacts on housing needs.

Table 2 displays the age distribution of the City's population in 2000 and 2010, and compares this with Los Angeles County. Although the population grew by less than 100 people, the City experienced a fairly substantial shift in age characteristics. As displayed below, 29 percent of San Fernando's population was comprised of children under the age of 18 in 2010, which is down from 35 percent in 2000. This decrease in the proportion of young children is consistent with statewide trends of a more stable immigrant population with lower birth rates, and can be expected to continue in the future. Although the proportion of the City's population that is comprised of children declined during the last decade, it is still higher than the countywide proportion (25 percent).

San Fernando's share of college age adults (18-24 years) and young adults (25-44 years) remained fairly constant, and is comparable to the presence of this age group countywide. The City's middle age population (45-64 years) grew significantly during the decade from 15 to 21 percent, though this age group still falls below the countywide average of 24 percent. Finally, while the *proportion* of senior citizens in San Fernando increased by only one percent over the decade, the *number* of seniors increased by over 300. This numeric increase may be attributed the opening of several senior housing complexes in San Fernando since the 2000 Census.

Table 2: Age Distribution 2000-2010

Age Group	2000		2010		
	Persons	Percent	Persons	Percent	L.A. Co. %
Preschool (<5 yrs)	2,255	10%	1,895	8%	7%
School Age (5-17 yrs)	5,830	25%	5,046	21%	18%
College Age (18-24 yrs)	2,706	11%	2,659	11%	11%
Young Adults (25-44 yrs)	7,571	32%	7,132	30%	30%
Middle Age (45-64 yrs)	3,542	15%	4,920	21%	24%
Seniors (65+ years)	1,660	7%	1,993	8%	11%
Total	23,564	100%	23,645	100%	100%
Median Age	27.3 years		30.7 years		34.8 years

Source: U.S. Census 2000 and 2010.

3. Race and Ethnicity

Table 3 displays the racial/ethnic composition of San Fernando's population in 2000 and 2010, and compares this with the countywide distribution. Hispanic residents continue to comprise the vast majority of the City's population, increasing from 89 percent in 2000 to 93 percent in 2010. This increase in Hispanic residents was offset by the decline in White residents from eight to five percent of the population. Asians, African Americans, American Indians and "Other" races each continue to comprise less than one percent of the population.

While San Fernando has many, long time Latino families, it also serves as a place of residence for new Latino immigrants. The 2007-2011 ACS reported that, 36 percent of the City's residents were foreign-born, with 37 percent of the foreign-born population entering the United States after 2000. The 2007-2011 ACS also identifies 43 percent of the City's 16,674 Spanish speaking persons as linguistically isolated, defined as living in a household where no member over 14 years of age speaks English "very well". Recent Latino immigrants and linguistically isolated households may face greater difficulties in gaining meaningful employment and acquiring adequate housing as they adjust to their new surroundings. As a result, household problems such as overcrowding and overpayment may be more likely.

Table 3: Racial and Ethnic Composition 2000-2010

Racial/Ethnic Group	2000		2010		
	Persons	Percent	Persons	Percent	L.A. Co. %
Hispanic/Latino	21,859	89%	21,867	93%	48%
White	1,979	8%	1,259	5%	28%
Asian/Pacific Islander	251	1%	211	<1%	14%
African American	202	1%	146	<1%	8
American Indian	174	1%	66	<1%	<1%
Other Race	76	<1%	96	<1%	2%
Total Population	24,541	100%	23,645	100%	100%

Source: U.S. Census 2000 and 2010.

4. Employment

Evaluation of the types of jobs held by community residents provides insight into potential earning power and the segment of the housing market into which they fall. Information on how a community's employment base is growing and changing can help identify potential housing demand changes in the future.

The State Employment Development Department estimates that as of March 2013, 10,500 San Fernando residents are in the labor force, with 10.2 percent unemployment, compared to a Countywide unemployment rate of 9.9 percent and City of Los Angeles unemployment of 11.0 percent. The 2007-2011 ACS documents the following distribution of resident employment by occupational category:

- Production, transportation, and material moving occupations – 18 percent
- Sales and office occupations – 30 percent
- Service occupations – 18 percent
- Management, business, science, and arts occupations – 20 percent
- Natural resources, construction, and maintenance occupations – 15 percent

The SCAG Regional Integrated Forecast estimates the City's employment base at approximately 15,000 jobs (2008). SCAG projects a limited six-percent increase, or 900 additional jobs by year 2035, in comparison to the 11 percent job growth projected for Los Angeles County during the same period. San Fernando's employment base includes a diverse mix of small businesses, retail stores, manufacturing facilities, and corporate satellite offices.

B. HOUSEHOLD PROFILE

Household type and size, income levels, and the presence of special needs populations all affect the type of housing needed by residents. This section details the various household characteristics affecting housing needs in San Fernando.

1. Household Type

A household is defined as all persons living in a housing unit. Families are a subset of households, and include persons living together related by blood, marriage, or adoption. A single person living alone is also a household. "Other" households are unrelated people residing in the same dwelling unit. Group quarters, such as dormitories or convalescent homes are not considered households.

According to the 2010 Census, 5,967 households reside in San Fernando, with an average household size of 3.94 persons and average family size of 4.18 persons (refer to [Table 4](#)~~Table 4~~). This represents a decrease in household size (4.07) from 2000, but well above the Los Angeles County average household size of 2.98. Although larger households may translate into a greater number of overcrowded households, the proportion of households living in overcrowded conditions declined from 43 percent in 2000 (as reported by the Census) to only 15 percent between 2007 and 2011 (as reported by the ACS).

Families comprise the majority of households in San Fernando (83 percent), including families with children (45 percent), and those without children (46 percent). During the 2000s, families without children and other non-families (unrelated roommates) grew at the fastest rate of any household type. In contrast, the number of families with children decreased by 13 percent and singles remained relatively constant.

Table 4: Household Characteristics 2000-2010

Household Type	2000		2010		Percent Change
	Households	Percent	Households	Percent	
Families	4,834	84%	4,972	83%	+3%
With children	3,048	53%	2,663	45%	-13%
With no children	1,786	31%	2,309	46%	+29%
Singles	717	12%	731	12%	+2%
Other non-families	223	4%	264	4%	+18%
Total Households	5,774	100%	5,967	100%	+3%
Average Household Size	4.07		3.94		-3%
Average Family Size	4.33		4.18		-3%

Source: U.S. Census 2000 and 2010.

2. Household Income

Household income is one of the most important factors affecting housing opportunity and determining a household's ability to balance housing costs with other basic necessities of life.

Income Definitions

The State and Federal government classify household income into several groupings based upon the relationship to the area median income (AMI) in a county, which for San Fernando is the County of Los Angeles. The State of California utilizes the income grouping and names presented in [Table 5](#)~~Table-5~~. However, federal housing programs utilize slightly different income groupings and definitions, with the highest income category generally ending at 80 percent AMI. For purposes of the Housing Element, the State income definitions are used throughout, except where specifically noted.

Table 5: State Income Categories

Income Category	% County Area Median Income (AMI)
Extremely Low	0-30% AMI
Very Low	0-50% AMI
Low	51-80% AMI
Moderate	81-120% AMI
Above Moderate	120%+ AMI

Source: Section 50093 of the California Health and Safety Code

Income Characteristics

Between 2000 and 2007-2011, the area median income (AMI) in San Fernando grew from \$39,900 to \$52,021, an increase of 30 percent. The median income level in San Fernando remained below that of Los Angeles County (\$56,266) and the City has seen an increase in both the number and proportion of lower income (<80 percent AMI) households, and a corresponding decrease in households earning moderate incomes and above since 2000. As illustrated in [Table 6](#)~~Table-6~~, during the 2000s the City experienced increases in extremely low (59 percent), very low (11 percent) and low (29 percent) households, while evidencing decreases in its moderate and above moderate (-21 percent) populations.

Table 6: Household Income Levels 2000-2009

Income Level	2000		2005-2009		Percent Change
	Households	%	Households	%	
Extremely Low (<30% AMI)	690	12%	1,100	18%	59%
Very Low (31-50% AMI)	924	16%	1,025	17%	11%
Low (51-80% AMI)	1,095	19%	1,410	24%	29%
Moderate and Above (>80% AMI)	3,065	53%	2,415	41%	-21%
Total	5,774	100%	5,950	100%	n/a

Source: U.S. Department of Housing and Urban Development (HUD), Comprehensive Housing Affordability Strategy (CHAS), 2000 and 2005-2009.

Income by Household Type and Tenure

[Table 7](#)~~Table-7~~ shows the income level of San Fernando residents by household tenure. A significantly higher percentage of renter-households (69 percent) were lower income (<80 percent AMI) compared to residents who owned their homes (50 percent). The high incidence of lower income renter-households is of particular significance as market rents in San Fernando currently exceed the level of affordability for lower income households. (This issue is further evaluated in the Housing Profile section of the Needs Assessment.) The median income of renter-households between 2007 and 2011 was \$34,361 compared to \$60,244 for homeowners.

Table 7: Income by Owner/Renter Tenure 2005-2009

Income Level	Renters		Owners		Total %
	Households	%	Households	%	
Extremely Low (<30% AMI)	780	27%	320	11%	18%
Very Low (31-50% AMI)	585	20%	440	14%	17%
Low (51-80% AMI)	655	22%	755	25%	24%
Moderate and Above (>80% AMI)	900	31%	1,515	50%	41%
Total Households	2,920	100%	3,030	100%	100%

Source: U.S. Department of Housing and Urban Development (HUD), Comprehensive Housing Affordability Strategy (CHAS), 2005-2009.

While renters were more likely to have lower incomes than owners, there is also significant variation in income levels by household type, as presented in [Table 8](#)~~Table 8~~. Approximately 70 percent of elderly and large households in San Fernando have lower income (<80 percent AMI), with more than one-third having extremely low incomes. About 48 percent of small families have lower incomes.

Table 8: Income Level by Household Type 2005-2009

Income Level	Elderly	Small Family	Large Family	Other
Extremely Low (<30% AMI)	37%	10%	21%	27%
Very Low (31-50% AMI)	22%	15%	18%	18%
Low (51-80% AMI)	11%	23%	31%	24%
Moderate and Above (>80% MFI)	30%	52%	30%	31%
Total Households	695	2,840	1,645	770

Source: U.S. Department of Housing and Urban Development (HUD), Comprehensive Housing Affordability Strategy (CHAS), 2005-2009.

Households in Poverty

The federal government publishes national poverty thresholds that define the minimum income level necessary to obtain the necessities of life. For example, the 2011 U.S. poverty threshold for a family of four was \$23,021. As indicated in [Table 9](#)~~Table 9~~, approximately 16 percent of all San Fernando residents lived in poverty between 2007 and 2011, a decrease of more than 600 persons living below the poverty line since 2000. Nearly one in four children under the age of 18 in San Fernando is estimated to live in poverty. As a group, female-headed households with children are most impacted by poverty, with one-fourth of this group living in poverty.

Table 9: Poverty Status 2000-2011

Groups in Poverty	2000		2007-2011	
	Persons /Families	Percent	Persons /Families	Percent
Individuals	4,450	19%	3,783	16%
Children (under 18)	1,819	23%	1,596	23%
Families	749	15%	542	11%
Female-Headed w/ Children	224	33%	143	26%

Source: U.S. Census 2000; American Community Survey 2007-2011.

3. Special Needs Populations

State law recognizes that certain households have more difficulty in finding decent and affordable housing due to special circumstances including, but not limited to, the following: economic status, age, disability, household size and household type. Special needs populations in San Fernando include large households, the elderly, persons with disabilities, female-headed households, farmworkers, and the homeless. ~~Table 10~~ **Table 10** summarizes the number of households or persons in each of these special needs groups in the City.

Table 10: Special Needs Populations

Special Needs Groups	Persons	Households	Percent*
Large Households	--	1,478	24%
Renter	--	629	(43%)
Owner	--	849	57%
Seniors (65+)	2,146	--	9%
With a Disability	784	--	(37%)
Senior Households	--	1,138	18%
Owner	--	873	(77%)
Renter	--	265	(23%)
Seniors Living Alone	372	--	17%
Owner	230	--	(62%)
Renter	142	--	(38%)
Persons with Disability	2,800	--	12%
Female-Headed Households	--	1,390	22%
with Related Children	--	769	(55%)
Farmworkers**	116	--	1%
Homeless	12	--	<1%

Source: American Community Survey (ACS), 2007-2011; ACS, 2009-2011; and Los Angeles Homeless Services Agency, 2013.

* Numbers in () reflect the % of the special needs group, and not the % of the total City population/households. For example, of the City's large households, 43% are renters and 57% are owners.

** Persons employed in agriculture, forestry, fishing and hunting, and mining industries.

Large Households

Large households consist of five or more persons and are considered a special needs population due to the limited availability of affordable and adequately sized housing. The lack of large units is especially evident among rental units. Large households often live in overcrowded conditions, due to both the lack of large enough units, and insufficient income to afford available units, which often consist of single-family homes of adequate size.

San Fernando had a total of 1,478 large households, and at 24 percent, represents the most significant special needs group in the City. Of these large households, 43 percent are renters and a large majority of these large renter households (70 percent) earned lower incomes between 2007 and 2011. Based on the Comprehensive Housing Affordability Strategy (CHAS) Databook prepared by HUD, 81 percent of San Fernando's large renter-households suffer from one or more housing problems, including housing overpayment, overcrowding and/or substandard housing conditions.

The American Community Survey (ACS) further documents the mismatch between the need for larger rental units and the City's supply of smaller units. The ACS identifies 772 rental units in San Fernando with three or more bedrooms, in general, the appropriate sized unit for a large household of five or more members. The City has approximately 636 large renter households. Although housing options for large renter-households in the City are numerically sufficient to meet the needs of the 1,548 large renter-households documented in the 2007-2011 ACS, lower income large renter-households may have difficulty finding adequately sized and affordable housing in San Fernando. This imbalance between supply of larger units and demand from lower income large households may contribute to 20 percent of the City's renter-households residing in severely overcrowded conditions.

Senior Households

Approximately nine percent of San Fernando residents are over age 65, and about 18 percent of all households are headed by seniors. Most of the City's seniors are homeowners (77 percent), and about 17 percent of the City's elderly live alone. Over 370 senior homeowners live alone in San Fernando, with approximately 37 percent of elderly residents in the City having some type of disability which may limit their mobility.

The elderly have a number of special needs including housing, transportation, health care, and other services. Rising rents are a particular concern due to the fact that most seniors are on fixed incomes. Of San Fernando's approximately 265 senior renter-households, 91 percent are lower income. As shown in ~~Table 21~~ [Table 21](#) (page 30), San Fernando has four senior housing projects providing 112 rental units affordable to a mix of very low, low and moderate income households.

For those seniors who live on their own, many have limited incomes and as a result of their age may not be able to maintain their homes or perform minor repairs. Furthermore, the installation of grab bars and other assistance devices in the home may be needed. The City

operates a housing rehabilitation program that assists low and moderate income homeowners in making needed repairs. However, with the dissolution of redevelopment in California, the City, like many other cities in the State, has lost a significant funding resource for supportive affordable housing activities. As a result, this program is operating at a limited capacity.

The San Fernando Recreation and Community Services Division offers a number of programs for seniors including recreational and social activities, a nutrition program, supportive services including information and referral services, and blood pressure and diabetes screening. Senior centers are located at San Fernando Recreation Park and Las Palmas Park.

Female-Headed Households

Single-parent households typically have a special need for such services as childcare and health care, among others. Female-headed households with children in particular tend to have lower incomes, which limits their housing options and access to supportive services. The 2007-2011 ACS reports 1,390 female-headed households in San Fernando; 769 of these households (55 percent) had children. Of those households with children, over one-quarter lived in poverty. These households need assistance with housing subsidies, as well as accessible and affordable day care.

Persons with Disabilities

According to the Census, a disability is defined as a long lasting condition that impairs an individual's mobility, ability to work, or ability to care for oneself. Persons with disabilities include those with physical, mental, or emotional disabilities. Disabled persons have special housing needs because of their fixed income, shortage of affordable and accessible housing, and higher health costs associated with their disability.

According to the 2009-2011 ACS, approximately 12 percent of San Fernando residents (2,800 persons) have one or more disabilities. Approximately 507 residents had a hearing difficulty, 831 had a vision difficulty, 1,089 have cognitive difficulty, 1,644 have an ambulatory difficulty, 779 have a self-care difficulty, and 973 have difficulty with independent living. Of the City's senior population, approximately 37 percent have one or more of these types of disabilities.

The living arrangements for persons with disabilities depend on the severity of the disability. Many persons live at home in an independent environment with the help of other family members. To maintain independent living, disabled persons may require assistance. This can include special housing design features for the physically disabled, income support for those who are unable to work, and in-home supportive services for persons with medical conditions.

Developmental Disabilities: A recent change in State law requires that the Housing Element discuss the housing needs of persons with developmental disabilities. As defined by Section 4512 of the Welfare and Institutions Code, “developmental disability” means “a disability that originates before an individual attains age 18 years, continues, or can be expected to continue, indefinitely, and constitutes a substantial disability for that individual. As defined by the California Director of Developmental Services, in consultation with the Superintendent of Public Instruction, this term shall include mental retardation, cerebral palsy, epilepsy, and autism. This term shall also include disabling conditions found to be closely related to mental retardation or to require treatment similar to that required for individuals with mental retardation, but shall not include other handicapping conditions that are solely physical in nature.” This definition also reflects the individual’s need for a combination and sequence of special, interdisciplinary, or generic services, individualized supports, or other forms of assistance that are of lifelong or extended duration and are individually planned and coordinated.

The Census does not record developmental disabilities. According to the U.S. Administration on Developmental Disabilities, an accepted estimate of the percentage of the population that can be defined as developmentally disabled is 1.5 percent. This generally equates to 355 persons in the City of San Fernando with developmental disabilities, based on the 2010 Census population. However, according to the State’s Department of Developmental Services, as of November 2012, between 273 and 388 residents with developmental disabilities living within the 91340 zip code (which approximates the San Fernando city limits) were being assisted at the North Los Angeles County Regional Center. Most of these individuals are under the age of 18 and reside in a private home with their parent or guardian (refer [Table 11](#) ~~Table 11~~).

Table 11: Developmentally Disabled (November 2012)

Housing Arrangement	Age Group	Number
Family/Foster Home Agency	Under 18	<25
Home of Parent/Family/Guardian	Under 18	176
Family/Foster Home Agency	18 to 21	<25
Home of Parent/Family/Guardian	18 to 21	<25
ILS/SLS	22 to 64	<25
Home of Parent/Family/Guardian	22 to 64	92
Home of Parent/Family/Guardian	65 or Older	<25

ILS/SLS = Independent Living Skills/Supportive Living Services

Source: State Department of Developmental Services, November 2012.

Many developmentally disabled persons can live and work independently within a conventional housing environment. More severely disabled individuals require a group living environment where supervision is provided. The most severely affected individuals may require an institutional environment where medical attention and physical therapy are provided. Because developmental disabilities exist before adulthood, the first issue in supportive housing for the developmentally disabled is the transition from the person’s living situation as a child to an appropriate level of independence as an adult.

Accessibility Accommodations: Both the Federal Fair Housing Act and the California Fair Employment and Housing Act impose an affirmative duty on local governments to make reasonable accommodations (i.e. modifications or exceptions) in their zoning and other land use regulations when such accommodations may be necessary to afford disabled persons an equal opportunity to use and enjoy a dwelling. For example, it may be a reasonable accommodation to allow covered ramps in the setbacks of properties that have already been developed to accommodate residents with mobility impairments. In October 2013, the City amended the Zoning Code to establish a ministerial procedure to provide individuals with disabilities reasonable accommodation in the application of the City's rules, policies, practices, and procedures, as necessary, in order to ensure equal access to housing and facilitate the development of housing for individuals with disabilities, pursuant to Federal and State fair housing laws. The City does not require special building codes or burdensome project review to construct, improve, or convert housing for persons with disabilities. Residential care facilities with six or fewer persons are permitted by right in all residential zoning districts.

Farmworkers

Farmworkers are traditionally defined as persons whose primary incomes are earned through seasonal agricultural work. Farmworkers have special housing needs because they earn lower incomes than many other workers and move throughout the season from one harvest to the next.

According to the 2007-2011 ACS, 116 San Fernando residents were employed in agriculture, forestry, fishing and hunting, or mining industries, representing approximately one percent of the City's labor force. Therefore, given the extremely limited presence of farmworkers in the community and the highly urbanized character of the San Fernando Valley, the City has no specialized housing programs targeted to this group beyond overall programs for housing affordability.

Homeless

In January 2013, the Los Angeles Homeless Services Authority (LAHSA) conducted a count of homeless housed overnight in shelters and institutions throughout Los Angeles County, as well as a three-day unsheltered homeless street count. Based on LAHSA's methodology, they estimate the Los Angeles homeless population at approximately 58,423 persons. Of that total, 79 percent are estimated to be single individuals, while 20 percent are in families and one percent is unaccompanied youth under age 18.

San Fernando is located in LAHSA's Service Planning Area (SPA) 2, which encompasses the entire San Fernando Valley. Approximately 5,258 homeless persons, or nine percent of the County's total homeless population, fall within the San Fernando Valley. LAHSA is responsible for developing a continuum of care plan for all of Los Angeles County, and establishing priority needs, services, shelter and housing by Service Planning Area.

The City of San Fernando's homeless population is estimated at around 12 persons, according to the 2013 LAHSA point-in-time count. According to City law enforcement personnel, these are the "chronic" homeless that live in San Fernando in makeshift dwellings under bridges, railroad tresses, and by the Pacoima Wash. In general, this group consists of single men, age 20 to 55, with alcohol and/or drug dependencies. Homeless families with children are also occasionally seen in the streets in San Fernando, some of these families may sometimes come to the police station seeking aid. The police do what they can to place these families in shelters.

Catholic Charities' Loaves and Fishes is located in San Fernando and provides case management, food, clothing, shelter referrals, and a variety of other services for the homeless and persons at-risk of homelessness. The City is also working with the Los Angeles County Department of Public Health and the Los Angeles Homeless Services Authority to provide additional services to homeless individuals in the City.

C. HOUSING STOCK CHARACTERISTICS

This section identifies the characteristics of San Fernando's physical housing stock. This includes an analysis of housing growth trends, housing conditions, lead-based paint hazards, housing prices and rents, and housing affordability.

1. Housing Growth

Table 12~~Table 12~~ displays housing production in the City, compared to the City of Los Angeles and the entire County region. Between 1980 and 1990, San Fernando's housing stock grew by five percent, in contrast to the City and County of Los Angeles, which evidenced a growth rate of approximately double that of San Fernando. During the 1990s, housing growth dropped dramatically throughout the region, with San Fernando adding only 138 new units (2% growth) the entire decade.

According to the 2010 Census, San Fernando has a housing stock of 6,506 units, representing an increase of 574 units (or 10 percent) since 2000. The City's dwelling unit growth rate out-paced that of the City and County of Los Angeles during the same period. Most of this development occurred within the City's R-2 (Multiple Family Dwelling) and R-3 (Multiple Family) zones and the San Fernando Corridors Specific Plan areas.

Table 12: Regional Housing Growth Trends 1980-2010

Jurisdiction	1980	1990	2000	2010	Percent Change		
					1980-1990	1990-2000	2000-2010
San Fernando	5,522	5,794	5,932	6,506	5%	2%	10%
L.A. City	1,189,475	1,299,963	1,337,654	1,412,641	9%	3%	6%
L.A. County	2,853,653	3,163,343	3,270,909	3,437,584	11%	3%	5%

Source: U.S. Census 1980, 1990, 2000, and 2010.

2. Housing Type and Tenure

Table 13 presents the mix of housing types in San Fernando. Unlike many urbanized communities, single-family homes have increased in relative proportion and number over the past two decades, from 76 percent (4,365 units) in 1990 to 80 percent (5,182 units) between 2007 and 2011. This growing number of single-family attached and detached homes is consistent with the City's high rate of homeownership.

Table 13: Housing Type 1990-2011

Unit Type	1990		2000		2007-2011	
	Units	Percent	Units	Percent	Units	Percent
Single-Family (SF) Detached	3,868	67%	3,993	67%	4,707	72%
SF Attached	497	9%	635	11%	475	8%
Total Single-Family	4,365	76%	4,628	78%	5,182	80%
2 to 4 Units	496	9%	479	8%	472	7%
5 or more units	750	13%	763	12%	734	11%
Total Multi-Family	1,246	21%	1,242	20%	1,206	19%
Mobile Homes & Other	183	3%	73	2%	118	2%
Total Housing Units	5,794	100%	5,943	100%	6,506	100%
Vacancy Rate	2.8%	--	2.7%	--	4.6%	--

Source: U.S. Census 1990 and 2000; and American Community Survey (ACS), 2007-2011.

Housing tenure refers to whether a housing unit is owned, rented or is vacant. Tenure is an important indicator of the housing climate of a community, reflecting the relative cost of housing opportunities, and the ability of residents to afford housing. Tenure also influences residential mobility, with owner units generally evidencing lower turnover rates than rental housing. According to the 2010 Census, 55 percent of San Fernando's households were homeowners, fairly consistent with 2000 levels (see **Table 14**). In Los Angeles County, only 48 percent of households are owner-occupied.

Table 14: Housing Tenure

Occupied Housing Units	2000		2010	
	Households	Percent	Households	Percent
Renter	2,659	46%	2,715	45%
Owner	3,115	54%	3,252	55%
Total	5,774	100%	5,967	100%

Source: U.S. Census, 2000 and 2010.

Vacancy Rate

A vacancy rate measures the overall housing availability in a community and is often a good indicator of how efficiently for-sale and rental housing units are meeting the current demand for housing. A vacancy rate of five percent for rental housing and two percent for ownership housing is generally considered healthy and suggests that there is a balance between the demand and supply of housing. A lower vacancy rate may indicate that households are having difficulty in finding housing that is affordable, leading to overcrowding or households having to pay more than they can afford. A low vacancy rate or a particularly ‘tight’ housing market may also lead to high competition for units, raising rental and housing prices substantially.

As measured by the 2007-2011 ACS, the citywide residential vacancy rate in San Fernando was 4.6 percent for all housing units compared to the 2.7 percent vacancy rate in 2000. In terms of tenure, the 2007-2011 ACS estimated that the vacancy rate was 3.8 percent for rental units, well below the five percent considered healthy for rental housing, and less than 1.6 percent for ownership housing. These low vacancy rates indicate that a high ‘pent-up’ demand for housing exists, putting upward pressure on housing prices, and making it increasingly difficult to find available housing in the community.

3. Housing Age and Condition

The age of a community’s housing stock can provide an indicator of overall housing conditions. Typically housing over 30 years in age is likely to have rehabilitation needs that may include new plumbing, roof repairs, foundation work and other repairs. [Table 15](#) displays the age of San Fernando’s occupied housing stock by owner/renter tenure as of the 2007-2011 ACS. As a mature community, the majority of San Fernando’s housing stock consists of units older than 30 years of age. Among owner-occupied housing, over 80 percent of units were constructed prior to 1970, and is reflective of the community’s numerous older single-family neighborhoods. While a lesser proportion of renter housing is greater than 30 years in age (76 percent), this housing is typically of lesser quality construction and suffers more wear-and-tear from tenants than owner-occupied housing. The advanced age of the majority of San Fernando’s housing stock indicates the significant need for continued code enforcement, property maintenance and housing rehabilitation programs to stem housing deterioration.

Table 15: Age of Housing Stock

Year Structure Built	Renter Occupied Housing	Percent Renter	Owner Occupied Housing	Percent Owner	Total Percent
2005 or later	147	5%	33	1%	3%
2000-2005	98	3%	31	1%	2%
1990-1999	46	2%	45	1%	1%
1980-1989	182	6%	231	7%	7%
1970-1979	217	8%	161	5%	6%
1960-1969	313	11%	322	10%	10%
1950-1959	899	31%	1,207	36%	34%
1940-1949	505	18%	668	20%	19%
1939 or earlier	456	16%	647	19%	18%
Total	2,863	100%	3,345	100%	100%

Source: American Community Survey (ACS), 2007-2011.

In mid-2013, the City's Community Development Department staff conducted a citywide windshield survey of housing conditions. Building conditions on each of the City's 4,100+ residential parcels were rated as excellent; good; fair; poor; and vacant. As shown in [Table 16](#), approximately one-quarter of residential structures in San Fernando exhibited signs of deferred maintenance, with an additional eight percent identified as in poor condition and in need of substantial rehabilitation. The survey data supports the strong need for continued housing rehabilitation assistance, in conjunction with the City's code enforcement and Community Action Plan for Neighborhood Protection and Preservation (CAPP) programs, described below.

Table 16: Housing Conditions Survey

Ranking	Criteria	Parcel Count	Percent
Good	Housing is new or in good overall condition. No visible repairs needed to structure. No to very low level of trash and debris on property.	2,664	65%
Fair	Housing is in visible need of light upkeep and repairs. Structure may need to be painted, with light exterior repairs needed to restore property to good overall condition (i.e., window replacement, reroof, fix minor cracks in stucco or minor damage to siding). Light level of trash and debris on property.	1,091	27%
Poor	Housing in need of extensive rehabilitation. Structure of dwelling in poor living condition, potentially uninhabitable. Heavily deteriorated exterior with large cracks in stucco or rotted out siding, broken or nonfunctional windows, and structural alterations required to restore property to good overall condition. Presence of high level of trash, debris, and vandalism on property.	333	8%
Vacant	Vacant lot.	19	<1%
Total		4,107	100%

In order to address housing conditions, the City operates a code enforcement program, as well as an apartment inspection program and an inspection upon resale program. Owners of properties in violation of codes are encouraged to participate in City-sponsored rehabilitation programs.

In mid-2006, the City initiated the Community Action Plan for Neighborhood Protection and Preservation (CAPP). CAPP is a comprehensive, multi-departmental, multi-agency approach to identify and abate repeated illegal activity and/or nuisance behaviors and substandard conditions at individual problem properties. A key component of CAPP involves the designation of neighborhood focus areas and outreach to residents adversely impacted by nuisance properties within these areas. ~~Figure 1~~ ~~Figure 1~~ provides a map of the designated neighborhood focus areas.

Implementation of CAPP involves the following three initiatives:

1. Comprehensive Problem Assessment.

The first step in implementing CAPP involves analysis of the following characteristics of properties in the focus area:

- Criminal or illegal activity
- Dilapidated structures associated with property maintenance issues
- Illegally constructed residential units or converted garages
- Animal control violations
- Absentee property owners of rental property
- Lack of proper business licenses
- Invalid water accounts
- Infrastructure deficiencies, improvements, repairs

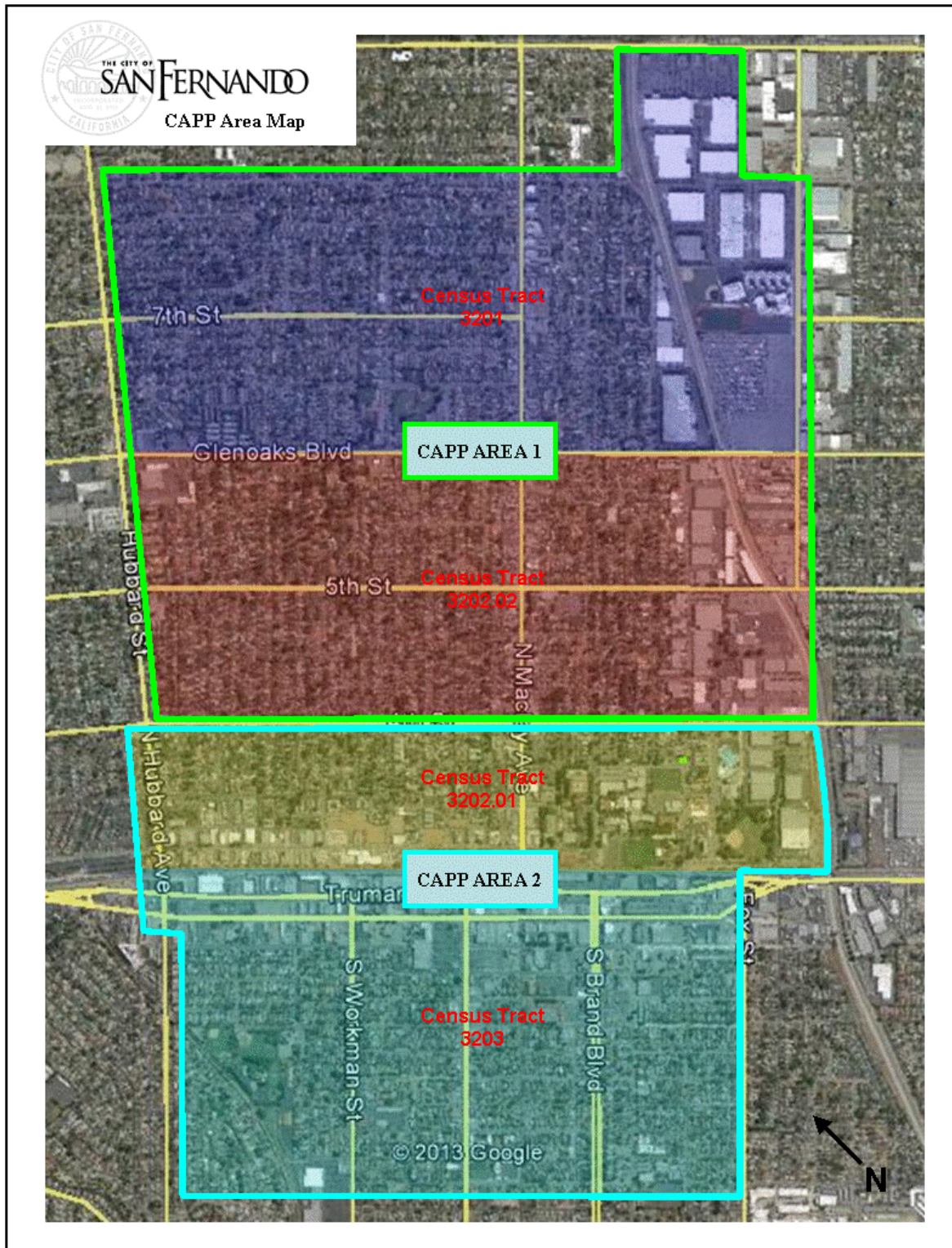
2. In-field Intervention to Permanently Abate Nuisances at Problem Properties.

This step involves team inspections of problem properties and follow-up through the City Prosecutor

3. Neighborhood-wide Improvement Measures Throughout Each Focus Area.

This final step involves:

- Mailing of notices throughout focus area to correct code violations
- Public improvements
- Graffiti removal and prevention

Figure 1: CAPP Focus Areas

4. Housing Costs and Affordability

The cost of housing is directly related to the extent of housing problems in a community. If housing costs are relatively high in comparison to household income, there will be a higher prevalence of overpayment and overcrowding. This section summarizes the cost and affordability of the housing stock to San Fernando residents.

Rental Housing Market

With renters comprising approximately 45 percent of the City's households, it is important to understand the rental market in San Fernando. Rental information for San Fernando was obtained from more than 100 listings posted on online classifieds website Craigslist (www.craigslist.org) between May 11, 2013 and May 19, 2013. Due to the limited number of rental vacancies within the City of San Fernando, the rent survey encompassed a broader market area, including the communities of Pacoima, Sylmar, Mission Hills and North Hills. Rather than formal newspaper or internet advertising, many of the smaller rental properties in San Fernando advertise through sign postings on the property, through word of mouth or other informal means.

As illustrated in ~~Table 17~~**Table 17**, median rents range from \$800 for a studio, \$995 for a one-bedroom, \$1,298 for a two-bedroom, and \$2,200 for a three-bedroom unit. Of 113 units advertised during the survey period, three were studios, 25 were one-bedroom units, 40 were two-bedroom units, and 45 units had three or more bedrooms. Historically, the shortage of all types of rental housing in the community, particularly at the lower end of the market, has resulted in a large number of illegal garage conversions being used as rentals.

Table 17: Apartment Rents in San Fernando and Nearby Communities 2013

Unit Size	Number of Listings	Rental Range	Median Rent
Studio	3	\$800-\$825	\$800
1 Bedroom	25	\$825-\$1,400	\$995
2 Bedroom	40	\$926-\$2,050	\$1,298
3+ Bedroom	45	\$1,595-\$2,900	\$2,200

Source: www.craigslist.org, May 11, 2013 to May 19, 2013.

Homeownership Market

Southern California, like most of the country, has experienced a significant decline in home sales prices during the past few years. With an overall median sales price of \$357,000 recorded in April 2013 by DQNews.com, Southern California home prices are 29 percent below their peak of \$505,000 in the summer of 2007, but up 43 percent from the \$250,000 median sales price recorded in January 2009. The drop in the median sales price from 2007 to 2009 was overstated by the large number of discounted home foreclosures, which comprised a significant portion of Southern California sales, particularly in the lower cost

inland areas. In contrast, homes in the upper half of the market were not selling well due in part to the difficulty in obtaining financing for jumbo mortgages, and were thus under-represented in the median sales price statistics. The modest recovery in sales prices since 2009 suggests a shrinking foreclosure inventory and general stabilization of credit markets. According to RealtyTrac (www.realtytrac.com), 107 homes in the City were in the foreclosure process as of May 2013. This represents less than two percent of the City's housing stock and is less than one-third of the 347 San Fernando homes in the foreclosure process on April 2009.

As shown in ~~Table 18~~**Table 18**, the median home price in San Fernando increased by 36 percent, from \$232,500 in March 2012 to \$315,000 in March 2013. During this same time period, changes to median home prices in neighboring jurisdictions, San Fernando Valley, and Los Angeles County were lower.

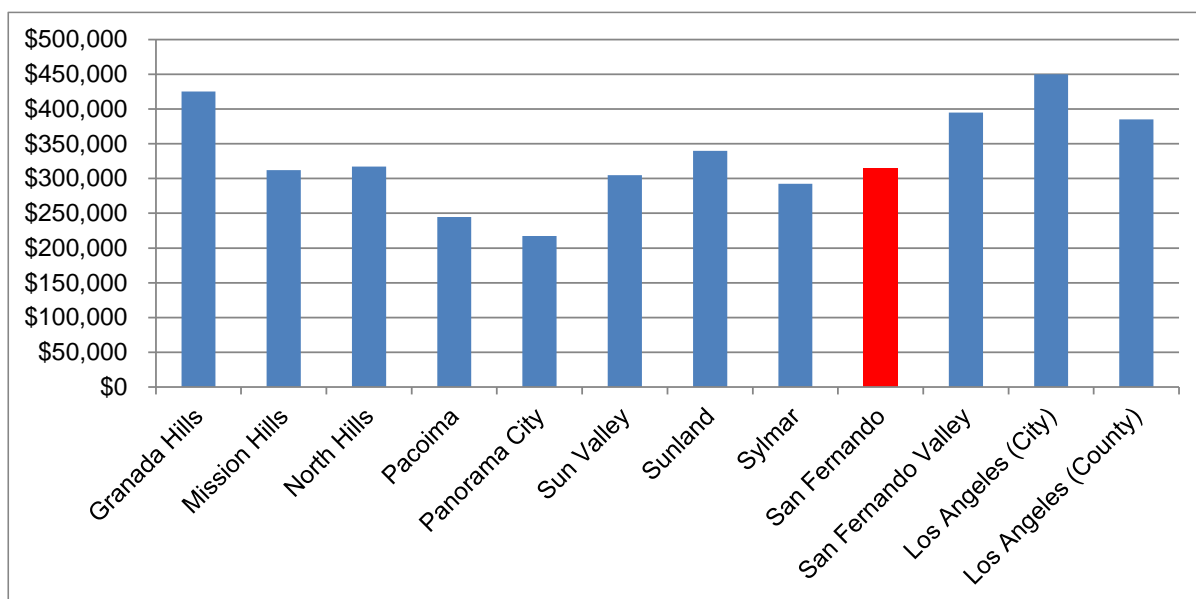
Table 18: Median Home Sales Prices 2012-2013

Geography	March 2013		March 2012 Median Price	Change
	# Sold	Median Price		
Granada Hills	57	\$425,000	\$377,500	13%
Mission Hills	14	\$312,000	\$269,500	16%
North Hills	52	\$317,000	\$276,000	15%
Pacoima	39	\$244,500	\$230,000	6%
Panorama City	30	\$217,500	\$211,500	3%
Sun Valley	25	\$305,000	\$275,000	11%
Sunland	24	\$340,000	\$328,500	4%
Sylmar	78	\$292,500	\$240,000	22%
San Fernando	19	\$315,000	\$232,500	36%
San Fernando Valley	1,093	\$395,000	\$320,000	23%
City of Los Angeles	1,153	\$450,000	\$335,000	34%
Los Angeles County	7,172	\$385,000	\$310,000	24%

Source: DQNews.com, 2013.

~~Figure 2~~**Figure 2** compares median home prices in San Fernando with home prices in neighboring jurisdictions in March 2013. The median price for a home in the City was similar to those in neighboring jurisdictions but somewhat below median prices in the San Fernando Valley, the City of Los Angeles, and Los Angeles County.

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Figure 2: Median Home Sales Prices March 2013

Source: DQnews, 2013.

While ~~Table 18~~ ~~Table 18~~ provides an overview of the subregional housing sales market during March 2013, the ~~Table 19~~ ~~Table 19~~ provides detailed information on current single-family home and condominium listings within the San Fernando city limits in May 2013. A total of 44 single-family homes and two condominiums were listed for-sale. Two-and three-bedroom units were the most prevalent home sales listings, characteristic of San Fernando's older single-family housing stock of smaller sized units. Sales prices for two, three and four bedroom units generally ranged from \$159,900 to \$425,000. The overall median single-family home price for the 46 listings was \$269,500, significantly lower than the \$315,000 median sale price recorded in March 2013.

Table 19: Home and Condominium Asking Prices (May 2013)

# Bdrms	Units Listed	Price Range	Median Price	Average Price
1-2	16	\$199,000-\$284,900	\$249,900	\$247,516
3	19	\$159,900-\$375,000	\$279,900	\$280,650
4	7	\$259,500-\$425,000	\$325,000	\$331,629
5+	4	\$220,000-\$499,950	\$285,450	\$322,713
Total	46	\$159,900-\$499,950	\$269,950	\$280,540

Source: Realtor.com, May 2013.

Housing Affordability

The affordability of housing in San Fernando can be assessed by comparing market rents and sales prices with the amount that households of different income levels can afford to pay for housing. Compared together, this information can reveal who can afford what size and type of housing as well as indicate the type of households that would most likely experience overcrowding or overpayment.

Housing affordability is an important indicator of quality of life in San Fernando. If residents pay too much for housing they will not have sufficient income for other necessities, such as health care. Households that spend a substantial portion of their income on housing may also be at risk of becoming homeless in the event of unexpected circumstances such as illness or loss of employment. State law requires that the City facilitate housing opportunities that are affordable to all economic segments of the community through adoption of provisions in the City Code that provide the mechanism to facilitate such housing types.

The Federal Department of Housing and Urban Development (HUD) conducts annual household income surveys nationwide to determine a household's eligibility for Federal housing assistance. Based on this survey, the California Department of Housing and Community Development (HCD) developed income limits that can be used to determine the maximum price that could be affordable to households in the upper range of their respective income category (see [Table 20](#)~~Table 20~~). Households in the lower end of each category can afford less by comparison than those at the upper end.

Table 20: Affordable Housing Costs by Household Size and Tenure

Household	Annual Income	Affordable Costs (All Costs)		Estimated Utility Allowance, Taxes & Insurance		Affordable Rent	Affordable Home Price
		Rental	Ownership	Renters	Owners		
Extremely Low Income (0-30% AMI)							
1-Person	\$17,950	\$449	\$449	\$89	\$153	\$360	\$68,889
2-Person	\$20,500	\$513	\$513	\$102	\$180	\$411	\$77,501
3-Person	\$23,050	\$576	\$576	\$115	\$206	\$461	\$86,112
4-Person	\$25,600	\$640	\$640	\$134	\$238	\$506	\$93,559
5-Person	\$27,650	\$691	\$691	\$153	\$271	\$538	\$97,749
Very Low Income (31-50% AMI)							
1-Person	\$29,900	\$748	\$748	\$89	\$213	\$659	\$124,513
2-Person	\$34,200	\$855	\$855	\$102	\$248	\$753	\$141,270
3-Person	\$38,450	\$961	\$961	\$115	\$283	\$846	\$157,794
4-Person	\$42,700	\$1,068	\$1,068	\$134	\$324	\$934	\$173,155
5-Person	\$46,150	\$1,154	\$1,154	\$153	\$364	\$1,001	\$183,860
Low Income (51-80% AMI)							
1-Person	\$47,850	\$1,196	\$1,196	\$89	\$302	\$1,107	\$208,065
2-Person	\$54,650	\$1,366	\$1,366	\$102	\$350	\$1,264	\$236,458
3-Person	\$61,500	\$1,538	\$1,538	\$115	\$399	\$1,423	\$265,085
4-Person	\$68,300	\$1,708	\$1,708	\$134	\$452	\$1,574	\$292,315
5-Person	\$73,800	\$1,845	\$1,845	\$153	\$502	\$1,692	\$312,563
Median Income (80-100% AMI)							
1-Person	\$45,350	\$1,134	\$1,323	\$89	\$328	\$1,045	\$231,610
2-Person	\$51,850	\$1,296	\$1,512	\$102	\$379	\$1,194	\$263,650
3-Person	\$58,300	\$1,458	\$1,700	\$115	\$431	\$1,343	\$295,418
4-Person	\$64,800	\$1,620	\$1,890	\$134	\$488	\$1,486	\$326,294
5-Person	\$70,000	\$1,750	\$2,042	\$153	\$541	\$1,597	\$349,180
Moderate Income (100-120% AMI)							
1-Person	\$54,450	\$1,361	\$1,588	\$89	\$381	\$1,272	\$281,027
2-Person	\$62,200	\$1,555	\$1,814	\$102	\$440	\$1,453	\$319,855
3-Person	\$70,000	\$1,750	\$2,042	\$115	\$499	\$1,635	\$358,955
4-Person	\$77,750	\$1,944	\$2,268	\$134	\$564	\$1,810	\$396,619
5-Person	\$83,950	\$2,099	\$2,449	\$153	\$623	\$1,946	\$424,935

Source: California Department of Housing and Community Development, 2013 Income limits; and Veronica Tam and Associates Assumptions: 2013 HCD income limits; 30-35% gross household income as affordable housing cost; 20% of monthly affordable cost for taxes and insurance; 10% downpayment; and 4.0% interest rate for a 30-year fixed-rate mortgage loan. Utilities based on Housing Authority of the County of Los Angeles Utility Allowance, 2012 assuming use of electric services.

Based on the income limits for Los Angeles County in 2013 presented in [Table 20](#)~~Table-20~~ and current real estate asking prices documented in [Table 19](#)~~Table-19~~, homeownership in San Fernando is within the reach of some low income and most moderate income households, with the exception of single and two-person low income households. Housing options for extremely low and very low income households are virtually non-existent unless public assistance is provided.

Extremely low income and very low income households cannot afford market rents in San Fernando without subsidies or overcrowding (compare [Table 17](#)~~Table-17~~ and [Table 20](#)~~Table-20~~). Singles and families up to four persons with low, median, and moderate incomes can generally afford the market rents for apartments in the City. However, housing options for large households with five or more persons with incomes over 50 percent of AMI are extremely limited. Competition for appropriately sized rental homes may lead to an increased housing cost burden or overcrowding among these households.

5. Assisted Housing At-Risk of Conversion

The Housing Element is required to evaluate the potential for currently rent-restricted low income housing units to convert to market rate housing. This section presents an inventory of all assisted rental housing in San Fernando, including all multi-family units assisted under Federal, State, and local programs, including HUD, State/local bond programs, density bonus, inclusionary, and local redevelopment or direct assistance programs, and evaluates those units at risk of conversion during the ten year, 2013-2023 at-risk period.

As illustrated in [Table 21](#)~~Table-21~~, San Fernando has facilitated the development of several residential developments with long-term affordability covenants within its Redevelopment Project Areas.

These projects include: two senior housing projects on Jessie Street, Las Palmas I and II on Kalisher Street, and the senior units on Park Avenue. These rental projects provide a total of 112 affordable units, including 54 very low income units, and 58 low/moderate income units. The City also approved two density bonus projects in 2012 that will provide 60 lower income units during the planning period. None of these rental projects are at risk of conversion during the planning period.

Table 21: Assisted Housing Inventory

Date Built	Project Name and Location	Owner vs. Renter	Senior vs. Family	Total # Units	Restricted Affordable Units	Affordability Period	Funding Sources
1996	Park Vista Apartments #1 202 Jessie Street	Renter	Senior	8	2 very low income, 4 low income, 2 moderate income	2026 (30 years)	Former RDA
1996	Park Vista Apartments #2 222 Jessie Street	Renter	Senior	8	2 very low income, 4 low income, 2 moderate income	2026 (30 years)	Former RDA
2005	Las Palmas I and II 333/499 Kalisher Street	Renter	Senior	46	39 very low income, 6 low income, 1 market	2060 (55 years)	Former RDA, HOME, CHFA, Tax Credits
2006	Park Avenue Senior 601 Park Avenue	Renter	Senior	52	11 very low income, 40 low income, 1 market	2061 (55 years)	Former RDA, HOME, CHFA, Tax Credits
2012	San Fernando Community Housing 131 Park Avenue	Renter	Family	62	41 low income, 21 market	2067 (55 years)	Former RDA, HOME, Tax Credits
2012	Mid-Celis Apartments 1422 San Fernando Road	Renter	Family	19	19 very low income	2111 (99 years)	Former RDA, HOME, AHP, Tax Credits, Energy Efficiency Incentives
1992	Kewen Street Townhomes 1231 and 1235 Kewen St	Owner	Family	4	4 moderate income	No resale controls	Former RDA, Mortgage Revenue Bond
1999	Habitat for Humanity 1230 Mott St, 1032 Griffith St	Owner	Family	2	2 very low income	No resale controls	Former RDA

D. EXISTING HOUSING PROBLEMS

1. Overcrowding

The Census defines overcrowding as an average of more than one person per room in a housing unit (excluding kitchens, porches, and hallways). The incidence of overcrowded housing is a general measure of whether there is an available supply of adequately sized housing units. ~~Table 22~~Table 22 shows the incidence of overcrowding in San Fernando by tenure, as measured by the 2007-2011 ACS.

Table 22: Overcrowded Households

Overcrowding	Households	Percent	L.A. Co. %
Owners			
Overcrowding	351	11%	6%
Severe Overcrowding	85	3%	2%
Renters			
Overcrowding	559	20%	17%
Severe Overcrowding	177	6%	8%
Total Overcrowding	910	15%	12%

Source: American Community Survey (ACS), 2007-2011.

Note: Severe overcrowding is a subset of overcrowding.

Between 2007 and 2011, there were an estimated 910 households living in overcrowded conditions in San Fernando, representing 15 percent of all households. As ~~Table 22~~Table 22 shows, overcrowding is a more serious problem for rental households. Approximately 20 percent of renter-households were overcrowded compared to 11 percent of owner households. In comparison to Los Angeles County as a whole, household overcrowding levels are slightly higher in San Fernando.

Severe overcrowding, which is defined as more than 1.5 persons per room, was especially high among renters. Almost 177 renter-households (six percent) experienced severe overcrowding. Overcrowding is a concern for lower-income families that often double-up in order to afford adequately-sized units, or reside in illegally converted garages.

The 2007-2011 ACS shows a significant improvement with overcrowding conditions in the City compared to the 2000 Census, where 44 percent of all households in the City were overcrowded (36 percent of the owner-households and 54 percent of the renter-households). This improvement may be attributed to actual improvements in housing conditions given the lowered housing costs, smaller household size, and sampling errors of the ACS.

2. Cost Burden by Household Type

The 2005-2009 Comprehensive Housing Affordability Strategy (CHAS) data indicates that cost burden remains a critical need for lower income households, who are disproportionately affected by this burden compared to other households. Affordability problems occur when housing costs become so high in relation to income that households have to pay an excessive proportion of their income for housing, or are unable to afford any housing and are homeless.

Table 23 provides a detailed review of households that experienced housing cost burden. A cost burden is defined as spending over 30 percent of the household income on housing costs; a severe housing cost burden is spending over 50 percent of the household income on housing costs.

Among renters, the elderly were most impacted by cost burden, with nearly half of the City's estimated 235 elderly renters spending more than 50 percent of their income on rent. The addition of 96 units of affordable senior rental housing since the 2000 Census will help to address the housing affordability needs of the City's senior renter-households. Among homeowners, small family and elderly households experienced a fairly comparable level of severe cost burden, ranging from 11-16 percent; however, approximately 43 percent of large families spend more than half of their income on housing costs. These households are most at risk of foreclosure, particularly in a declining housing market with rising interest rates.

Table 23: Severe Housing Cost Burden by Type and Tenure

	Elderly	Small Family	Large Family	Other	Total
Renter-Households					
Total # by Household Type	235	1,460	750	475	2,920
% with Cost Burden	68%	57%	61%	45%	57%
% with Severe Cost Burden	47%	27%	23%	12%	25%
Owner-Households					
Total # by Household Type	460	1,380	895	295	3,030
% with Cost Burden	33%	40%	64%	61%	48%
% with Severe Cost Burden	11%	16%	43%	20%	24%

Source: U.S. Department of Housing and Urban Development (HUD), Comprehensive Housing Affordability Strategy (CHAS), 2005-2009

3. Cost Burden by Household Income

CHAS data also provides housing cost burden information by household income and tenure. As shown in **Table 24**, extremely low and very low income renter-households were most impacted by housing cost burden.

Table 24: Severe Housing Cost Burden by Income and Tenure

	Extremely Low	Very Low	Low	Moderate/ Above	Total
Renter-Households					
Total # by Household Type	780	585	655	900	2,920
% with Cost Burden	83%	89%	54%	15%	57%
% with Severe Cost Burden	61%	26%	14%	0%	25%
Owner-Households					
Total # by Household Type	320	440	755	1,515	3,030
% with Cost Burden	67%	78%	62%	28%	48%
% with Severe Cost Burden	47%	60%	34%	3%	23%

Source: U.S. Department of Housing and Urban Development (HUD), Comprehensive Housing Affordability Strategy (CHAS), 2005-2009

III. HOUSING CONSTRAINTS

The provision of adequate and affordable housing can be constrained by a number of factors. This section assesses the various governmental, market, infrastructure and environmental factors that may serve as a potential constraint to housing development and improvement in San Fernando.

A. GOVERNMENTAL CONSTRAINTS

1. Land Use Controls

The San Fernando General Plan, Zoning and Subdivision Ordinances provide for a range of residential land use designations/zones in the City:

- *Low Density Residential (R-1)* – up to six dwelling units per acre
- *Medium Density Residential (R-2)* – up to 17 dwelling units per acre
- *High Density Residential (R-3)* – up to 43 dwelling units per acre

In addition to these density provisions, the City has adopted a Residential Planned Development (RPD) Overlay Zone that provides flexible development standards within the density standards of the underlying zone district. The intent of the RPD overlay is to permit a more efficient and aesthetic use of land through the arrangement of buildings not permitted through the strict application of the underlying zoning's development regulations. Providing such flexibility in development standards can result in lowering the cost of development.

The City's residential development standards are summarized in [Table 25](#)~~Table 25~~. The City's standards are fairly comparable to other Los Angeles County communities, and have not served as a constraint to development. The cumulative effects of the City's standards in themselves do not serve to limit densities, although other site factors - such as small parcel sizes or the desire to preserve an existing residential structure - often result in projects developed at less than maximum densities. These factors are taken into consideration when assessing the realistic development capacity of the Housing Element sites inventory in Section IV. San Fernando has a history of providing for modified standards in support of affordable and senior housing, and has incorporated this practice as part of the density bonus program in the updated Housing Element. For example, the Las Palmas and Park Avenue senior housing developments received density increases, a 50 percent reduction in parking requirements, and increased lot coverage from 40 to 50 percent. Another seven multi-family developments with 254 units benefited from density bonuses and related incentives since 2010. San Fernando has adopted an administrative modification process to accommodate minor reductions in certain development standards - including residential setback and parking dimensions. By allowing such modifications through an administrative process, the requirement for making variance findings and conducting a public hearing is eliminated, thereby reducing the time and cost for project processing.

Table 25: Residential Development Standards

	R-1	R-2	R-3	RPD
Density	1 du/lot	1 du/2,562 sq.ft. lot area	1 du/1,013 sq. ft. lot area	Based on underlying zone
Minimum Lot Size	7,500	7,500	7,500	2 acres total, 5,000 sq.ft./lot
Minimum Lot Width	50*	50*	50*	50*
Minimum Lot Depth	100	100	100	100
Minimum Setbacks	Setbacks for lots abutting a street. Interior lots and lots off alleys have reduced setback requirements.			
Front (ft.)	20	20	20	15 – 25
Side (ft.)	5	5	5	5
Rear (ft.)	20	20	20	25
Maximum Height (ft.)	35	35	45	35 or 2 stories
Max. % Lot Coverage	50	40	40	50
Usable Open Space (Private)	--	150/unit**	150/unit**	Based on underlying zone
Common Area/Recreational	--	100/unit**	100/unit**	Based on underlying zone
Parking				
Single Family	2	2	2	2
Multi-Family	--	1 br = 1.5 covered 2 br = 2.0 covered 3 br = 2.5 covered Each additional br above 3 adds 0.5 covered	1 br = 1.5 covered 2 br = 2.0 covered 3 br = 2.5 covered Each additional br above 3 adds 0.5 covered	2 covered
Guests	--	0.2 per unit	0.2 per unit	--

Source: San Fernando Municipal Code Chapter 106: Zoning.

* 55 feet for corner lots

** For development with more than four units

San Fernando Corridors Specific Plan

In January 2005, the City adopted the San Fernando Corridors Specific Plan. Amended in December 2017, the Plan lays out a policy and regulatory framework for the revitalization of the North Maclay Avenue, Truman Street, and San Fernando Road corridors. These three commercial arterials are currently characterized by strip commercial land uses, with the goal of the Specific Plan to transform these areas into distinctive districts providing for residential, retail, restaurant, entertainment and public uses. The vision for each of these districts can be described as follows:

- **The Maclay District** is established as a neighborhood spine for the community. It is planned to transition into a residentially-focused corridor that can provide new housing, while complementing the adjacent neighborhoods that lie behind it.
- **The Downtown District** is intended as a focal point of activity, concentrating civic and retail activity into one walkable district. Residential uses are permitted on the upper floors of multi-story buildings.

- **The Mixed-Use Corridor District** is established as a pedestrian-oriented, mixed-use neighborhood located between the San Fernando Mall and the Sylmar/San Fernando Metrolink Station. Commercial and mixed-use buildings face the sidewalk with inviting shopfront frontages, particularly along San Fernando Road. Residential buildings are setback behind small setbacks with ground floor units accessed through lobbies or directly to each unit through stoops or porches.
- **The Auto Commercial District** is established as a flexible district devoted to automobile-related uses, particularly auto dealerships, while also accommodating office and retail uses. Residential uses are not permitted within the Auto Commercial District.
- **The General Neighborhood District** is established to ensure that new multi-family housing along the south side of Second Street provides a suitable transition between Downtown's mixed-use buildings, First Street's commercial and light industrial buildings, and the single-family residential neighborhoods to the north. New multi-family buildings face Second Street with two-story masses at the front of the lot with massing that is in character with the single-family houses across the street. Higher masses - up to four stories - are allowed at the center and rear of the lot.
- **The Workplace Flex District** supports the continued functioning and expansion of the City's light industrial, workshop, and large-scale commercial sectors. It also accommodates live-work uses, subject to a conditional use permit.

~~**The Truman/San Fernando District** is established as the City's workplace district, with appropriate areas for housing and limited retail. Within the larger district, the Mixed-Use Transition Sub-District will support development of a mix of use type, including residential and office uses, and limited areas of retail and services.~~

As illustrated in ~~**Error! Reference source not found.**~~ ~~Table 26~~, the Specific Plan establishes generous development standards as a means of facilitating development. ~~Both the~~ The Downtown District ~~and Truman/San Fernando District~~ provides an FAR bonuses for the inclusion of residential, with a one-story height bonus ~~also offered in the Truman/San Fernando District.~~

Table 26: San Fernando Corridors Specific Plan - Development Standards

	Maclay District	Downtown District	Truman/San Fernando District*	Mixed-Use Corridor District	Auto Commercial District	Workplace Flex District	General Neighborhood District
<u>Floor Area Ratio (FAR)</u>	1.5 FAR* with mixed-use**	3.0 FAR , 3.5 FAR with mixed-use***		<u>3.0</u>	<u>3.0</u>	<u>2.0</u>	<u>n/a</u>
Density <u>(du/acre)</u>	12 - 36 <u>37 du/acre;</u>	<u>24 – 50</u> with mixed-use***	25 – 45 du/acre; <u>2.5 FAR</u> with mixed-use**	<u>24 – 37</u>	<u>n/a</u>	<u>18</u>	<u>43</u>
Minimum Setbacks							
Front (ft.)	0 - 15	1 st floor – 0 2 nd story residential – 15 - 20	San Fernando – 0 Other – 15	<u>0 - 15</u>	<u>15</u>	<u>4 – 15</u>	<u>20</u>
Side (ft.)	5 - 10	0	San Fernando – 0 Other – 5 – 15	<u>5 – 10+</u>	<u>5 - 10</u>	<u>5 – 15</u>	<u>1st floor: 5</u> <u>Upper floors: 10</u>
Rear (ft.)	15	None	10	<u>None</u>	<u>None</u>	<u>10</u>	<u>20</u>
Maximum Height	<u>Lesser of 3 floors (40 ft)</u>	<u>Lesser of 4 floors (50 ft)</u>	3 floors (40 ft); 4 floors (50 ft) with mixed-use	<u>Lesser of 3 floors (40 ft)</u>	<u>Lesser of 3 floors (40 ft)</u>	<u>Lesser of 3 floors (40 ft)</u>	<u>Lesser of 4 floors (40 ft);</u> <u>Lesser of 2 floors (24 ft) along Second Street frontage</u>
Publicly Accessible Open Space <u>(sq. ft.)</u>	150 per unit	150 per unit	150 per unit	<u>150 per unit</u>	<u>n/a</u>	<u>none</u>	<u>100/unit</u>
Private Open Space <u>(sq. ft.)</u>	60 <u>50</u> per unit	60 per unit	60 per unit	<u>60 per unit</u>	<u>n/a</u>	<u>50 per unit</u>	<u>150/unit</u>

Parking	Studio	1 Bedroom	2 Bedroom or Larger
Multi-Family	1 covered	1 covered	2 covered
Guests	0.2 spaces/unit (option for off-site or in-lieu fee)		

~~—*~~ ~~Standards apply to Mixed-Use Transition Sub-District within the SP-4 Zone.~~

~~**~~ Area from parking facilities or structures is not used to calculate maximum FAR

~~**~~ Mixed-Use allowed only within Neighborhood Services Overlay

~~***~~ Residential allowed only within Downtown Residential Overlay

~~+~~ Min. front setback for buildings facing Celis Street between San Fernando Misoisn Blvd. and Hubbard Ave. is 10 ft.

Although local density bonus procedures had not been adopted until October 2013, the City worked with developers to grant incentives and concessions to facilitate the approval of 254 market-rate and affordable housing units since 2010 utilizing the State's density bonus provisions. Regulatory concessions approved in conjunction with increased density included reduced parking, common open space, private open space, setbacks, and increased lot coverage.

2. Provision for a Variety of Housing Types

Housing Element law specifies that jurisdictions must identify adequate sites to be made available through appropriate zoning and development standards to encourage the development of various types of housing for all economic segments of the population. [Table 27](#)~~Table 27~~ summarizes the housing types permitted in each of the San Fernando zoning districts, inclusive of the San Fernando Corridors Specific Plan (~~SP-4~~[SP-5](#)) zone.

Table 27: Housing Types by Residential Zone Category

Housing Types Permitted	Zoning District						San Fernando Corridors Specific Plan District						
	R-1	R-2	R-3	C-1, C-2	M-1	M-2	Maclay	Downtown*	Truman/San Fernando**	Mixed-Use Corridor	Auto Commercial	Workplace Flex	General Neighborhood
Single-Family	P	P	P	--	--	--	--	--	—		==	==	<u>P</u>
Multiple-Family	--	P	P	--	--	--	<u>PC</u>	<u>PC</u>	<u>P</u>	<u>C</u>	==	==	<u>C</u>
Residential Condominium	--	C	C	--	--	--	<u>PC</u>	<u>PC</u>	<u>P</u>	<u>C</u>	==	==	<u>C</u>
Accessory Second Dwelling Units	P	P	P	--	--	--	--	--	—	==	==	==	==
Mobile Home Park	--	C	C	--	--	--	--	--	—	==	==	==	==
Manufactured Housing	P	P	P	--	--	--	--	--	—	==	==	==	==
Live/Work	--	--	--	--	--	--	<u>CP</u>	<u>PC</u>	<u>P</u>	<u>C</u>	==	<u>C</u>	==
Boardinghouses	--	--	C	--	--	--	--	--	—		==	==	==
Single Room Occupancy (SRO)	--	--	--	C	--	--	--	--	—		==	==	==
Care Facilities (6 or fewer)	P	P	P	--	--	--	P	P	<u>P</u>	<u>P</u>	==	==	<u>P</u>
Care Facilities (7 or more)	C	C	C	C	--	--	C	C	<u>C</u>	<u>C</u>	==	==	<u>C</u>
Transitional Housing	P	P	P	--	C	C	<u>PC</u>	<u>PC</u>	<u>P</u>	<u>C</u>	==	==	<u>C</u>
Supportive Housing	P	P	P	--	--	--	<u>PC</u>	<u>PC</u>	<u>P</u>	<u>C</u>	==	==	<u>C</u>
Emergency Shelters	--	--	--	--	--	P	--	--	—	==	==	==	==
Farmworker Housing	--	--	--	--	--	--	--	--	—	==	==	==	==

“P” = Permitted “C” = Conditional Use Permit “--” = Prohibited

* Residential uses permitted on upper stories only within the Downtown Residential District Overlay within the SP-4/SP-5 Zone.

** ~~Applicable to Mixed-Use Transition Sub-District within the SP-4 Zone.~~

Multi-Family Residential

The San Fernando Zoning Code expressly permits duplexes and multiple-family dwelling units in the R-2 and R-3 zone districts. Since 2008, the City has approved entitlements for 265 new affordable and market rate multi-family dwelling units. City Code Section 106-6 provides the following definition for multiple-family dwelling:

“a dwelling consisting of three or more dwelling units per lot, including townhouses, condominiums, and apartments.”

However, the City’s Zoning Code also identifies residential condominiums as requiring a CUP in the R-2 and R-3 zones. Since 2005, the City has approved CUPs on two separate occasions for residential condominium projects within the R-2 zone, amounting to the approval of nine market rate residential units. In addition, the SP-5 Zone of the San Fernando Corridors Specific Plan requires a CUP for all multi-family development, whether apartments or condominiums, within the Downtown District, Mixed-Use District, Maclay District, and General Neighborhood District. Furthermore, the SP-4 Zone of the San Fernando Corridors Specific Plan allows by-right development of multi-family residential apartment units within the Mixed-Use Transitional Sub-District, Downtown District, and Maclay District; eCondominiums are permitted with the City’s approval of a parcel or tract map. Under the 2005 Corridors Specific Plan (SP-4), 76 multi-family dwellings have been approved since 2007, including two density bonus projects.

Second-Accessory Dwelling Units

The passage of AB 1866 (effective July 2003) requires local governments to use a ministerial process for second-accessory dwelling unit applications for the purpose of facilitating production of affordable housing. AB 1866 does allow cities to impose development standards on second-accessory dwelling units addressing issues such as building size, parking, height, setbacks, and lot coverage. The passage of SB 1069 and AB 2299 (both effective January 2017) reduces the barriers California homeowners face when seeking to build an accessory dwelling unit by eliminating excessive sprinkler requirements, providing several exceptions to parking restrictions, such as if the home is located within a half mile from public transit, requiring ministerial approval for the remodeling of existing homes and garages when they are compliant with building and safety codes, and making utility connection fees for brand new construction proportionate to the burden the accessory dwelling will place on the water or sewer systems.

In January 2004, the San Fernando City Council added Section 106-358 to the Zoning Code to permit second-accessory dwelling units as an accessory use in all residential zone districts. In July 2017, in response to the passage of SB 1069 and AB 2299, the City amended and retitled Section 106-358 of the Zoning Code to be consistent with Government Code sections 65852.2 and 65852.150. Per Section 106-358 of the City Code:

- Accessory dwellings are allowed in the R-1 (Single-Family residential); R-2 (Multiple Family Dwelling); or R-3 (Multiple family) zones.
- The lot on which the accessory dwelling unit is constructed may contain no more than one lawful and pre-existing single-family residence, and not more than one accessory dwelling unit. Upon the addition of an accessory dwelling unit on an R-2 or R-3 zoned parcel, the development of additional dwelling units is prohibited.
- The maximum size of the detached ~~second~~ accessory dwelling unit is 640 square feet and the unit is required to be located to the rear of the existing primary single-family dwelling. Attached units are also permitted, with the City Code allowing the construction of an attached unit that is 30 percent of the floor area of the existing single-family dwelling.
- One additional on-site ~~(non-tandem)~~ parking space (which may be provided in a tandem configuration, on an existing driveway, or within a paved setback) is required for each bedroom in the ~~second~~ accessory unit. No parking shall be required, however, if the accessory dwelling unit is located within one-half mile of public transit; is located within an architecturally and historically significant district; when the accessory dwelling unit is part of the existing primary residence or an existing accessory structure; when on-street parking permits are required but not offered to the occupant of the accessory dwelling unit; or when there is a car share located within one block of the accessory dwelling unit.
- All of the required parking spaces for the primary single-family dwelling and the ~~second~~ accessory dwelling unit must be permanently reserved, maintained and used as accessible parking for vehicles.
- An existing garage shall not be converted to an ~~accessory~~ ~~second~~ dwelling unit unless alternate covered parking is provided on-site that meets current zoning and building code requirements. In an effort to increase the number of potentially affordable units, the City's ~~second~~ accessory dwelling unit requirements do provide possible modifications to the development standards as part of a conditional use permit approval process that may allow for the reduction in parking standards, increase in the ~~accessory~~ ~~second~~ dwelling unit's building square footage and/or reduced building setbacks.
- ~~The City requires that e~~Either the primary single-family dwelling or the ~~second~~ accessory dwelling unit ~~to~~ must be occupied by the owner of the lot. The property owner is required to enter into a restrictive covenant with the City, with the restrictive covenant recorded on the property to enforce these provisions.
- Accessory dwelling units are not considered to be new residential for the purpose of calculating local agency utility connection fees or capacity charges for utilities, including water and sewer service. In addition, the City does not require a separate utility connection between the accessory dwelling unit and the utility.

~~Second~~ Accessory dwelling units are subject to site plan review and approval by the Community Development Department. Since adoption of the new ministerial ~~accessory~~ ~~second~~ unit provisions in 2004, the City has received a total of 65 applications for ~~accessory~~ ~~second~~ units, indicative of the demand for such units in the community. Accessory ~~Second~~ units are being provided in several configurations, including above a

detached garage, as a ~~stand-alone~~stand-alone detached structure, attached to the existing single-family home, and through conversion of an existing accessory structure. A large number of ~~accessory~~second unit applications result from code enforcement referrals, with staff working with property owners to remedy illegal conditions (garage conversions, converted recreation or storage rooms, subdivided single-family dwellings, etc.) and replacement of these un-permitted structures with legal ~~accessory~~second units.

Manufactured Housing and Mobile Home Parks

State law requires jurisdictions to permit manufactured housing as defined in Health and Safety Code Section 18007 in any residential district where single-family detached units are permitted subject to the same property development standards. In accordance with State law, the City amended the Zoning Code in March 2013 to accommodate manufactured housing in the R-1, R-2, and R-3 zones. Since 2002, the City has approved two manufactured housing units constructed as single-family residential structures, each on an individual residential parcel. The City also allows the development and operation of mobile home parks with a CUP in the R-2 and R-3 districts.

Live/Work

The San Fernando Corridors Specific Plan (SP-5) accommodates live/work units within the ~~Mixed Use Transitional Sub District~~, Downtown District, the Mixed-Use Corridor District, the Workplace Flex District, and the Maclay District. The residential use must be the predominant use of the premise and occupational activity is limited to a business office or a studio, including the making of arts and crafts or other occupational activity compatible with a residential use.

Boardinghouses

The City's Zoning Ordinance defines "boardinghouse" as "a building where lodging and meals are provided for compensation for five or more persons, not including a retirement home." Boardinghouses, which are allowed in San Fernando with approval of a CUP in the R-3, provide a housing type that can accommodate special needs households, including extremely low income households.

Single Room Occupancy (SRO)

Like boardinghouses, SROs can be a source of affordable housing for special needs households, including extremely low income households. The City revised the Zoning Ordinance in March 2013 to include the following definition and allow SROs with a CUP in the C-1 and C-2 zones:

"Single room occupancy unit (SRO)" means any building containing five or more guestrooms or units intended or designed to be used, or which are used,

rented, or hired out, to be occupied, or which are occupied, for sleeping purposes by residents, which is also the primary residence of those residents. The individual units shall lack either cooking facilities or individual sanitary facilities, or both. However, for purposes of this definition, an SRO does not include residential care homes, senior housing projects, rooming and boarding houses, hotels and motels, bed and breakfast lodging, extended care facilities or hospitals.

SROs are subject to a number of development standards that are intended to facilitate and not discourage the provision of safe and function units for occupants. For example, units must be between 150 and 400 square feet and may - but are not required to - contain full or partial kitchens or bathrooms. Each unit must have a separate closet and the facility must provide common areas, laundry facilities, a cleaning supply room. The owner must submit a management plan for the facility and projects with 10 or more units must provide office space for an on-site manager. At least one parking space per unit plus one space for an on-site manager is also required.

Housing for Persons with Disabilities

Community Care Facilities: The Lanterman Developmental Disabilities Services Act and Community Care Facilities Act state that mentally, physically, developmentally disabled persons and children and adults who require supervised care are entitled to live in normal residential settings. To that end, State law requires that licensed family care homes, foster homes, and group homes serving six or fewer persons be treated like single-family homes and be allowed by right in all residential zones.

The City amended the Zoning Code in March 2013 to include the following definitions of “community care facilities”:

“Community care facility/Small means any facility as defined in the Health and Safety Code Section 1502(a), which provides nonmedical care on a 24-hour a day basis to six or less persons including, but not limited to persons with substance abuse illnesses, physically handicapped, mentally impaired, incompetent persons, and abused or neglected children. Small community care facility shall be considered a permitted use within all residential zoned districts.”

“Community care facility/Large means any facility as defined in the Health and Safety Code Section 1502(a), which provides nonmedical care on a 24-hour a day basis to seven or more persons including, but not limited to persons with substance abuse illnesses, physically handicapped, mentally impaired, incompetent persons, and abused or neglected children. Large community care facility shall be considered a conditionally permitted use within all residential zoned districts.”

The R-1, R-2 and R-3 zone districts permit community care facilities/small by-right. The Zoning Code does not subject small community care facilities to a use permit, building standard, or regulation not otherwise required of single-family homes in the same zone, and imposes no spacing requirements between small community care facilities; a licensed six-bed adult residential facility is currently in operation on Jackman Avenue.

Large community care facilities (i.e., those that serve seven or more clients) are allowed with approval of a CUP in residential zones and the C-1 and C-2 zones. These facilities are subject to the same development standards, permit processing procedures, and findings for approval as all other uses that require a CUP in these zones. Furthermore, there are no spacing requirements between large community care facilities.

~~Although the~~The San Fernando Corridors Specific Plan ~~does not explicitly list small community care facilities as an allowable use, the Specific Plan includes a provision that allows the City's chief planning official to permit similar and compatible uses. The Zoning Code treats small community care facilities as a regular residential use and these facilities are allowed by right within residential zones, including the R-2 and R-3 zones, which permit multi-family uses. Accordingly, small community care facilities are considered similar to and compatible with multi-family residential uses that are~~ allowed small community care facilities by-right within the Specific Plan's Downtown District, Mixed Use Transitional Sub-Corridor District, Downtown District, and Maclay District, and General Neighborhood District. The San Fernando Corridors Specific Plan allows large community care facilities subject to a conditional use permit within the Specific Plan's Downtown District, Mixed Use Corridor District, Maclay District, and General Neighborhood District.

Definition of Family: The following definition of “family” contained in the Zoning Code does not limit housing opportunities for persons for disabilities:

“Family” means any number of persons living together in a room comprising a single dwelling unit and related by blood, marriage or adoption or bearing the generic character of a family unit as a relatively permanent single household, including servants and other live-in employees, who reside therein as though members of the family. Any group of persons not related by blood, marriage or adoption but inhabiting a dwelling unit shall for the purpose of this chapter be considered to constitute one family if a bona fide single household, including servants and other live-in employees, is contained in such group.

Reasonable Accommodation: Both the Federal Fair Housing Act and the California Fair Employment and Housing Act impose an affirmative duty on local governments to make reasonable accommodations in their zoning and other land use regulations as necessary to afford disabled persons an equal opportunity to use and enjoy a dwelling. For example, it may be a reasonable accommodation to allow covered ramps in the setbacks of properties that have already been developed to accommodate residents with mobility impairments. San Fernando accommodates most accessibility modifications through issuance of a building

permit. Handicapped ramps or guardrails (up to 42" in height) are permitted to intrude into the standard setbacks required under zoning to allow first floor access for physically disabled residents, eliminating the need for a zoning variance. Furthermore, in October 2013, the City adopted a formal ministerial procedure for reviewing and approving requests for reasonable accommodation under Federal and State law (City Code Chapter 106, Article VI, Division 16). Currently the City does not charge a fee for reasonable accommodation requests; however, the ordinance states that a fee will be collected after the City studies options and a fee is established by resolution.

Building Codes: For new construction, the City's building code requires new housing to comply with the 1998 amendment to the Fair Housing Act, with multi-family development also subject to the Americans with Disabilities Act (ADA) standards. New apartment buildings are subject to requirements for unit "adaptability" on ground floor units. Adaptable units are built for easy conversion to disabled access, such as doorway and hallway widths, and added structural support in the bathroom to allow the addition of handrails.

Conclusion: The City has conducted a review of zoning and building code requirements, and has not identified any barriers to the provision of accessible housing.

Transitional Housing

In compliance with State law (SB2), the City amended the Zoning Code in March 2013 to allow transitional housing by-right within the R-1, R-2, and R-3 zones and subject to the same permitting process and development standards as other residential uses within these zones. The Zoning Code amendment also added the following definition:

"Transitional housing" means housing operated under program requirements that call for 1) the termination of any assistance to an existing program recipient and 2) the subsequent recirculation of the assisted residential unit to another eligible program recipient at some predetermined future point in time, which point in time shall be no less than six months into the future (Health and Safety Code Section 50675.2(h)). Transitional housing may provide, but not be limited to, meals, counseling, and other services as well as common areas for residents. Transitional housing may be provided under all residential housing types. In all cases, Transitional housing shall be treated as a residential use under this chapter and shall be subject only to those restrictions that apply to other residential uses of the same residential housing type located in the same zoning district.

~~Although the San Fernando Corridors Specific Plan allows transitional housing subject to a conditional use permit within the Downtown District, the Mixed-Use Corridor District, the General Neighborhood District, the Workplace Flex District, and the Maclay District. does not explicitly list transitional housing as an allowable use, the Specific Plan includes a provision that allows the City's chief planning official (the Community Development Director) to permit similar and compatible uses. The Zoning Ordinance treats transitional~~

~~housing as a regular residential use and this use is allowed by right within residential zones, including the R-2 and R-3 zones, which permit multi-family uses. Accordingly, transitional housing is considered similar to and compatible with multi-family residential uses that are allowed by right within the Specific Plan's Mixed Use Transitional Sub-District, Downtown District, and Maclay District. As with all residential uses, the applicant of transitional housing is required to obtain a land use determination to ensure the proposed use is an allowable use within the specific zoning district (including in the Specific Plan). The Community Development Director or his or her designee will make the determination via an administrative process to deem the transitional housing (meeting SB-2 definition) as a residential use subject to the same regulations as similar uses in the same zone. This process is similar for all residential uses.~~

~~The City also accommodates transitional housing within the M-1 and M-2 districts with approval of a CUP.~~

Supportive Housing

Supportive housing links the provision of housing and social services for the homeless, people with disabilities, and a variety of other special needs populations. The City amended the Zoning Code in March 2013 to comply with recent changes in State law (SB2) and accommodate supportive housing as a regular residential use within the R-1, R-2, and R-3 zones and subject to the same permitting process and development standards as other residential uses within these zones. The City also amended the Zoning Code to include the following definition:

"Supportive housing" means housing with no limit on the length of stay and that is occupied by a target population as defined by Health and Safety Code Section 53260(d), as the same may be amended from time to time, and that provides a significant level of onsite and offsite services that assist the supportive housing residents in retaining the housing, improving their health status, maximizing their ability to live, and when possible, work in the community. Supportive housing shall be treated under this chapter as a residential use and shall be allowed as a permitted use in all residential zoning districts.

~~The San Fernando Corridors Specific Plan allows supportive housing subject to a conditional use permit within the Downtown District, the Mixed-Use Corridor District, the General Neighborhood District, the Workplace Flex District, and the Maclay District. Although the San Fernando Corridors Specific Plan does not explicitly list supportive housing as an allowable use, the Specific Plan includes a provision that allows the City's chief planning official (Community Development Director) to permit similar and compatible uses. The Zoning Ordinance treats supportive housing as a regular residential use and this use is allowed by right within residential zones, including the R-2 and R-3 zones, which permit multi-family uses. Accordingly, supportive housing is considered similar to and compatible with multi-family residential uses that are allowed by right within the Specific Plan's Mixed~~

~~Use Transitional Sub District, Downtown District, and Maclay District. As with all residential uses, the applicant of supportive housing is required to obtain a land use determination to ensure the proposed use is an allowable use within the specific zoning district (including in the Specific Plan). The Community Development Director or his or her designee will make the determination via an administrative process to deem the supportive housing (meeting SB 2 definition) as a residential use subject to the same regulations as similar uses in the same zone. This process is similar for all residential uses.~~

Emergency Shelters

As discussed in homeless section of the Housing Element Needs Assessment (Section II), there are an estimated 12 homeless persons in San Fernando. Although numerous emergency shelters are located in the San Fernando Valley, existing shelters are all overcapacity and there is a need for additional shelters in the area. In compliance with recent changes to state law (SB 2), the City amended the Zoning Code in March 2013 to define and accommodate emergency shelters with a ministerial permit. The Zoning Code includes the following definition:

“Emergency homeless shelter” means housing with minimal supportive services for homeless persons that is limited to occupancy of six months per calendar year or less by a homeless person. No individual or household may be denied emergency shelter because of an inability to pay. (Health and Safety Code Section 50801(e)). Supportive services may include, but are not limited to, meal preparation, an activities center, day care for homeless person’s children, vocational rehabilitation and other similar activities.

Pursuant to SB 2, San Fernando conducted a review of its zoning districts and determined the M-2 (Light Industrial) Zone is most conducive to provision of an emergency homeless shelter. The M-2 zone is characterized by industrial land uses including contractors storage yards, manufacturing, food processing, and auto repair uses. The two primary M-2 corridors in San Fernando include: First Street between Hubbard Street and North Maclay Avenue, just north of the City’s bike path and the Southern Pacific Railroad right of way; and Arroyo Avenue, between Fifth and Eighth Street, just east of the Pacoima Wash and the City’s easterly boundary with the City of Los Angeles. Both M-2 Zone corridors are accessible to public transit. M-2 zoned properties located along First Street are within walking distance of the Sylmar/San Fernando Metrolink Station located at Hubbard and First Street, providing access to light rail and bus transportation services. The M-2 Arroyo Avenue corridor is within walking distance of bus stops located on Glenoaks Boulevard and Foothill Boulevard, two major vehicular thoroughfares providing access between the City and the neighboring communities of Sylmar and Pacoima.

The M-2 zone spans over 98 acres on 78 parcels, including more than 24 acres on 12 vacant parcels. A review of potential underutilized sites within the M-2 zone identified between 25 underutilized properties with improvement values reported to be less than half the reported land value indicating that the properties are feasible for either redevelopment or potential

conversion of existing structures to shelter use. Therefore, the City's vacant and underutilized M-2 parcels have more than adequate capacity to accommodate to City's estimated unsheltered need of 12 homeless persons and at least one year-round emergency shelter.

The City allows emergency shelters in the M-2 Zone with approval of a ministerial permit that is subject to the same development and operational standards as other permitted uses in the zone. The City's M-2 development standards are appropriate to facilitate emergency shelters, and can be summarized as follows:

- Minimum lot size: 10,000 square feet
- Maximum lot coverage: 60 percent
- Maximum height: 45 feet
- Minimum lot width: 75 feet
- Setbacks: 10 foot front, side and rear
- Walls: 8 foot where property abuts residential, 6 foot otherwise
- Landscaping: Min 10 foot front landscaping; 15% of total lot shall be landscaped.
- Storage: No outside storage permitted

In addition to application of M-2 development standards, pursuant to SB 2, the City adopted the following written, objective standards to regulate the following aspects of emergency shelters to enhance compatibility:

- Maximum Number of Persons/Beds. The shelter for the homeless shall contain a maximum of 30 beds and shall serve no more than 30 homeless persons.
- Lighting. Adequate external lighting shall be provided for security purposes. The lighting shall be stationary, directed away from adjacent properties and public rights-of-way, and of an intensity compatible with the neighborhood.
- Laundry Facilities. The development shall provide laundry facilities adequate for the number of residents.
- Common Facilities. The development may provide supportive services for homeless residents, including but not limited to: central cooking and dining room(s), recreation room, counseling center, child care facilities, and other support services.
- Security. Parking facilities shall be designed to provide security for residents, visitors, and employees.
- Landscaping. On-site landscaping shall be installed and maintained pursuant to the standards outlined in Section 106-833.
- On-Site Parking. On-site parking for homeless shelters shall be subject to requirements for similarly zoned industrial uses as set forth in Section 106-822(d)(1).
- Outdoor Activity. For the purposes of noise abatement in surrounding residential zoning districts, outdoor activities may only be conducted between the hours of 8:00 a.m. to 10:00 p.m.

- Concentration of Uses. No more than one shelter for the homeless shall be permitted within a radius of 300 feet from another such shelter.
- Refuse. Homeless shelters shall provide a trash storage area as required pursuant to Section 106-897(1) through Section 106-897(3).
- Health and Safety Standards. The shelter for the homeless must comply with all standards set forth in Title 25 of the California Administrative Code (Part 1, Chapter F, Subchapter 12, Section 7972).
- Shelter Provider. The agency or organization operating the shelter shall comply with the following requirements:
 - Temporary shelter shall be available to residents for no more than six months if no alternative housing is available.
 - Staff and services shall be provided to assist residents to obtain permanent shelter and income. Such services shall be available at no cost to all residents of a provider's shelter or shelters.
 - The provider shall not discriminate in any services provided.
 - The provider shall not require participation by residents in any religious or philosophical ritual, service, meeting or rite as a condition of eligibility.
 - The provider shall have a written management plan including, as applicable, provisions for staff training, neighborhood outreach, security, screening of residents to insure compatibility with services provided at the facility, and for training, counseling, and treatment programs for residents."

Farm Employee Housing

Because there is a very limited presence of farmworkers in the community, the City has not identified a need for specialized farmworker housing beyond overall programs for housing affordability. According to the 2007-2011 ACS, 116 San Fernando residents were employed in agriculture, forestry, fishing and hunting, or mining industries, representing approximately one percent of the City's labor force. The City allows agricultural uses within the M-1 and M-2 zones; however, agriculture is not the primary intent of these zones and therefore the farmworker housing provisions of the California Employee Housing Act are not applicable.

Employee Housing

Under the Employee Housing Act, any employee housing providing accommodations for six or fewer employees shall be deemed a single-family structure with a residential land use designation. The City will amend the Zoning Code to address the provision of employee housing.

3. Site Improvements

As an entirely urbanized community, all of the necessary infrastructure systems in San Fernando are already in place, and are generally sufficient to accommodate the level of development that occurs in the City. One exception is the City's aging water conveyance system, which is being replaced on an ongoing basis through payment of a Capital Facility Water charge on new development. The City actively maintains public improvements in association with new development, such reconstruction of older streets, curbs, gutters and sidewalks, in association with the construction of new private facilities.

Local streets comprise the majority of the residential street network in San Fernando, in contrast to major and secondary arterial streets. The City's standards for local streets right-of-way is 60 feet, with a curb-to-curb pavement width of 36 to 40 feet, having two lanes, and on-street parallel parking on both sides of the street. Within portions of the San Fernando Corridors Specific Plan, a Pedestrian Oriented Corridor roadway classification has been established to accommodate pedestrian use while meeting the demands of local traffic. This is accomplished through various traffic calming techniques and reducing travel lanes to one in each direction.

4. Development Fees

Various fees and improvements are charged by the City to cover costs of processing permits and providing services and facilities, such as utilities, schools and infrastructure. Almost all of these fees are charged on a prorata share system, based on the magnitude of the project's impact or the extent of benefit that will be derived.

Table 28~~Table 28~~ shows the list of fees charged for a typical residential development in San Fernando. In addition, there may be other fees assessed depending on the circumstances of the development. For example, the building may need to pay an inspection fee for sidewalks, curbs and gutters if their installation is needed. In order to lower the cost associated with building affordable housing, the City provides for reduced fees and modified development standards. For example, in December 2012, the City approved \$250,000 in funding assistance for the Harding Street Affordable Housing Project to facilitate an application for Los Angeles County HOME Funds to develop a 29-unit affordable apartment building. The local funding assistance package offered by the City included \$25,000 in waived building permit and water connection fees in addition to \$225,000 in partial funding for required off-site street and wastewater collection system improvements.

The City also charges two Capital Facility Fees: one for the sewer system, and one for the water system. The Capital Facility Sewer Charge is necessary so that the City can pay its share of upgrading the Hyperion Treatment Plant to increase its capacity. The Capital Facility Water Charge is necessary so that the City can replace its old, substandard water conveyance system.

Table 28: Permit Processing Fees

Type of Fee	Cost
Site Plan Review	\$1,763
Preliminary Parcel Map	\$2,187 + \$100/lot
Tentative Tract Map	\$2,451 + \$100/lot
Precise Development	\$1,015
Variance	\$2,567
Conditional Use Permit	\$3,005
Zone Change	\$3,601
General Plan Amendment	\$3,601
Environmental Documentation	
Categorical Exemption	\$204
Initial Study	Staff hourly rate
EIR	Staff hourly rate

Source: City of San Fernando, FY 2012/13.

As a means of further assessing the cost that fees contribute to development in San Fernando, the City has calculated total Planning, Building and Public Works fees associated with three common types of residential development in the community: a two-unit duplex, a 37-unit condominium development, and a 20-unit apartment building.

As indicated in Table 29~~Table 29~~, per-unit fees for a prototypical duplex run \$11,222, whereas prototypical fees run \$19,677 per condominium unit and \$7,145 per apartment unit. The City typically was able to use a Class 32 Categorical Exemption for infill development under CEQA, reducing the costs associated with preparation of an environmental document. Residential development fees constitute well under five percent of the estimated per-unit development cost.

A 2012 National Impact Fees Survey (www.impactfees.com) surveyed 37 jurisdictions in California, including two in Los Angeles County (Long Beach and Lancaster). The study reports average impact fees of \$31,014 per single-family unit and \$18,807 per multi-family unit in California. The City of San Fernando's development impact fees and taxes are well below the statewide average, constitute a relatively minor proportion of total development cost, and, therefore, do not constitute a constraint to the production or improvement of housing.

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Table 29: Residential Development Fees for Prototypical Projects

Type of Fee	Duplex (1,200 SF each)	Condominium Project (37 units, average 1,300 SF each)	Apartment Project (20 Units, average 700 SF each)
Planning Fees			
Site Plan Review	\$1,763	\$1,763	\$1,763
Tentative Tract/Parcel Map	\$0	\$6,151	\$0
Variance	\$0	\$0	\$0
Conditional Use Permit	\$0	\$3,005	\$0
Public Notification	\$0	\$1,120	\$0
Categorical Exemption (infill)	\$204	\$204	\$0
Sub Total	\$1,763	\$12,243	\$1,763
Building Fees			
Building Permit	\$1,125	\$34,500	\$18,911
NPDES	\$0	\$1,000	\$1,000
Demolition	\$0	\$375	\$475
Grading	\$0	\$1,750	\$933
Plan Check	\$956	\$19,029	\$12,874
School Fees (\$3.96 per sq. ft.)	\$9,600	\$192,400	\$56,000
Quimby/Park Development Fees	\$0	\$347,800	\$0
Sub Total	\$11,681	\$596,854	\$90,193
Public Works Fees			
Tract Map Plan check	\$0	\$5,850	\$0
Grading Plan check	\$1,030	\$3,345	\$2,330
Sewer Fees	\$1,798	\$57,868	\$25,020
Water Capital Fees	\$3,289	\$23,646	\$9,841
Water Installation	\$2,847	\$13,718	\$6,471
Fire Hydrant	\$0	\$14,476	\$7,238
Dumpster	\$36	\$36	\$36
Sub Total	\$9,000	\$118,939	\$50,936
Total Project Fees	\$22,444	\$728,036	\$142,892
Fees Per Unit	\$11,222	\$19,677	\$7,145
Estimated Per Unit Development Cost	\$325,000	\$425,000	\$400,000
% of Unit Development Cost	3.5%	4.6%	1.8%

Source: City of San Fernando Planning Division, 2013.

Note: Quimby/park fees only apply to subdivisions and therefore do not apply to typical duplex and apartment developments.

5. Local Processing and Permit Procedures

The evaluation and review process required by City procedures contributes to the cost of housing in that holding costs incurred by developers are ultimately reflected in the unit's selling price. One way to reduce housing costs is to reduce the time for processing permits.

As a small city with limited development, San Fernando does not experience the backlogs in development typical in many larger jurisdictions. In most cases, even when Planning and Preservation Commission or City Council review is required, approval can be obtained in about three months. Small projects, such as single-family units, may receive over-the-counter approval with a simple site plan. Residential Planned Development (RPD) Projects are required to submit a conceptual site plan for preliminary review by the Community Development Department planning staff in order to assist the applicant in developing more precise plans as part of the CUP application to be reviewed by Planning and Preservation Commission. In addition, the San Fernando Corridors Specific Plan provides for multi-family residential development ~~as a permitted use within the SP 4 Zones' Mixed-Use Transition Sub-District, Downtown District, and Maclay District~~ subject to a CUP within the Specific Plan's Downtown District, Mixed-Use Corridor District, General Neighborhood District, and Maclay District. Live-work development is permitted in the aforementioned districts as well as in the Workplace Flex District.

The development review process in the City of San Fernando is governed by three levels of decision-making bodies: the Community Development Department, the Planning and Preservation Commission, and the City Council.

Single-family, multi-family, and mixed-use residential projects are reviewed by the Community Development Department through the Site Plan Review process using applicable development standards, design guidelines, and the City's General Plan. The Community Development Department reviews development proposals for land-use applicability, environmental impacts, aesthetic value, architectural style, and landscape to ensure a quality physical and environmental design. Any required environmental assessment is conducted concurrent with the planning analysis.

San Fernando's citywide Design Guidelines provide clear examples of the quality and type of design that is recommended for all development in the City, including single-family, multi-family, and mixed-use residential projects. The City encourages property owners and design professionals to incorporate the guidelines into the design of a project. The application of design review is processed concurrently with other project entitlements, if any, and does not add time to the review process. Additionally, projects approved with minor modifications related to setbacks, landscaping requirements, parking totals and parking stall dimensions can be checked for compliance by staff and generally do not require subsequent review by the Commission, and/or Council.

The Planning and Preservation Commission reviews all discretionary projects that require an entitlement for a variance, CUP, subdivision, and/or specific plan. The Commission acts both as an advisory body to the City Council as well as a final decision making body.

The typical review process for a single-family residence usually consists of an “over-the-counter” plan check with the Community Development Department in which plans are checked for compliance with applicable codes, development standards, and design guidelines. From there, a property owner brings their plans to the Building & Safety Division for submittal of the plans into plan check. For smaller projects, such as residential remodels and accessory structures, the Building & Safety Supervisor may be able to provide an “over-the-counter” plan check and a property owner would be able to obtain their building permits the same day.

For all new residential projects and some significant residential remodel projects, a property owner will leave their plans with the Building & Safety Division for plan check. Depending on the quality and completeness of plans submitted for plan check, the entire process could take as little as 10 days but may take longer if subsequent plan checks are required. Average processing times for plan check/building permits in the City of San Fernando are comparatively shorter than the neighboring City of Los Angeles.

When a residential development proposes to deviate from applicable codes, a discretionary entitlement such as a variance may be required. The Planning Commission acts on these requests and processing times would be an additional two months longer than the Site Plan Review and plan check times noted above. However, minor accommodations to certain development standards may be accommodated through the Modification process regarding setbacks, parking dimensions/totals, and landscaping requirements. These modifications can significantly reduce processing times and eliminate the requirement for making variance findings and conducting a public hearing.

In summary, San Fernando’s processing and permit procedures are streamlined in comparison to many jurisdictions, and do not serve as a constraint to development.

6. Building Code

San Fernando contracts for services with the Los Angeles City Fire Department, and is hence required to adopt the building codes adopted by the City of Los Angeles. These building codes are based on the 2011 version of the California Building Code, along with all required updates. The California Building Code establishes construction standards necessary to protect public health, safety and welfare, and the local enforcement of these codes does not unduly constrain development of housing.

The building codes have been amended since the Northridge Earthquake (1994) to include additional seismic safety provisions for residences (e.g. a requirement for shear walls). Although this adds to the initial cost of housing, it is considered necessary to the health and safety of the residents, and also makes homeowners’ insurance easier to obtain.

The City of Los Angeles adopted the International Building Code (IBC), as required of all jurisdictions in California. San Fernando has in turn adopted the 2011 edition of the City of Los Angeles' building codes with amendments to the administrative section of the code to meet the City's requirements. While the IBC will bring California building codes into consistency with the rest of the country, changes from the prior State Building Code are anticipated to moderately increase the costs of development in the San Fernando.

In terms of the local enforcement of the City's Building Code, San Fernando maintains an annual Apartment Inspection Program as a means of maintaining the habitable condition of rental units. Through the CAPP program, the City implements targeted code enforcement in conjunction with comprehensive neighborhood revitalization activities. The City conducts the sensitive enforcement of its residential code by providing multiple written notices of code violations to property owners and allowing sufficient time for compliance. Code violation cases are also referred to the City's Planning and Building and Safety Divisions in order to facilitate correction of building code violations.

B. MARKET CONSTRAINTS

1. Availability of Financing

The availability of financing in a community depends on a number of factors, including the type of lending institutions active in the community and the lending practices of those institutions. Through analysis of Home Mortgage Disclosure Act (HMDA) data on the disposition of residential loan applications, an assessment can be made of the availability of residential financing within a community.

Residential lending activity in San Fernando has declined sharply since preparation of the 2008-2014 Housing Element. The number of applications for conventional home purchase loans declined from 745 in 2005 to 126 in 2011, reflecting the significant changes to mortgage lending practices since the housing market crash of 2008. ~~Table 30~~Table 30 provides information on the status of home purchase, refinance and home improvement loan applications in both San Fernando and Los Angeles County as a whole. As illustrated by this table, at 74 percent, the home loan approval rates in San Fernando were slightly above the 71 percent approval rate countywide. Denial rates were identical within the San Fernando and countywide (15 percent), but the City's fallout rate was lower than the county's (11 percent vs. 14 percent). The City's approval rate for refinance loans was slightly lower compared to the countywide approval rate for these types of loans (60 percent vs. 65 percent). Only 14 households applied for home improvement loans in 2011.

Table 30: Mortgage Lending - San Fernando and Los Angeles County (2011)

Loan Type	Loans Approved		Loans Denied		Loans Withdrawn/Incomplete	
	San Fernando	L.A. County	San Fernando	L.A. County	San Fernando	L.A. County
Home Mortgage # Applications	126	63,039	26	13,157	18	12,620
% of Total Home Mortgages	74%	71%	15%	15%	11%	14%
Refinance # Applications	185	148,867	76	44,492	46	35,320
% of Total Refinance	60%	65%	25%	20%	15%	15%
Home Improvement # Applications	6	6,252	6	3,217	2	2,008
% of Total Home Improvement	43%	55%	43%	28%	14%	18%

Source: Home Mortgage Disclosure Act Data, 2011.

Note: Approved loans include: loans originated and applications approved but not accepted.

2. Price of Land

The availability and price of land represents a significant market constraint to housing production throughout most of southern California. The former San Fernando Redevelopment Agency played an important role in both land assembly and write-down in support of affordable housing. In June 2010, the former Redevelopment Agency obtained an appraisal for a 0.34-acre parcel located at 1422 San Fernando Road within the Mixed-Use ~~Transition Sub-Corridor~~ District of the San Fernando Corridors Specific Plan. The appraisal, which took into account comparable commercial properties sold or listed for sale between December 2009 and June 2010, established an appraised value of \$900,000, or \$60 per square foot of land. Pursuant to the San Fernando Corridors Specific Plan, the City -intends to facilitate redevelopment within the San Fernando Downtown on surface parking lots that would provide for mixed-use projects with new residential units, new public parking facilities, and commercial floor space.

Although construction and home purchase financing markets have constricted in recent years, resulting in reduced demand for new residential construction, the cost of raw, developable land still comprises a significant portion of overall housing costs. For this reason, developers often seek City approval for the largest number of units allowed on a given parcel. This allows the developer to distribute land and site development costs over a larger number of units to be sold or rented.

3. Development Costs

A major cost associated with the development of housing is the cost of building materials, labor, and site preparation. The City reviewed the proforma for a recently-approved affordable housing development to obtain an estimate of current development costs. The total development cost for the 20-unit Mid-Celis Apartments, inclusive of permit processing, inspections, site preparation, construction, and labor, was \$412,172 per unit.

A reduction in amenities and the quality of building materials (above a minimum acceptability for health, safety, and adequate performance) can result in lower development costs. As part of the density bonus ordinance, the City could allow for affordable units to be smaller in size (maintaining the same number of bedrooms) and have different features and interior finishes than market rate units, provided all project units were comparable in construction quality and exterior design. Another factor related to construction costs is the number of units built at one time. As that number increases, overall costs generally decrease as builders are able to take advantage of the benefits of economies of scale.

C. ENVIRONMENTAL AND INFRASTRUCTURE CONSTRAINTS

1. Environmental Hazards

The primary environmental hazard in San Fernando impacting development is earthquake risk. An Alquist-Priolo Special Studies Zone has been designated in the western portion of the City around the San Fernando Fault, requiring special building considerations. Detailed geologic investigations are required prior to development within the Special Studies Zone to confirm the absence any active faults or fault branches. In addition to the San Fernando Fault, the potentially active Verdugo Fault is located directly south of the City's boundaries. The Verdugo Fault is within a secondary zone where similar studies are required by the City for all critical facilities, and high rise or complex buildings.

Portions of San Fernando would be subject to inundation if water breached the Pacoima Dam. The City adopted a Pacoima Dam Evacuation Contingency Plan in 1985, establishing procedures for the efficient evacuation of all people for potential inundation areas.

2. Public Services and Facilities

As a completely urbanized community, the City of San Fernando already has in place all of the necessary infrastructure to support future development. All land designated for residential use is served by sewer and water lines, streets, storm drains, telephone, electrical, and gas lines. However, as an older community, much of the City's infrastructure is aging and will require improvements or replacement over time. The City is replacing the water conveyance system on an ongoing basis so that the existing capacity will be maintained.

Water and Sewer Service

The City of San Fernando Water Production Division is responsible for the operation and maintenance of the City's four water wells, three booster pump stations, four reservoirs, and two pressure regulation stations. Local water supplies are drawn from the City's wells located in the Sylmar basin. This groundwater basin has been adjudicated, and the City of San Fernando is limited in the amount of draw that it can safely extract from the basin annually, as determined by a court appointed Watermaster. The City's current allotted draw from the basin is 3,405 acre-feet per year, which represents approximately 95 percent of the City's total water demand. The remaining five percent is in the form of imported water purchased from the Metropolitan Water District (MWD), which supplements the local ground water supplies. The current storage capacity for potable water in the City of San Fernando is approximately 8.9 million gallons, with peak day demand of approximately 7.3 million gallons, or 82 percent of total storage capacity, as of 2008.

Sewer lines in San Fernando are maintained by the City's Public Works Department, Sewer Maintenance Division. The treatment and disposal of effluent is currently being provided under contract with the City of Los Angeles at the Donald C. Tillman Water Reclamation Plant located in Van Nuys (in the San Fernando Valley's Sepulveda Basin). The Tillman Reclamation Plant has a current treatment capacity of 80 million gallons per day (mgd).

The Environmental Impact Report (EIR) prepared for the City's Downtown Developments project in 2008 concluded that water supply, water storage, and wastewater treatment facilities had adequate capacity to accommodate 261 to 272 new dwelling units and between 55,000 and 64,000 square feet of additional commercial space without mitigation measures. Because the City's residential sites inventory relies on existing land use designations and zoning, it can be assumed that existing water supply, water storage, and wastewater treatment facilities have adequate capacity to accommodate the City's RHNA of 217 dwelling units over the 2014-2021 RHNA period.

Senate Bill 1087 (enacted 2006) requires that water and sewer service providers develop written policies that grant priority to proposed development that includes housing affordable to lower income households. The legislation also prohibits water and sewer providers from denying or conditioning the approval of development that includes housing affordable to lower-income households, unless specific written findings are made. The City will provide a copy of the adopted Housing Element to the MWD and City of Los Angeles within 30 days of adoption. The City will also continue to coordinate with the MWD and City of Los Angeles to ensure affordable housing developments receive priority water and sewer service provision.

IV. HOUSING OPPORTUNITIES

The following section presents the resources available for the development, rehabilitation and preservation of housing in San Fernando. The section begins with an overview of the availability of residential sites for future housing development and the adequacy of these sites to address the City's identified share of future housing needs. This section also presents the financial resources available to support in the provision of affordable housing in the community. The final part of the section is an overview of energy conservation and green building resources available to the City and its residents.

A. LAND RESOURCES

1. Future Housing Needs (2014-2021)

In accordance with Government Code Section 65584, projected housing needs for each city and county in the Southern California region are prepared by the Southern California Association of Governments (SCAG) under a process known as the Regional Housing Needs Assessment (RHNA). SCAG's Regional Council adopted the final RHNA in October 2012. The RHNA for the fifth cycle of the Housing Element update covers the period of January 1, 2014 to October 31, 2021.

The RHNA process began with an update of the population, employment and household forecasts for both the region as a whole and for each county. These forecasts were largely derived from Department of Finance (DOF) population and employment forecasts and modified by regional demographic and modeling efforts by SCAG. SCAG then disaggregated the regional and county forecasts to each jurisdiction and estimated the number of dwelling units needed to achieve a regional target vacancy rates and to account for projected housing demolitions. Finally, SCAG distributed the total housing needed in each jurisdiction by income category (very low, low, moderate and upper income).

To avoid the over concentration of new lower-income households in jurisdictions with higher proportions of existing lower-income households (as required by State law), SCAG adjusted the percentage of households in each income category for each jurisdiction.

In October 2012, SCAG adopted the final RHNA growth needs for each of Los Angeles County's cities plus the unincorporated area. The total housing growth need for the City of San Fernando during the 2014-2021 RHNA planning period is 217 units. This total is distributed by income category as shown in [Table 31](#)~~Table 31~~. While not explicitly addressed in the RHNA, State law [Government Code Section 65583(a)(1)] now requires the Housing Element to estimate the future needs of extremely low income households. As provided by State law, the extremely low income need is estimated to be 50 percent of the very low income category, or 28 units during this planning period.

Table 31: Regional Housing Needs Allocation 2014-2021

Very Low*	Low	Moderate	Above Moderate	Total
55	32	35	95	217
25.3%	14.8%	16.1%	43.8%	100.0%

Source: SCAG 2012

*Includes extremely-low households, estimated to be one-half the very-low need (28 units)

2. Future Housing Potential

State law mandates that a jurisdiction provide sufficient land to accommodate a variety of housing opportunities for all economic segments of the community. California Government Code Section 65583(a)(3) requires housing elements to contain an “inventory of land suitable for residential development, including vacant sites and sites having potential for redevelopment, and an analysis of the relationship of zoning and public facilities and services to these sites.” Compliance with this requirement is measured by the jurisdiction’s ability in providing adequate land with adequate density and appropriate development standards to accommodate the RHNA.

The City of San Fernando has adequate land capacity to meet the needs of all income groups through the 2014-2021 RHNA period. The City intends to accommodate the RHNA on sites that are under construction but will be occupied after January 1, 2014, can accommodate ~~second~~accessory dwelling units, or can be developed or redeveloped with multi-family residential uses.

Projects Approved

In 2012, the City approved entitlements for two multi-family developments. To facilitate these projects, the City supported developer applications for HOME funds and approved density bonuses and rezones from industrial to multi-family residential. The Fermore Apartments at 1501 and 1529 First Street (APNs: 2520-011-038, -041, and -042) will include 84 moderate income units. The Harding Apartments at 112, 116, and 124 Harding Avenue (APNs: 2520-017-002, -003, and -004) will include 29 very low income units. Both projects are currently seeking tax credits and gap financing, and are anticipated to be constructed and occupied after January 1, 2014.

~~Second~~Accessory Dwelling Units

San Fernando has facilitated infill development within its single-family residential neighborhoods through provisions for ~~second~~accessory dwelling units. As previously discussed in Section III, the City’s accessory~~second~~-dwelling unit regulations include: a ministerial approval process; allowance for both attached and detached units; unit sizes up to 640 square feet for detached units and 30 percent of the single-family unit size for attached units; and parking of one space per bedroom. As a means of facilitating and encouraging

~~accessorysecond~~ units, the City also provides for modification of ~~accessory second~~-dwelling unit standards through a conditional use permit process, including reduction in parking, increased ~~accessory second~~-dwelling unit square footage, and/or reduced building setbacks.

Since adoption of the ~~accessory second~~-dwelling unit ordinance in 2004, San Fernando has received 64 site plan review applications, or an average of eight applications per year. While the City has not denied any of these applications, nine were withdrawn by the applicants, resulting in issuance of building permits for 55 ~~accessorysecond~~ units. Given an average historical issuance of nearly seven ~~accessorysecond~~ unit building permits per year since 2004, it is assumed that 55 additional ~~accessorysecond~~ dwelling units could be realistically developed during the 2014-2021 RHNA period.

~~AccessorySecond~~ dwelling units are considered an important source of “below market” housing pursuant to Government Code Section 65852.150. Moderate income households can afford a wide range of market-rate rental housing product in San Fernando (refer to Table 17~~Table 17~~ and Table 20~~Table 20~~); therefore, ~~accessorysecond~~ dwelling units are considered to be a suitable “below market” housing option for lower income households.

Residential Sites Inventory

In addition to the units under construction and anticipated ~~accessorysecond~~ dwelling units, Table 32~~Table 32~~ and Figure 3~~Figure 3~~ identifies 25 residential sites that consist of 62 parcels and 14.8 acres of land to accommodate the 2014-2021 RHNA. The sites are either undeveloped or contain minimal improvements in poor condition. The City owns 21 parcels (7.0 acres) and the County owns another six (1.5 acres). The remaining 35 parcels (6.3 acres) in the inventory have private owners. Most of the parcels are located in the San Fernando Corridors Specific Plan area (SP-4 land use designation and zone) and can accommodate mixed use development at up to 4350 units per acre. The other parcels are designated High Density Residential (HDR) and zoned R-3, which allow up to 43 units per acre.

All of the sites are suitable for lower income housing because the maximum allowable density is greater than 20 du/ac; the default lower income density for San Fernando per Government Code Section 65583.2(c)(3)(B). However, based on past development experience and local market conditions, the City has determined that sites larger than 1.25 acres are the most suitable to accommodate lower income housing. Sites that are smaller 1.25 acres in size but larger than 0.75 acre could potentially accommodate lower income housing; however, moderate income housing is most likely to occur on these sites. For the purposes of this analysis, sites that are smaller than 0.75 acre are assumed to be adequate to accommodate above moderate income housing.

Table 32: Residential Sites Inventory

Site #	APN	Owner	Parcel SF	General Plan/ Zone	Site SF	Site Acres	Density Factor	Realistic Capacity	Existing Uses/ Reason Why Selected
Lower Income									
1	2521016003	Private	5,000	SP-4 SP-5 / SP-5 SP-4	100,100	2.3	37	85	Contiguous parcels with consolidation potential. Four parcels owned by a single private owner. The largest parcel is owned by the City. Improvements are limited to parking lots and an unoccupied one-story, 2,500 SF building in poor condition constructed in 1948.
	2521016011		2,511						
	2521016012		2,489						
	2521016020	30,006							
	2521016018	Private	20,040						
	2521016900	City	40,055						
2	2522001001	Private	14,925	SP-5 / SP-5 SP-4 / SP-4	64,893	1.5	37	55	Contiguous parcels with consolidation potential. The privately-owned parcel is occupied by a one-story 7,000 SF operating auto repair retail building in fair condition constructed in 1938. The City-owned parcels are parking lots.
	2522001901	City	29,978						
	2522001902		19,990						
3	2519006904	County	8,412	SP-5 / SP-5 SP-4 / SP-4	65,002	1.5	37	30	Parking lot on contiguous county-owned parcels.
	2519006908		12,612						
	2519006909		14,035						
	2519006905		14,984	HDR / R-3			32	22	
	2519006906		7,497						
	2519006907		7,462						
4	2522004904	City	43,071	SP-5 /	59,449	1.4	37	50	Parking lot on

Table 32: Residential Sites Inventory

Site #	APN	Owner	Parcel SF	General Plan/ Zone	Site SF	Site Acres	Density Factor	Realistic Capacity	Existing Uses/ Reason Why Selected	
	2522004905		1,513	SP-5 SP-4 SP-4					contiguous City-owned parcels.	
	2522014900		6,637							
	2522014901		8,228							
5	2521031902	City	23,401	SP-5 / SP-5 SP-4 SP-4	55,847	1.3	37	47	Parking lot on contiguous City-owned parcels.	
	2521031903		9,015							
	2521031901		23,431							
Moderate Income										
6	2521034007	Private	9,705	SP-5 / SP-5 SP-4 SP-4	50,159	1.2	37	43	Contiguous parcels with consolidation potential. The two City-owned parcels and one of the privately-owned parcels are occupied by a parking lot. The other privately-owned lot is undeveloped.	
	2521034009		15,395							
	2521034904	18,405	City							6,654
	2521034905									
7	2519001903	City	33,599	SP-5 / SP-5 SP-4 SP-4	33,599	0.8	37	28	Parking lot.	
Above Moderate Income										
8	2520019016	Private	32,353	SP-5 / SP-5 SP-4 SP-4	32,353	0.7	37	27	Undeveloped lot.	
9	2522003021	Private	5,600	SP-5 / SP-5 SP-4 SP-4	25,135	0.6	37	21	Contiguous parcels with consolidation potential. Of the parcels that make up this site, six are City-owned parking lots and the seventh is privately-owned and developed with a 5,000 SF single-story unoccupied building in poor condition constructed in 1946 with a	
	2522003901	City	3,251							
	2522003902		2,326							
	2522003903		2,789							
	2522003900		2,787							
	2522003904		5,579							
	2522003905		2,803							

Table 32: Residential Sites Inventory

Site #	APN	Owner	Parcel SF	General Plan/ Zone	Site SF	Site Acres	Density Factor	Realistic Capacity	Existing Uses/ Reason Why Selected
									"For Rent" sign posted on the facade.
10	2520022014	Private	7,048	HDR / R-3	21,172	0.5	32	15	Parking lot on contiguous parcels with a single private owner.
	2520022016		14,124						
11	2520024003	Private	3,557	SP-5 / SP-5HDR / R-3	21,278	0.5	32	10	Contiguous parcels with consolidation potential. Two undeveloped parcels have the same private owner. The other two parcels are occupied by parking lots.
	2520024004	Private	3,559						
	2520024005	Private	7,117						
	2520024011	Private	7,045						
12	2521003001	Private	10,018	SP-5 / SP-5SP-4 / SP-4	19,993	0.5	37	16	Contiguous undeveloped parcels with consolidation potential. Four of the parcels are owned by a single private owner.
	2521003022	Private	2,501						
	2521003023		2,501						
	2521003024		2,500						
	2521003025		2,474						
13	2521003006	Private	4,997	SP-5 / SP-5SP-4 / SP-4	19,962	0.5	37	16	Contiguous undeveloped parcels with consolidation potential. The largest parcel is City-owned.
	2521003900	City	14,965						
14	2515028016	Private	15,041	SP-5 / SP-5SP-4 / SP-4	15,041	0.4	37	12	Undeveloped lot.
15	2519020015	Private	11,032	HDR / R-3	11,032	0.3	32	8	Undeveloped lot.
16	2521034901	City	7,156	SP-5 / SP-5SP-4 / SP-4	7,156	0.2	37	6	City-owned parking lot.
17	2518009026	Private	6,868	SP-5 / SP-5SP-4 / SP-4	6,868	0.2	37	6	Undeveloped lot.
18	2515014010	Private	6,646	SP-5 / SP-5SP-4 / SP-4	6,646	0.2	37	5	Undeveloped lot.
19	2521002010	Private	3,038	SP-5 /	6,058	0.1	37	5	Contiguous

Table 32: Residential Sites Inventory

Site #	APN	Owner	Parcel SF	General Plan/ Zone	Site SF	Site Acres	Density Factor	Realistic Capacity	Existing Uses/ Reason Why Selected
	2521002011		3,020	SP-5 SP-4/ SP-4					undeveloped parcels with the same private owner.
20	2521031012	Private	5,250	SP-5 / SP-5 SP-4/ SP-4	5,250	0.1	37	4	Undeveloped lot.
21	2516027003	Private	2,511	SP-5 / SP-5 SP-4/ SP-4	5,012	0.1	37	4	Contiguous undeveloped parcels with the same private owner.
	2516027004		2,502						
22	2520020019	Private	4,581	HDR / R-3	4,581	0.1	32	3	Undeveloped lot.
23	2515016015	Private	3,616	SP-5 / SP-5 SP-4/ SP-4	3,616	0.1	37	3	Undeveloped lot.
24	2519009003	Private	3,336	SP-5 / SP-5 SP-4/ SP-4	3,336	0.1	37	3	Undeveloped lot.
25	2519016047	Private	2,773	HDR / R-3	2,773	0.1	32	2	Undeveloped lot.

Figure 3: Residential Land Inventory

3. Adequacy of Land Inventory to Accommodate RHNA

When the City's land inventory, including building permits for units with occupancy anticipated after January 1, 2014, anticipated ~~accessory~~~~second~~ dwelling units, and available residential sites, provides a potential capacity for approximately 694 units. However, taking into account that not all of the mixed use sites will be redeveloped with a residential component, this Housing Element assumes a 50 percent reduction in the capacity for the mixed use properties. This reduction would lower the development potential on the sites inventory to 478 units, adequate to accommodate the City's RHNA for the planning period. Compared to the RHNA, there is a surplus of 153 lower income sites, 85 moderate income sites, and 23 above moderate sites for this planning period (~~Table 33~~~~Table 33~~). As discussed in Section III (Constraints), there are no infrastructure deficiencies that would preclude development of these sites. Section V (Housing Action Plan) contains Program 5 to encourage and facilitate the development of affordable housing units during the current planning period.

Table 33: Land Inventory Summary

	Income Category			Total
	Lower	Mod	Above	
Projects Approved	29	84	0	113
Accessory Second Dwelling Units	55	0	0	55
Sites Inventory	289	71	166	526
<i>Mixed-Use (SP-4)</i>	267	71	96	408
<i>High-Density Residential (R-3)</i>	22	--	70	118
Total Capacity	373	155	166	694
Capacity with 50% Mixed Use	240	120	118	478
RHNA	87	35	95	217
Surplus	+153	+85	+23	+261

In summary, San Fernando has provided more than adequate sites to fulfill its regional housing needs by income category. In addition to providing appropriate zoning and development standards, the City will further encourage and facilitate production of affordable units on these sites through regulatory incentives such as density bonuses and direct financial assistance as funding becomes available.

4. Availability of Infrastructure and Public Services

As a completely urbanized community, the City of San Fernando has already in place all of the necessary infrastructure to support future development. All land designated for residential use is served by sewer and water lines, streets, storm drains, and telephone, electrical, and gas lines. However, as an older community, much of the City's infrastructure is aging and will require improvements or replacement over time. The City is replacing the

water conveyance system on an ongoing basis in conjunction with new development projects so that the existing capacity will be maintained.

SB 1087, effective January 2006, requires water and sewer providers to grant priority for service allocations to proposed developments that include units affordable to lower income households. Pursuant to these statutes, upon adoption of its Housing Element, San Fernando will immediately deliver the Element to local water and sewer providers, along with a summary of its regional housing needs allocation.

B. FINANCIAL RESOURCES

There are a variety of potential funding sources available for housing activities in San Fernando. Due to both the high cost of developing and preserving housing, and limitations on both the amount and uses of funds, a variety of funding sources may be required. The State's dissolution of the San Fernando's Redevelopment Agency in 2012 eliminated the City's primary source of affordable housing funding. Other funding resources available for affordable housing development are highly limited.

Table 34 ~~Table 34~~ lists the potential funding sources that are available for housing activities. They are divided into five categories including: Federal, State, county, local and private resources.

Table 34: Financial Resources Available for Housing Activities

Program Name	Description	Eligible Activities
1. Federal Programs		
Community Development Block Grant (CDBG)	As a participating City in Urban LA County, grants are allocated directly to the City on a formula basis for housing and community development activities primarily benefiting low and moderate income households. San Fernando receives approximately \$350,000 in CDBG funds from LACDC on an annual basis. Currently, the majority of the City's CDBG allocation is being used for Section 108 loan repayments. Funds will become available in 2018 when the loans are repaid.	<ul style="list-style-type: none"> ▪ Acquisition ▪ Rehabilitation ▪ Homebuyer Assistance ▪ Economic Development ▪ Homeless Assistance ▪ Public Services
HOME www.lacdc.org	Funding used to support a variety of County housing programs the City has access to. Funds are used to assist low income (80% AMI) households.	<ul style="list-style-type: none"> ▪ New Construction ▪ Acquisition ▪ Rehabilitation ▪ Homebuyer Assistance ▪ Rental Assistance

Table 34: Financial Resources Available for Housing Activities

Program Name	Description	Eligible Activities
Housing Choice Voucher www.lacdc.org	Rental assistance payments to owners of private market rate units on behalf of low-income (50% AMI) tenants. Administered by the Housing Authority of the County of Los Angeles. An average of 32 San Fernando households have received assistance annually since 2008, with 79 residents on the waiting list as of August 2013.	<ul style="list-style-type: none"> ▪ Rental Assistance
Section 202	Grants to non-profit developers of supportive housing for the elderly.	<ul style="list-style-type: none"> ▪ Acquisition ▪ Rehabilitation ▪ New Construction
Section 811	Grants to non-profit developers of supportive housing for persons with disabilities, including group homes, independent living facilities and intermediate care facilities.	<ul style="list-style-type: none"> ▪ Acquisition ▪ Rehabilitation ▪ New Construction ▪ Rental Assistance
Mortgage Credit Certificate www.lacdc.org	Federal income tax credits (15% mortgage interest) available to low income first-time homebuyers to purchase housing in San Fernando. The County makes certificates available through participating lenders.	<ul style="list-style-type: none"> ▪ Home Buyer Assistance
2. State Programs		
Low-income Housing Tax Credit (LIHTC)	Tax credits are available to persons and corporations that invest in low-income rental housing. Proceeds from the sale are typically used to create housing.	<ul style="list-style-type: none"> ▪ New Construction
Multi-Family Housing Program (MHP) www.hcd.ca.gov/fa/mhp/	Deferred payment loans to local governments, non-profit and for-profit developers for new construction, rehabilitation and preservation of permanent and transitional rental housing for lower income households.	<ul style="list-style-type: none"> ▪ New Construction ▪ Rehabilitation ▪ Preservation ▪ Conversion of nonresidential to rental
Building Equity and Growth in Neighborhoods (BEGIN) www.hcd.ca.gov/fa/begin/	Grants to cities to provide downpayment assistance to low and moderate income first-time homebuyers of new homes in projects with affordability enhanced by local regulatory incentives or barrier reductions.	<ul style="list-style-type: none"> ▪ Homebuyer Assistance
CalHome www.hcd.ca.gov/fa/calhome	Grants to cities and non-profit developers to offer homebuyer assistance, including downpayment assistance, rehabilitation, acquisition/rehabilitation, and homebuyer counseling. Loans to developers for property acquisition, site development, predevelopment and construction period expenses for homeownership projects.	<ul style="list-style-type: none"> ▪ Predevelopment, site development, site acquisition for development projects ▪ Rehabilitation ▪ Acquisitions/rehabilitation ▪ Downpayment assistance ▪ Mortgage financing ▪ Homebuyer counseling
CalHFA Homebuyer's Downpayment Assistance Program www.calhfa.ca.gov/homeownership/programs/chdap.htm	CalHFA makes below market loans to first-time homebuyers of up to 3% of sales price. Program operates through participating lenders who originate loans for CalHFA. Funds available upon request to qualified borrowers.	<ul style="list-style-type: none"> ▪ Homebuyer Assistance

Table 34: Financial Resources Available for Housing Activities

Program Name	Description	Eligible Activities
3. Local Programs		
Los Angeles County Housing Innovation Fund http://www.liifund.org/LA%20Innovation%20Fund/LAInnovationFund.html	LACHIF provides low-cost financing up to \$5,000,000 to support the creation and preservation of affordable housing, including supportive housing projects. Eligible entities include non-profit and for-profit developers, and cities in Los Angeles County.	<ul style="list-style-type: none"> ▪ Predevelopment ▪ Acquisition
Tax Exempt Housing Revenue Bond	The City can support low-income housing by issuing housing mortgage revenue bonds requiring the developer to lease a fixed percentage of the units to low-income families at specified rental rates.	<ul style="list-style-type: none"> ▪ New Construction ▪ Rehabilitation ▪ Acquisition
4. Private Resources/Financing Programs		
Federal National Mortgage Association (Fannie Mae)	Fixed rate mortgages issued by private mortgage insurers.	<ul style="list-style-type: none"> ▪ Home Buyer Assistance
	Mortgages which fund the purchase and rehabilitation of a home.	<ul style="list-style-type: none"> ▪ Home Buyer Assistance ▪ Rehabilitation
	Low Down-Payment Mortgages for Single-Family Homes in under served low-income and minority cities.	<ul style="list-style-type: none"> ▪ Home Buyer Assistance
Federal Home Loan Bank Affordable Housing Program	Direct Subsidies to non-profit and for profit developers and public agencies for affordable low-income ownership and rental projects.	<ul style="list-style-type: none"> ▪ New Construction
5: Housing Successor Agency		
The San Fernando Agency is engaged in ongoing discussions with the State Department of Finance in terms of the amount of redevelopment funds that may be returned to the City. Such funding may be available in the future to support affordable housing activities.		

C. OPPORTUNITIES FOR ENERGY CONSERVATION

Establishment and enforcement of energy and water conservation standards, as well as continuing programs and establishing new programs aimed at efficiency awareness, are key factors in reducing energy and water consumption. Some conservation measures require a higher up-front cost, but result in a net savings over the life of the improvement from reduced energy and/or water consumption. In large part, utility bill reductions through energy and water savings can be realized through the incorporation of energy conserving design features.

1. Active Energy Efficiency Programs

While the City does not directly offer energy-efficient programs, it serves as an information center for the various programs available for area residents. Below is a list of energy efficiency programs offered by various organizations:

Southern California Edison (SCE) Energy Efficiency Programs

The following two programs are offered by SCE to help lower income customers reduce energy costs and control their energy use:

- **California Alternate Rates for Energy (CARE) /Family Electric Rate Assistance (FERA):** These programs provide income-qualified customers with much-needed bill relief.
- **Energy Savings Assistance Program:** This program is designed to assist income-qualified households with the conserving of energy and reducing their monthly electrical costs. SCE may provide free appliances and installation of energy-efficient refrigerators, air conditioners and more, as well as home efficiency solutions like weatherization
- **Energy Assistance Fund (EAF):** SCE and United Way work together to assist customers who are unable to pay their electric bill due to a financial constraint. A maximum of \$100 is available to eligible customers once in a 12-month period. In order to receive this assistance, customers requiring this assistance should
- **Medical Baseline:** Customers with a medical condition that requires electricity-powered life support equipment may be eligible to receive additional baseline allocation. The Baseline program offers an additional year-round baseline allocation of 16.5 kWh per day in addition to a customer's applicable seasonal baseline and the baseline for its region.

Southern California Gas Company Energy Efficiency Programs

The following five programs are available to SoCalGas customers who reside in the City of San Fernando.

- **California Alternate Rates for Energy (CARE):** The CARE program provides eligible SoCalGas customers a 20 percent discount on their monthly gas bill. In addition, new customers who are approved within 90 days of starting new gas service will also receive a \$15 discount on the Service Establishment Charge.
- **Energy Savings Assistance Program:** SoCalGas offers no-cost energy-saving home improvements and furnace repair or replacement services for qualified limited-income renters and homeowners. Available energy-saving services may include attic insulation, door weather-stripping, caulking, low flow shower heads and faucet aerators, water heater blankets, and energy education.
- **Medical Baseline Allowance:** SoCalGas knows that not all customers are alike. Some of them suffer from poor health, which can make it even harder to make ends meet. If someone in the household has a life-threatening illness, is seriously disabled,

or requires more heat in winter due to a serious health condition, the household may qualify for an additional allowance of gas at a lower rate.

- **Home Energy Upgrade Financing (HEUF):** Under this program, customers may qualify for \$2,500 to \$20,000 to purchase and install energy-efficient upgrades. This includes an unsecured financing with terms ranging between one and ten years. Twelve year financing is available for ENERGY STAR® measures, which may also qualify for interest rate discounts. Eligible improvements may include water heater replacement, cooling, windows, ceiling and attic, insulation, and roofing.

2. Building and Site Design Conservation

Conventional building construction, use and demolition along with the manufacturing of building materials have multiple negative impacts on the environment. A rise in environmental consciousness has led to the development of various building and site designs that promote conservation. This includes site design standards associated with the orientation of the building, installation of solar panels, and so forth.

In California, the Green Building Order challenges the State government to demonstrate leadership by becoming a model of energy and resource efficiency at State-owned buildings. This has been achieved through attainment of Leadership in Energy and Environmental Design (LEED) certification. Some local jurisdictions have taken this certification as an opportunity to further promote a green building program.

LEED is an internationally recognized green building certification system that provides different levels of certification in terms of energy efficiency. In summary, it is a third-party verification that a building was designed and built using strategies aimed at improving performance across the following metrics: energy savings, water efficiency, CO₂ emissions reduction, improved indoor environmental quality, and stewardship of resources and sensitivity to their impacts.

There are four levels of progressive certification, based on the total number of points earned within each of the LEED categories noted above, as follows: Certified; Silver; Gold; and Platinum.

A total of six performance measures are utilized to review and promote a whole-building approach to sustainability:

- Sustainable Site Development
- Water Efficiency
- Energy and Atmosphere
- Materials and Resources
- Indoor Environmental Quality

- **Innovation and Design**

While this is a relatively new concept and certification process, the City has several participants in the LEED program, including affordable special needs projects. It is the developer's goal to generate enough energy to power all common areas including the community center and all hallways.

It is the City's intent to facilitate the permitting process for commercial and residential property owners wishing to pursue a LEED certification.

Glazing

Energy efficient window glazing resists heat flow. The strategic placement of such windows can reduce energy consumption for more efficient interior climate control. Glazed windows on south-facing walls allow for passive solar heating by allowing direct sunlight to enter a room and warm the space. Because the windows minimize heat flow, this warmth remains in the building. The sun is higher in the sky during the summer. Therefore, less direct sunlight enters the building during these months than in winter. Also, during winter weather, the glazing minimizes the amount of heat that is transferred directly through the window to the cooler air outside. Typically, avoidance of window placement on the west side of a building will minimize the overheating effects of direct afternoon sun.

Landscaping

Strategically placed vegetation can help regulate the amount of direct sunlight on windows, as well as reduce indirect heating from concrete and other hardscape materials. The incorporation of deciduous trees and vines in landscaping plans along the south and west facing sides of buildings can buffer the heating effects of direct sun light in summer, while allowing winter sun light to warm the building. The use of native or low-water use plants and efficient irrigation, such as drip systems, can minimize water needs for outside landscaping. Automatic irrigation systems that incorporate time clocks with multiple stations can offer options for varying water needs.

Building Design

There are several variables in the design of a building that impacts the energy efficiency of the structure. The building orientation, placement and specification on windows, and design of details, such as exterior overhead structures and roof overhangs, can affect the passive solar performance of a building. These measures reduce the need for energy-consuming heating and cooling system use. The installation of over-head structures such as eaves, arbors, and roof overhangs can reduce the amount of direct sunlight that passes through windows, thus preventing overheating. An arbor directly above a south-facing window can limit solar access in the summer and allow for passive heating in winter when combined with deciduous vines.

Cooling/Heating Systems

There are several energy-saving alternatives to using traditional energy sources for cooling and heating systems that can reduce the cost of housing. Attic ventilation systems allow rising heat to escape the building. This type of system, such as a whole-house fan, can create an air circulation pattern that encourages the movement of cooler air to circulate through a building with the use of traditional energy sources. Solar heating systems for swimming pool facilities reduce energy costs. Hot water solar panels can provide solar-heated domestic water with minimal use of flow restrictors on all hot water faucets and showerheads.

Weatherization Techniques

Weatherization techniques such as insulation, caulking, and weather-stripping can reduce energy use for air-conditioning up to 55 percent and for heating as much as 40 percent. These techniques help to seal a dwelling unit to guard against heat gain in the summer and prevent heat loss in the winter. Other comfortable benefits include noise and dust reduction.

Efficient Use of Appliances

Most households contain a variety of appliances. Regardless of the types present, appliances can be used in ways that increase their energy efficiency. Elimination of unnecessary and/or older appliances and proper maintenance and use of the stoves, ovens, clothes dryers, clothes washers, dishwashers, refrigerators, and other major appliances will keep energy costs to a minimum.

Efficient Use of Lighting

Costs of lighting a home can be reduced through the purchase of efficient light bulbs that produce the most lumens per watt. New fluorescent bulb fixtures can greatly improve lighting levels while reducing energy costs. Compact fluorescent bulbs replace existing incandescent bulbs in average fixtures. These compact fluorescent bulbs are 10 times more efficient and last longer than regular incandescent bulbs. Time clocks, photocell sensors, and motion sensors for security lights and areas where lights might be left on otherwise can make a significant reduction in lighting usage.

Load Management

The time and day when power is used can be as important as how much power is used. Power plants must have enough generating capacity to meet the highest level of consumer demand for electricity. Peak demands for electricity occur on summer afternoons and coincide with higher costs for electric generation. Therefore, reduction use of appliances during these peak load hours can reduce the need for new power plants just to meet unusually high power demands and will reduce overall energy costs.

V. HOUSING PLAN

Sections II, III, and IV of the Housing Element establish the housing needs, opportunities and constraints in San Fernando. This Plan sets forth the City's goals, policies and programs to address identified housing needs.

A. GOALS, POLICIES AND PROGRAMS

This section of the Housing Element sets forth the goals, policies and programs the City intends to implement in order to address housing needs, and constitutes San Fernando's Housing Plan. Housing programs include both programs currently in operation in the City, as well as new programs developed in response to the analysis of housing needs, constraints, and opportunities. The Housing Program Implementation ~~Table 35~~ ~~Table 35~~ located at the end of this section summarizes the 2013-2021 goals for each program, as well as program funding sources and time frame for implementation. ~~Table 36~~ ~~Table 36~~ summarizes San Fernando's quantified objectives for new construction, rehabilitation and preservation for the 2013-2021 planning period.

The City's housing goals are organized around the following issue areas:

- Housing and Neighborhood Conditions
- Development of New Affordable Housing
- Tenant Assistance
- Homeownership

GOAL 1.0: Maintain and Enhance the Quality of Existing Housing, Neighborhoods, and Health of Residents

Policy 1.1: Support healthy neighborhoods by addressing public health and safety issues, performing property inspections, and eliminating threats to public health.

Policy 1.2: Preserve the character, scale, and quality of established residential neighborhoods.

Policy 1.3: Work in conjunction with residents to revitalize neighborhoods by supporting neighborhood organizations, reducing crime, improving deteriorated housing, managing traffic and parking, and eliminating blighting conditions.

Policy 1.4: Promote the rehabilitation of residential structures that are substandard or in disrepair and general maintenance of the housing stock.

Policy 1.5: Provide focused code enforcement and rehabilitation efforts in targeted neighborhoods to achieve substantive neighborhood improvements and

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preserve community character. Address household overcrowding and illegal conversion of garages and patios, and subdivision of single-family units through pro-active code enforcement efforts, combined with information on provision of legal accessory ~~second~~-dwelling units.

Policy 1.6: Maintain the quality of life within neighborhoods by providing adequate maintenance to streets, sidewalks and alleys, parks, and other public facilities.

Policy 1.7: Promote the preservation and rehabilitation of identified historic residential structures/sites that are substandard or in disrepair.

Implementing Programs

1. Residential Rehabilitation Program: The City's Single-family Rehabilitation Loan Program for lower and moderate income (up to 120 percent of AMI) residential property owners has historically provided up to \$50,000 per loan to perform major rehabilitation, general property repairs, seismic retrofit, and code deficiency repairs. Once properties are brought up to code, funds may also be used for bedroom additions to address household overcrowding. As part of the rehabilitation program, the City has also offered Single-family Emergency Rehabilitation Grants. These are for emergency repairs for health and safety related issues reserved for lower income (80 percent of AMI) households. As the primary focus of the City's rehabilitation program is the correction of building code violations, there is close coordination between the City's code enforcement activities and rehabilitation programs.

The State's dissolution of the Redevelopment Agency effectively crippled the City's ability to continue the Residential Rehabilitation Program. The City will allocate a portion of the "boomerang" funds allowed through the Department of Finance meet-and-confer process to the continuation of this program. The City will also seek to establish partnerships with other public agencies and non-profit organizations to obtain funding for residential rehabilitation activities during the planning period.

2013-2021 Objectives:

- Subject to available funding, assist 20 households during the planning period.
- Seek partnerships with public agencies and non-profit organizations that provide rehabilitation assistance. Support affordable housing providers in their funding applications for acquisition/rehabilitation activities, such as providing letter of support or consistency finding with the City's General Plan.
- Promote energy efficiency programs offered by utility companies on City website and public counters; in 2014, add links to websites of utility companies and update annually.
- Annually research State and Federal funds available for housing rehabilitation assistance and pursue funding if feasible and appropriate.

2. Neighborhood Preservation and Revitalization Program (CAPP): In mid-2006, the City initiated a new program entitled Community Action Plan for Neighborhood Protection and Preservation (CAPP). CAPP utilizes a comprehensive approach to identifying and abating illegal activity, nuisance behaviors, and substandard physical conditions at individual problem properties. CAPP involves the designation of neighborhood focus areas and outreach to residents adversely impacted by nuisance properties within these areas (refer to [Figure 1](#) in the Needs Assessment for the current CAPP Focus Areas).

The City conducts the sensitive enforcement of its residential codes by conducting outreach and education on property maintenance issues, providing multiple written notices of code violations to property owners, and allowing sufficient time for compliance. Illegal dwelling units are frequently encountered as part of CAPP code enforcement activities. Code enforcement staff continues to work with applicants to obtain proper permits to address overcrowding problems and ensure that the construction and occupancy of [accessory second](#) dwelling units is legal and safe. Code violation cases are directly referred to the City's Planning and Building and Safety Divisions.

2013-2021 Objectives:

- Continue to implement CAPP within designated Focus Areas.
- Annually report to City Council on the status of the program.

3. Housing Inspection Programs: The City has initiated a multi-family residential inspection program, allocating the City's Building and Safety Supervisor and Community Preservation staff time for program implementation. The program is focused on improving the overall quality of the housing stock, and focuses on apartments with three or more units, encompassing over 1,000 of the City's housing units. The inspection program utilizes HUD's Housing Quality Standards (HQS) as the basis for evaluating housing conditions. Similar to CAPP, identified code violations under the Apartment Inspection Program are directly referred to the City's housing rehabilitation program coordinator. In addition, the City requires inspection of an ownership housing unit dwelling prior to re-sale.

2013-2021 Objectives:

- Complete inspections of approximately 200 apartment units on an annual basis.
- Complete inspection of ownership housing units prior to resale.
- Conduct outreach and education efforts on the City's housing inspection programs annually.
- Provide City Council with annual program status reports.

4. Lead Based Paint Awareness: San Fernando is one of 10 cities selected by the Los Angeles County Childhood Lead Poisoning Prevention Program (CLPPB) for primary prevention activities. Community outreach about lead poisoning will be programmed in

conjunction with the CLPPB and Pacoima Beautiful, including grant applications and seeking other funding sources.

2013-2021 Objectives:

- Continue to remediate lead cases through the City's residential rehabilitation programs.
- Annually coordinate with LA County and Pacoima Beautiful regarding funding and programs.
- Host or conduct a lead based paint seminar every other year during the planning period, subject to available funding.

GOAL 2.0: Provide a Range of Housing Types to Meet Community Needs

Policy 2.1: Provide adequate housing sites to facilitate the development of a range of residential development types in San Fernando that fulfill regional housing needs. Assist residential developers in identifying sites through dissemination of the sites inventory.

Policy 2.2: Provide opportunities for mixed use and infill housing development in the City's Corridor Specific Plan Areas as part of the City's overall revitalization strategy.

Policy 2.3: Provide affordable housing opportunities for San Fernando's lower income population, including extremely low income households, and households with special needs (such as seniors and persons with disabilities, including persons with developmental disabilities).

Policy 2.4: Encourage developers to include rental housing that is large enough to accommodate large households and provide zoning incentives, such as through the density bonus ordinance, to facilitate family housing development.

Policy 2.5: Utilize zoning tools, including density bonus, to provide affordable units within market rate developments.

Policy 2.6: Facilitate infill development on small parcels by allowing for modified development standards where multi-family projects include the preservation of an existing historic property. Provide property tax incentives for maintaining historic residences.

Policy 2.7: Support collaborative partnerships with non-profit organizations and for-profit developers to provide greater access to affordable housing funds.

Policy 2.8: Promote the creation of s accessory ~~second~~-dwelling units within residential neighborhoods as a means of providing additional rental housing and addressing household overcrowding.

Policy 2.9: Encourage use of sustainable and green building features in new and existing housing.

Implementing Programs

5. Facilitate Affordable and Special Needs Housing Development: Affordable and special needs housing developments face a number of hurdles, including financing, development codes and standards, and in some cases, public opposition. The City can encourage and facilitate affordable and special needs housing through financial assistance, removal of regulatory constraints, and administrative support. With limited funding, the City will rely on the following actions to encourage affordable and special needs housing production during the planning period:

- *Collaborate with Affordable Housing Developers:* Affordable and special needs housing developers work to develop, conserve and promote rental and ownership affordable housing. Particularly in relation to senior housing and housing for persons with disabilities (including persons with developmental disabilities), the developer is often, but not always, a local organization interested in developing affordable housing. The affordable and special needs housing developer is often involved with what is called "assisted housing", where some type of government assistance (tax-exempt bonds or tax credits) is provided to keep rents affordable. An affordable or special needs housing developer can help meet the goals for additional housing by implementing or assisting with the implementation of programs described in this Housing Element. The City will continue to collaborate with affordable and special needs housing developers to identify potential sites, write letters of support to help secure governmental and private-sector funding, and offer technical assistance related to the application of State density bonus provisions.
- *Regulatory Concessions and Incentives:* The City will continue to work with developers on a case-by-case basis to provide regulatory concessions and incentives to assist with the development of affordable and senior housing. In a relatively small city like San Fernando, this is the most effective method of assisting developers, as each individual project can be analyzed to determine which concessions and incentives would be the most beneficial to the project's feasibility. State-mandated regulatory concessions and incentives could include, but are not limited to, density bonuses, parking reductions, fee reductions or deferral, expedited permit processing, and modified or waived development standards. Any requested concessions or incentives would be evaluated on a case-by-case basis while simultaneously working to ensure the project is compatible with the surrounding neighborhood.
- *Transit-Oriented Development:* Much of the City's future residential development potential is located within the San Fernando Corridors Specific Plan area. To facilitate development in the Corridors Specific Plan area, the City will be working to

establish Transit-Oriented Development (TOD) standards in the vicinity of the Sylmar-San Fernando Metrolink Station, located along First Street and North Hubbard Avenue. In 2013, the City received a planning grant to develop a TOD overlay zone in the vicinity of the Metrolink Station that would facilitate the development of additional housing stock. This planning effort will establish appropriate development standards for TOD projects and develop incentives for affordable housing and housing for persons with special needs (such as seniors, persons with disabilities, including developmental disabilities). The City will also pursue funding from State and Federal programs to make infrastructure improvements in the area.

- *New Funding Sources:* Dissolution of the City's Redevelopment Agency by the State in 2012 eliminated the Low and Moderate Income Housing Fund (LMIHF), formerly the City's primary mechanism for providing direct funding support of affordable and special needs housing development. The City will actively pursue County, State, Federal and private funding sources as a means of leveraging local funds and maximizing assistance, with a goal of securing at least three new funding sources.

2013-2021 Objectives:

- On an ongoing basis, maintain contact information for affordable and special needs housing developers for the purposes of soliciting their involvement in development projects in San Fernando.
- Participate with affordable and special needs housing developers to review available Federal and State financing subsidies and apply as feasible on an annual basis.
- On an ongoing basis, assist and support developers of housing for lower income households, especially housing for extremely low income households and persons with special needs (such as seniors, large families, persons with disabilities, including persons with developmental disabilities), with site identification, supporting applications, conducting pre-application meetings, assisting with design and site requirements, and providing State-mandated regulatory incentives and concessions.
- Collaborate with developers of affordable and special needs housing over the planning period to facilitate the construction of 195 affordable units over the planning period (10 extremely low income, 30 very low income, 55 low income, and 100 moderate income units).
- Complete TOD overlay in 2016 with incentives for affordable housing and housing for persons with special needs (including persons with disabilities/developmental disabilities).

6. Conservation of Existing and Future Affordable Units: The City's former Redevelopment Agency assisted in the development of 172 deed-restricted affordable lower and moderate income units within seven different multifamily rental housing developments

since 1996 (see [Table 21](#)~~Table 21~~). None of these projects are at risk of converting to market rents by 2023. Another 113 affordable units within two developments are slated for construction during the 2013-2021 planning period.

2013-2021 Objectives: Monitor the status of the existing and future affordable rental housing stock in San Fernando. The City will work with property owners, interest groups and the State and Federal governments to implement the following actions on an ongoing basis to conserve its affordable housing stock:

- *Monitor Units:* On an ongoing basis maintain contact with providers and owners to monitor the status of existing and future affordable units.
- *Work with Potential Purchasers:* If units are discovered to be at risk of converting to market rate during the planning period, where feasible, provide technical assistance to public and non-profit agencies interested in purchasing and/or managing the at-risk units.
- *Tenant Education:* The California legislature extended the noticing requirement of at-risk units opting out of lower income use restrictions to one year. Should a property owner pursue conversion of the units to market rate, the City will ensure that tenants were properly noticed and informed of their rights.

7. Monitor Residential Capacity: City staff will monitor the consumption of residential acreage to ensure an adequate inventory is available to meet the City's RHNA obligations. To ensure sufficient residential capacity is maintained to accommodate the RHNA, the City will develop and implement a formal ongoing (project-by-project) evaluation procedure pursuant to Government Code Section 65863. The City's development application tracking software will notify staff when an application has been submitted for development of a property included in the residential sites inventory ([Table 32](#)~~Table 32~~). Should an approval of development result in a reduction of capacity below the residential capacity needed to accommodate the remaining need for lower income households, the City will identify and, if necessary, rezone sufficient sites to accommodate the shortfall and ensure "no net loss" in capacity to accommodate the RHNA.

2013-2021 Objective:

- Develop and implement a formal evaluation procedure pursuant to Government Code Section 65863 by January 1, 2015.
- Monitor the City's continued ability to meet its RHNA as part of the City's annual report to HCD on Housing Element implementation.

8. Removal of Governmental Constraints: State law requires that Housing Elements address, and where appropriate and legally possible, remove governmental constraints to the maintenance, improvement, and development of housing. The City will continue to monitor its development process and zoning regulations to identify and remove constraints to the development of housing.

The City will also continue to monitor federal and State legislation that could impact housing and comment on, support, or oppose proposed changes or additions to existing legislation, as well as support new legislation when appropriate. Special attention will be given by the City in the minimizing of governmental constraints to the development, improvement, and maintenance of housing.

2013-2021 Objectives:

- On an ongoing basis, monitor changes in State and Federal laws and revise City policies, programs, and regulations as necessary and appropriate.

GOAL 3.0: Assist Lower Income Tenants in Finding the Appropriate Resources to Allow them to Remain in the Community

Policy 3.1: Take positive steps to ensure all segments of the population are aware of their rights and responsibilities regarding fair housing.

Policy 3.2: Assist in settling disputes between tenants and landlords.

Policy 3.3: Assist residents in locating providers of housing services.

Policy 3.4: Maintain a housing service directory that provides referrals for rental assistance, local affordable housing projects, senior housing, housing legal assistance, fair housing, homeownership assistance, and rehabilitation, and disseminate information to the public.

Implementing Programs

9. Housing Choice Voucher Rental Assistance Program: The Housing Choice Voucher program extends rental subsidies to very low income households, providing a voucher to pay the difference between the fair market rent (FMR) as established by HUD and what a tenant can afford to pay (i.e. 30 percent of household income). The voucher allows a tenant to choose housing that costs above the payment standard, providing the tenant pays the extra cost. The Housing Authority of the County of Los Angeles (HaCOLA) coordinates Housing Choice Voucher rental assistance on behalf of the City. An average of 34 San Fernando residents received Housing Choice Voucher rental assistance annually between 2008 and 2012, with 79 additional residents on the waiting list as of July 2013. HUD requires that 75 percent of new admissions be limited to extremely low income households (30 percent AMI). Given the significant gap between market rents and what these extremely low and very low income households can afford to pay for housing, Housing Choice Vouchers play a critical role in allowing such households to remain in the community, and is a key program to address the needs of extremely low and very low income households.

2013-2021 Objectives:

- Continue to support HaCOLA's administration of the Housing Choice Vouchers Program and assist an average of approximately 35 extremely low and very low income households annually during the planning period.
- Encourage landlords to accept Housing Choice Vouchers.
- Prepare bilingual HaCOLA program contact information for prospective landlords and tenants.
- Support the IVHA's applications for additional voucher allocations and efforts to maintain and expand voucher use in the City.

10. Fair Housing Program: The City currently contracts with the Fair Housing Council of San Fernando Valley (FHCSFV) to provide fair housing and tenant/landlord information to residents. Services include: investigation of discrimination complaints; community outreach and education; and counseling and referrals to other agencies when individuals may have been victims of discrimination. The FHC conducts several workshops each year in the San Fernando Valley for tenants, and separately for landlords/owners to discuss fair housing rights and responsibilities. Landlord/tenant counseling services involves informing landlords and tenants of their rights and responsibilities under the California Civil Code and mediating conflicts between tenants and landlords. They also offer free apartment manager trainings in English and Spanish at their offices.

The City monitors and attempts to minimize discriminatory housing practices with the assistance of the FHC. The City advertises the availability of fair housing services by posting bi-lingual fair housing brochures at public counters, including at recreation and senior centers. Furthermore, the City has accommodated FHC workshops and City staff refer potential fair housing issues to the FHC.

2013-2021 Objectives:

- Annually contract with a fair housing service provider to promote open housing practices for residents, and to facilitate communication between tenants and landlords.
- Continue to disseminate bi-lingual fair housing brochures in a variety of public locations, including City Hall, San Fernando Recreation Park community center, Las Palmas Park community center, and the local County library, and provide enhanced outreach through coordination of fair housing education with existing community events.

GOAL 4.0: Provide Opportunities for Lower and Moderate Income Households to Become First-Time Homebuyers

- Policy 4.1:** Provide information and referral about homebuyer assistance programs available through the County, State, and private lenders to existing and potential residents.
- Policy 4.2:** Promote homebuyer education seminars offered through the Los Angeles County Community Development Commission.
- Policy 4.3:** Provide homebuyer assistance to lower and moderate income purchasers in City-assisted developments, when feasible.
- Policy 4.4:** Promote available foreclosure resources through the City's website and informational handouts at the Community Development Department public counter.

Implementing Programs

11. Homeownership Programs: Prospective lower and moderate income San Fernando homeowners have access to three County-run first-time homebuyer programs:

- Mortgage Credit Certificate (MCC): This program enables lower and moderate income first-time homebuyers to receive a Federal income tax credit of up to 15 percent of the annual mortgage interest paid. The MCC reduces Federal income tax, increases take-home pay, and increases the qualifying loan amount for homebuyers. Program assistance is available only to income-eligible persons and families who have not owned a home in the last three years. The property must be a single-family detached home, condominium, or townhouse.
- Home Ownership Program (HOP): The Los Angeles County Community Development Commission (CDC) administers the County's Home Ownership Program (HOP), offering up to \$60,000 in deferred payment, zero percent interest loans for downpayment and closing cost assistance for lower income households. San Fernando is a participating jurisdiction in the HOP program, and has for-sale housing stock that falls within the sales price maximums. This program can be used in conjunction with the Mortgage Credit Certificate (MCC). Prospective participants must attend eight hours of homebuyer counseling.
- Southern California Home Financing Authority (SCHFA): SCHFA offers a mortgage revenue bond program that issues 30-year mortgage revenue funds at below-market interest rates. To be eligible for the program, the buyer must be a first-time homebuyer whose income may not exceed 120 percent of the Los Angeles County median income. The program also provides downpayment and closing cost assistance in the form of a gift equivalent to four percent of the first loan amount.

2013-2021 Objectives:

- Actively promote the MCC, HOP, and SCHFA programs to expand homeownership. Update the City website in 2014 to provide links to County resources.
- Prepare and distribute a bi-lingual program flyer.
- Annually conduct targeted outreach to realtors, mortgage brokers and lending institutions to advise them of these homebuyer assistance programs.

Table 35: Housing Program Summary

Housing Program	Program Goal	2013-2021 Objective	Funding Source	Responsible Agency	Time Frame
1. Residential Rehabilitation Program	Provide financial assistance for home repairs for lower and moderate income households.	Assist 20 households during the planning period, subject to available funding.	Residual LMIHF housing set-aside funds (if any)	Community Development Department	Ongoing through 2021
2. Neighborhood Preservation and Revitalization Program (CAPP)	Abate illegal activity, nuisance behaviors and problem properties.	Implement CAPP within designated Focus Areas.	Department Budget	Community Development Department	Ongoing through 2021
3. Housing Inspection Programs	Improve the quality of housing through housing conditions inspections.	Complete inspections of approximately 200 apartment units annually. Inspect ownership units upon re-sale.	Department Budget; Inspection fees	Community Development Department	Ongoing through 2021; Conduct outreach and education annually
4. Lead Based Paint Awareness	Reduce the risk of lead based paint hazards to health through educational outreach.	Remediate lead cases. Coordinate with LA County and Pacoima Beautiful on educational programs and identification of funding sources.	Department Budget	Community Development Department; LA County Dept of Health Services	Ongoing through 2021
5. Facilitate Affordable and Special Needs Housing Development	Provide financial and regulatory assistance in support of affordable and special needs housing.	Collaborate with affordable housing developers, offer regulatory concessions and incentives, and identify new funding sources to facilitate production of at least 195 new affordable and special needs housing units.	Department Budget	Community Development Department	Ongoing through 2021
6. Conservation of Existing and Future Affordable Units	Conserve the City's existing and future lower and moderate income rental housing stock.	Monitor the status of the existing and future affordable rental housing stock in San Fernando. Work with property owners, interest groups and the State and Federal governments to conserve its affordable housing stock.	Department Budget	Community Development Department	Ongoing through 2021

Table 35: Housing Program Summary

Housing Program	Program Goal	2013-2021 Objective	Funding Source	Responsible Agency	Time Frame
7. Monitor Residential Capacity	Ensure that the City's land inventory is adequate to accommodate the RHNA throughout the planning period.	Monitor the consumption of residential acreage to ensure an adequate inventory is available to meet the City's RHNA obligations. Develop and implement a formal ongoing (project-by-project) evaluation procedure pursuant to Government Code Section 65863.	Department Budget	Community Development Department	Annually as part of the City's report to HCD on Housing Element implementation
8. Removal of Governmental Constraints	Identify and eliminate governmental constraints to the provision of affordable and special needs housing.	Monitor changes in State and Federal laws and revise City policies, programs, and regulations as necessary and appropriate.	Department Budget	Community Development Department	Ongoing through 2021
9. Housing Choice Voucher Assistance Program	Provide rental assistance to extremely low and very low income households.	Support HaCOLA's administration of the program to assist an average of 35 extremely low and very low income households annually. Encourage landlords to register units; prepare bilingual HaCOLA program contact information.	HUD Section 8	Community Development Department; HaCOLA	Ongoing through 2021
10. Fair Housing Program	Promote fair housing practices.	Contract with the FHCSFV or another fair housing service provider; disseminate brochures; coordinate fair housing education with community events.	CDBG	Community Development Department; Fair housing service provider	Ongoing through 2021
11. Homeownership Programs	Expand homeownership opportunities for lower and moderate income households.	Promote County homebuyer programs. Prepare and distribute bi-lingual program flyer. Advertise County programs at public counters and on the City's website.	Department Budget	Community Development Department	Ongoing through 2021

Table 36: Summary of Quantified Objectives

Income Level	RHNA	New Construction	Rehabilitation*	Conservation**
Extremely Low	27	10	0	0
Very Low	28	30	2	73
Low	32	55	8	95
Moderate	35	100	5	4
Above Moderate	95	125	0	0
Totals	217	320	15	172

Notes:

* Reflects single-family rehabilitation program through the exhaustion of residual RDA set-aside funds, if any (see Program 1).

** Reflects preservation of very low, low and moderate income rental units identified in [Table 21](#)~~Table 21~~.

Formate

APPENDIX A: PUBLIC PARTICIPATION SUMMARY

Government Code Section 65583(c)(8) requires that local governments make “a diligent effort...to achieve public participation of all economic segments of the community in the development of the housing element.” Public participation played an important role in the formulation and refinement of the City’s housing goals and policies and in the development of a Land Use Plan, which determines the extent and density of future residential development in the community.

City residents had several opportunities to recommend strategies, review, and comment on the Housing Element. Two Community Workshops were held prior to completion of the draft Housing Element and the draft document was presented to the Planning Commission at a noticed public meeting prior to transmittal of the document to the State Department of Housing and Community Development (HCD).

Meeting notices were posted on the City’s website, and notification was published in the local newspaper in advance of the workshops/meetings. Copies of the draft Element were made available for review at City Hall and were posted on the City website, and notices were sent directly to agencies that serve the City’s special needs populations and to others who requested to receive such notification. These service providers and interested parties included organizations that represent the housing interest groups. Table A-1 provides a summary of public comments while Table A-2 includes the public notification distribution list.

The following is a list of opportunities for public involvement in the preparation of this Housing Element update.

Workshop #1	September 14, 2013
Workshop #2	September 28, 2013
Planning Commission Public Meeting	October 15, 2013
Planning Commission Adoption Hearing	January 7, 2013
City Council Adoption Hearing	TBD

Table A-1: Summary of Public Comments and Housing Element Response

Comment Themes	Housing Element Response
<i>Community Workshops– September 14, 2013 and September 28, 2013</i>	
The City should actively advertise and inform residents and the real estate community of available programs that offer assistance to prospective homeowners.	Program 11 – homeownership programs
The City should actively market the City to the commercial development community. Economic development is greatly needed to create businesses and services that cater to the needs of residents.	This City is actively marketing opportunities within the San Fernando Corridor Specific Plan to the commercial real estate community. Creating a vibrant population within the Specific Plan area will help attract new businesses to locate to the City.
The City should enhance outreach efforts so more residents participate in public meetings.	The two community workshops were publicly noticed, with special notification to service providers and housing professionals. Flyers have also been distributed to all properties within the City and posted on the City’s website and community locations. Approximately 15 people attended the two community workshops.
The City should enhance or improve outreach efforts to educate owners and real estate professionals about the City’s inspection upon re-sale requirements.	Program 3 – Housing Inspection Programs
A resident described a number of ongoing issues she was having with her landlord and the property she is renting. Issues include substandard living conditions (e.g., windows don’t open, plumbing issues, doors do not close properly, fire safety issues, et cetera.). What help is available for tenants who are renting substandard units and the landlord is unresponsive?	Program 3 – Housing Inspection Programs Program 10 – Fair Housing
Many people have misconceptions about labels such as “low income.” The presentation could be improved to provide more information about the types of jobs that pay “low” or “moderate” income wages. That would help put a local face on the income categories. Some people who are often against lower and moderate income housing might actually meet those definitions.	The presentation was modified for the second community workshop held on September 28, 2013 to link typical jobs to the income categories.
The Housing Element should focus on addressing the needs of existing residents, not to attract new people to the City.	It was explained that the first goal of the Housing Element is to preserve and maintain the quality of the City’s housing stock and neighborhood. However, new housing opportunities are needed to allow “children” of long-time residents to return to the community after finishing college or to allow seniors to age in place. Accessory ^{Second} units, townhomes, condominiums, and apartments offer opportunities for affordable housing.
Higher density lower income housing will help preserve existing single-family neighborhoods by relieving overcrowding and illegal garage conversions. West Hollywood is one example where a high density corridor has helped preserve single-family neighborhoods. Also, the increased population and activity that comes with higher density corridors	The Housing Element Resources section includes a strategy to accommodate higher density housing within the Specific Plan corridors.

Table A-1: Summary of Public Comments and Housing Element Response

Comment Themes	Housing Element Response
will attract restaurants and other businesses within mixed use and commercial corridors.	
San Fernando needs to find ways to keep young professionals in the community. An income of \$80,000 to purchase a median priced home is too high.	The Housing Element includes various programs to create more lower and moderate housing and referral to homeownership assistance.
The City should define and establish a threshold for “slumlord” to categorize people who violate landlord/tenant law.	Enforce City codes and refer landlord/tenant issues to the Fair housing service provider – see Program 10. The City’s Community Preservation staff also addresses housing code violations.
Will the City buy land to build new housing to accommodate the RHNA? If not, where will this new housing go? San Fernando does not have a lot of large vacant lots.	Resources section identifies areas where the housing will be accommodated. New housing is primarily going to be accommodated through accessory second- dwelling unit construction, and housing within the City’s R-2 and R-3 multi-family residential zones and the San Fernando Corridor Specific Plan area.
San Fernando needs more higher-density housing to complement planned transit improvements, including the East San Fernando Valley Transit Corridor project and the statewide High Speed Rail project.	The Housing Element Resources section includes a strategy to accommodate higher density housing within the Specific Plan corridors and near major public transit centers/nodes..
Are there any programs to assist owners who are at risk of losing their home to foreclosure?	The Housing Element has a policy: Promote available foreclosure resources through the City’s website and informational handouts at the Community Development Department public counter. New website will include a registry of available resources.
<i>Planning Commission Meeting – October 15, 2013</i>	
Resident spoke on her need for accessible housing for her daughter with developmental disabilities	Staff responded that a new affordable housing project targeted for persons with disabilities is under construction and provided information for the resident to apply for the units.
Fair Housing Council of San Fernando Valley indicated that many accessorysecond units in the City are rented at high costs to lower income families.	City staff responded that there is also an emerging trend of accessorysecond units being used to accommodate caretakers of elderly households or adult children that are returning home due to financial reasons. AccessorySecond units allow for such options for families in need.

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
L.A. Family Housing	Stephanie	Klasky-Gamer	President/CEO	7843 Lankershim Boulevard	North Hollywood, CA 91605
Assistance League - San Fernando Valley				22700 Sherman Way Rms 7&8	West Hills, CA 91307
Catholic Charities of Los Angeles, Inc.	Monsignor Gregory	Cox	Executive Director	P.O. Box 15095	Los Angeles, CA 90015-0095
Fair Housing Council of the San Fernando Valley	Diana C.	Bruno	Executive Director	14621 Titus St., #100	San Fernando Valley, CA 91402
Greater San Fernando Valley Chamber of Commerce	Nancy Hoffman	Vanyek	CEO	7120 Hayvenhurst Avenue, Suite 114	Van Nuys, CA 91406
Habitat for Humanity - San Fernando Valley	Donna	Deutchman	CEO	21031 Ventura Blvd. Suite 610	Woodland Hills, CA
Housing Authority of the City of Los Angeles	Douglas	Guthrie	President/CEO	2600 Wilshire Blvd.	Los Angeles, CA 90057
Housing Authority of the County of Los Angeles	Sean	Rogan	Executive Director	700 West Main Street	Alhambra, CA 91801
Los Angeles Homeless Services Authority	Michael	Arnold	Executive Director	811 Wilshire Blvd., 6th Floor	Los Angeles, CA 90017
Mid Valley Family YMCA	Greg	Koubek	Executive Director	6901 Lennox Ave.	Van Nuys, CA 91405
Mid Valley Jeopardy Foundation				6015 Woodman Avenue	Van Nuys, CA 91401
National Council of Jewish Women/Los Angeles	Hillary	Selvin	Executive Director	543 N. Fairfax Ave.	Los Angeles, CA 90036
Rotary of Greater Van Nuys	Pete	Satuloff		20700 Ventura Blvd. Ste.205	Woodland Hills, CA 91364
San Fernando Valley Rescue Mission	Wade	Trimmer	Director	13422 Saticoy Street	North Hollywood, CA 91605
The Valley Economic Alliance				5121 Van Nuys Boulevard, Suite 200	Sherman Oaks, CA 91403
United Chambers - SFV & Region	Marian E.	Jocz	Executive Director	5121 Van Nuys Boulevard, Suite 203	Sherman Oaks, CA 91403
Valley Industry & Community Association	Stuart	Waldman	President	5121 Van Nuys Blvd., Suite 208	Sherman Oaks, CA 91403
Valley Interfaith Council (VIC)				4505 LAS VIRGENES RD., STE. 211	CALABASAS, CA 91302
VEDC Business Center	Roberto	Barragan	President	5121 Van Nuys Blvd., 3rd Floor	Van Nuys, CA 91403
Aid For Aids, Inc.				825 Colorado Blvd. Ste. 100	Los Angeles, CA 90041
AIDS Healthcare Foundation	Michael	Weinstein	President	6255 W. Sunset Blvd. 21st Fl.	Los Angeles, CA 90028
Bridge to Home	Tim	Davis	Executive Director	P.O. Box 802978	Santa Clarita, 91380
Child and Family Guidance Center	Roy	Marshall	President/CEO	9650 Zelzah Ave.	Northridge, CA 91325
Child Care Resource Center, Inc.	Dr. Michael	Olenick	President/CEO	20001 Prairie Street	Chatsworth, CA 91311
Children's Hunger Fund	Dave	Phillips	President	P.O. Box 7085	Mission Hills, CA 91346
Community Enhancement Services				16743 Schoenborn St.	North Hills CA 91343

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
Covenant House California	Patrick S.	McCabe	Executive Director	1325 North Western Avenue	Hollywood, California 90027
Creative Minds ADP, Inc.	Liana	Aidinova	Program Director	6045 Woodman Avenue	Van Nuys, CA 91401
Eliza Shanks Home Inc				13055 Weidner St.	Pacoima, CA 91331
Family Promise of East San Fernando Valley	Kimberly	Rose	Network Director	P.O. Box 1307	Burbank, CA 91507
Family Rescue Center				22103 Vanowen Street	Canoga Park, CA 91303
Food or Not				9663 Santa Monica Blvd., #743	Beverly Hills, CA 90210
Hillview Mental Health Center	Dr. Eva S.	McCraven	President/CEO	12450 Van Nuys Blvd., Suite 200	Pacoima, CA 91331
Hope of the Valley Rescue Mission	Ken	Craft	President/CEO	P.O. Box 248	Sun Valley, CA 91353
Independent Living Center of Southern California	Norma Jean	Vescovo		14407 Gilmore Street, #101	Van Nuys, CA 91401
Inner Circle Foster Care & Adoption Services	Pamela G.	Jordan	Executive Director	7120 Hayvenhurst Avenue, Ste. 204	Van Nuys, CA 91406
JFS/SOVA Community Food & Resource Program				16439 Vanowen Street	Van Nuys, CA 91406
Lamp Community	Donna	Gallup	CEO	526 San Pedro Street	Los Angeles, CA 90013
Los Angeles Community Builders, Inc.				14800 Sherman Way	Van Nuys, CA 91405
Lutheran Socia Services of Southern California				6425 Tyrone Ave.	Van Nuys, CA 91401
Many Mansions				1459 E. Thousand Oaks Blvd. - Bldg. D	Thousand Oaks, CA 91362
MEND - Meet Each Need with Dignity	Marianne Haver	Hill	President/CEO	10641 N. San Fernando Rd.	Pacoima, CA 91331
My Friend's Place	Heather	Carmichael	Executive Director	P.O. Box 3867	Hollywood, CA 90078
North Hollywood Interfaith Food Pantry				4390 Colfax Ave.	Studio City, CA 91604
North Los Angeles County Regional Center	George	Stevens	Director	15400 Sherman Way, Suite 170	Van Nuys, CA 91406-4211
North Valley Caring Services, Inc.	Ivette	Pineda	Executive Director	15453 Rayen Street,	North Hills, CA 91343, USA
Northeast Valley Health Corporation	Kimberly	Wyard	CEO	1172 N. Maclay Ave.	San Fernando, CA 91340
Penny Lane Centers	Ivelise	Markovits	CEO	15305 Rayen St.	North Hills, CA 91343
People in Progress				P.O. Box 17216	Los Angeles, CA 90017
San Fernando Valley Community Mental Health Center, Inc.	Ian	Hunter	President/CEO	6842 Van Nuys Blvd., 6th Floor	Van Nuys, CA 91405
Santa Clairta Shelter	Annette	Guzman	Shelter Manager	23031 Drayton St.	Santa Clarita, 91355
St. Charles Borromeo Family Service Center				10834 Moorpark St.	North Hollywood, CA 91602
Sylmar Emergency Winter Shelter	Roy	Sua	Shelter Manager	12860 Arroyo St.	Sylmar, CA 91342
The Center for Individual	Sherry	Brill	Executive	5445 Laurel Canyon	North Hollywood, CA

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
and Family Counseling			Director	Blvd.	91607
The Village Family Services	Hugo C.	Villa	CEO	6736 Laurel Canyon Blvd., Suite 200	North Hollywood, CA 91606
Tierra del Sol Foundation	Steve	Miller	Executive Director	9919 Sunland Boulevard	Sunland CA 91040
Valley Family Center	Gary	Bessler	Executive Director	302 S. Brand Blvd.	San Fernando, CA 91340
Valley Village	Debra	Donovan	Executive Director	20830 Sherman Way	Winnetka, CA 91306
Volunteer League of the San Fernando Valley				14603 Hamlin Street	Van Nuys, CA 91411
Walden Family Services	Marci	Galvez	Regional Director	18860 Nordhoff Street, Suite 200	Northridge, CA 91324
A Community of Friends				3701 Wilshire Blvd., Suite 700	Los Angeles, CA 90010
Beyond Housing	Christine	Mirasy-Glasco	President/CEO	340 North Madison Ave.	Los Angeles, CA 90004
Homes For Life Foundation				8939 S. Sepulveda Boulevard, Suite 460	Los Angeles, CA 90045
Western Seniors Housing				17748 Sky Park Circle, Suite 225	Irvine, CA 92614
Los Angeles Unified School District	Dr. John E.	Deasy	Superintendent	333 S. Beaudry Ave.	Los Angeles, CA 90017
Bank of America Home Loans	Eric	Mozilo	Home Loans Manager	345 N. Brand Blvd.	Glendale, CA 91203
Bank of America Home Loans	Bill	Greene	Home Loans Manager	24200 Magic Mountain Pkwy, Suite 110	Santa Clarita, CA 91355
Chase - Mortgage Services	Moses E.	Hernandez	Mortgage Banker	402 S. Brand Blvd.	San Fernando, CA 91340
CitiBank	Michelle	Keuchkerian	Mortgage Specialist	1965 N. Hillhurst Ave.	Los Angeles, CA 90027
KPL Select Mortgage, Inc.				4348 Van Nuys Boulevard, Suite 200	Sherman Oaks, California 91403
Prospect Mortgage, LLC				11011 Balboa Blvd.	Granada Hills, CA 91344
Prospect Mortgage, LLC				9324 Reseda Blvd	Northridge, CA 91324
Wells Fargo Home Mortgage				807 San Fernando Rd.	San Fernando, CA 91340
California Association of Realtors				525 South Virgil Ave.	Los Angeles, CA 90020-1403
Century 21 Albert Foulad Realty				17835 Ventura Boulevard, Suite 200	Encino, CA 91316
Century 21 All Moves				11011 Balboa Boulevard	Granada Hills, CA 91344
Century 21 Crest				1501 West Magnolia Boulevard	Burbank, CA 91506
Dilbeck Real Estate	Denis	Bolen	Manager	14601 Ventura Blvd.	Sherman Oaks, CA 91403
Keller Williams Realty Northridge				9324 Reseda Blvd.	Northridge, CA 91324

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
Park Regency Realty	Joe	Alexander	President	10146 Balboa Boulevard	Granada Hills, CA 91344
Prudential California Realty - Northridge	John	Maquar	Manager	9003 Reseda Blvd. Suite 105	Northridge, CA 91324
RE/MAX Olson & Associates				11141 Tampa Ave	Northridge, CA 91326
Southland Regional Association of Realtors, Inc.				7232 Balboa Blvd.	Van Nuys, CA 91406
Del Sol Realty				662 N Maclay Ave	San Fernando, CA 91340
Armas Norma				1000 N Maclay Ave	San Fernando, CA 91340
Ascencio Gerardo				458 N Maclay Ave	San Fernando, CA 91340
Hernandez Henry				1701 Truman St # I	San Fernando, CA 91340
Home Solution Team Inc				707 N Maclay Ave	San Fernando, CA 91340
San Fernando Realty				458 San Fernando Mission Blvd	San Fernando, CA 91340
Curiel Francisco				731 N Maclay Ave	San Fernando, CA 91340
Progressive Realtors				321 N Maclay Ave Apt N	San Fernando, CA 91340
Professional Modification Svc				110 N Maclay Ave	San Fernando, CA 91340
Reality Executives Media Center Team Trueman				1701 Truman St	San Fernando, CA 91340
Casa Linda Realty				130 N Maclay Ave	San Fernando, CA 91340
Sixteen Sixteen Second Street				1616 2nd St	San Fernando, CA 91340
Vaughn Street Partners				13618 Vaughn St	San Fernando, CA 91340
Aspen & Associates				1543 Truman St	San Fernando, CA 91340
Colon Anna Maria				832 N Maclay Ave	San Fernando, CA 91340
Pittmanh Aspen				1547 Truman St	San Fernando, CA 91340
Fajardo Josephina				662 N Maclay Ave	San Fernando, CA 91340
Herbert Gomez Real Estate				741 S Workman St	San Fernando, CA 91340
Era-Rocking Horse Realty				832 N Maclay Ave	San Fernando, CA 91340
Mata Salvador				1000 N Maclay Ave	San Fernando, CA 91340
Park Avenue Realty				1960 Glenoaks Blvd Ste 1	San Fernando, CA 91340
Montes Eddie				321 N Maclay Ave # B	San Fernando, CA 91340
Silva Benny				563 S Brand Blvd	San Fernando, CA 91340
Alvaro & Conception Gonzalez				703 Glenoaks Blvd	San Fernando, CA 91340

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
Ho Alen				760 N Huntington St	San Fernando, CA 91340
Gutierrez Maricruz				1000 N Maclay Ave	San Fernando, CA 91340
Moran Jorge				1960 Glenoaks Blvd # 1	San Fernando, CA 91340
Moran Marcelo				545 N Maclay Ave	San Fernando, CA 91340
Superior Realty & Services				330 N Maclay Ave Ste 201	San Fernando, CA 91340
AnMar Properties				523 S. Brand Blvd., #101	San Fernando, CA 91340
Home Sweet Home Realty				1000 N Maclay Ave	San Fernando, CA 91340
Vanoni Realty Corp.				811 San Fernando Road Ste 204	San Fernando, CA 91340
Champion Realty				1701 Truman St # I	San Fernando, CA 91340
Aszkenazy Development Inc				601 S Brand Blvd # 3	San Fernando, CA 91340
Mission Real Estate				458 N Maclay Ave	San Fernando, CA 91340
Emerita J Ramirez				1030 N Maclay Ave	San Fernando, CA 91340
Palacios Properties, Inc.				551 San Fernando Mission Blvd	San Fernando, CA 91340
G V Properties				1024 N Maclay Ave # K	San Fernando, CA 91340
Paramount Properties				451 S Brand Blvd	San Fernando, CA 91340
Milestone Mortgage & Realty				737 S Workman St	San Fernando, CA 91340
San Fernando Senior High School				11133 O'Melveny Ave	San Fernando, CA 91340
O'Melveny Elementry School				728 Woodworth St.	San Fernando, CA 91340
Morningside Elementary School				576 North Maclay	San Fernando, CA 91340
San Fernando Elementry School				1130 Mott Street	San Fernando, CA 91340
San Fernando Middle School				130 N Brand Blvd	San Fernando, CA 91340
César Chávez Learning Academies				1001 Arroyo Avenue	San Fernando, CA 91340
Gridley Elementary School				1907 Eighth St	San Fernando, CA 91340
Sylmar Senior High School				13050 Borden Ave.	San Fernando, CA 91340
Mission Continuation School				11015 O'Melveny Ave	San Fernando, CA 91340
Vista del Valle Dual Language Academy				12441 Bromont Ave	San Fernando, CA 91340
Lakeview Charter High School				919 Eighth Street	San Fernando, CA 91340
Vaughn Next Century Learning Center				13330 Vaughn Street	San Fernando, CA 91340

Table A-2: Housing Element Outreach List

Organization	Contact First	Contact Last	Title	Address	City, State Zip
Nueva Esperanza Charter Academy				1218 North Fourth Street	San Fernando, CA 91340
Santa Rosa / Bishop Alemany School				1316 Griffith Street	San Fernando, CA 91340
St. Ferdinand Elementary School				1012 Coronel Street	San Fernando, CA 91340
Concordia - San Fernando				777 North Maclay Avenue	San Fernando, CA 91340
Glenoaks Elementary				1525 Glenoaks Blvd	San Fernando, CA 91340
San Fernando KinderCare				2100 Frank Modugno Drive	San Fernando, CA 91340
YWCA Infant Learning Center				11133 O'melveny Ave	San Fernando, CA 91340
YWCA Child Development Center				1200 N Maclay Ave	San Fernando, CA 91340
San Fernando Child Development Center				1204 Woodworth St	San Fernando, CA 91340
Wooden Shoe Nursery School				1525 Glenoaks Blvd	San Fernando, CA 91340
Kalishar Head Start				340 Parkside Dr	San Fernando, CA 91340

APPENDIX B: REVIEW OF PAST ACCOMPLISHMENTS

Under State Housing Element law, communities are required to assess the achievements under their adopted housing programs as part of the periodic update to their housing elements. These results should be quantified where possible (e.g. the number of units rehabilitated), but may be qualitative where necessary (e.g. mitigation of governmental constraints). The results should then be compared with what was projected or planned in the earlier element. Where significant shortfalls exist between what was planned and what was achieved, the reasons for such differences must be discussed.

The City of San Fernando 2008-2014 Housing Element sets forth a series of housing programs with related objectives for the following areas:

- Housing and Neighborhood Conditions
- Development of New Affordable Housing
- Tenant Assistance
- Homeownership

This section reviews the City's progress to date in implementing these housing programs and their continued appropriateness for the 2013-2021 Housing Element. Table B-1 compares quantified objectives and accomplishments during the 2008-2014 planning period (through end of 2012); however, new construction objectives are reported for the RHNA cycle, which started in 2006. Table B-2 summarizes the City's housing program accomplishments, followed by a review of its quantified objectives. The results of this analysis will provide the basis for developing the comprehensive housing program strategy presented in Part C of this section.

Table B-1: Objectives vs. Accomplishments

Income Category	New Construction (2006-2012)*		Rehabilitation (2008-2012)**		Conservation (2008-2012)***	
	Objective	Result	Objective	Result	Objective	Result
Extremely Low	31	0	6	0	0	0
Very Low	31	19	5	0	54	54
Low	38	54	23	10	54	54
Moderate	42	2	8	4	4	4
Above Moderate	109	74	0	0	2	2
Total	251	149	42	14	114	114

Notes:

*Reflects 2006-2014 RHNA

** Reflects single-family rehabilitation program loans

*** Reflects preservation of affordable multi-family units in Park Vista, Las Palmas and Park Avenue senior projects.

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
1. Residential Rehabilitation Program	Assist six to eight households per year under the Single-Family Rehabilitation Program.	<p>The City's former Redevelopment Agency (RDA) assisted the rehabilitation of 14 single-single family in 2008 and 2009. Funding of the program ceased in 2010 anticipation of RDA dissolution by the State in 2012.</p> <p>Continued Appropriateness: This program is continued in the 2013-2021 Housing Element; however, funding for the program is currently limited to residual RDA set-aside funds that may result from the California Department of Finance meet and confer process (if any).</p>
	Develop program guidelines for establishment of a Rental Rehabilitation Loan component.	
2. Neighborhood Preservation and Revitalization Program (CAPP)	Continue to implement CAPP within the five designated Focus Areas, and identify additional Focus Areas as appropriate.	<p>The City implemented CAPP within the initial five Focus Areas as well as 15 additional focus areas identified during the planning period.</p> <p>The State's dissolution of the RDA led to a reorganization of the program. The City now conducts multi-agency inspections within two larger Focus Areas instead of 20 smaller areas. Each Focus Area currently has one full-time and one part-time code enforcement officer. Whereas program compliance had been driven by administrative citations, the current program is less punitive and community preservation Officers are now focused on compliance orders coupled with outreach and education of property maintenance issues.</p> <p>Continued Appropriateness: This program is continued but modified in the 2013-2021 Housing Element to account for recent programmatic changes resulting from the loss of RDA funding in 2012 and to include enforcement of the city's accessorysecond dwelling unit ordinance.</p>
	Annually report to the City Council on the status of the program.	
3. Rental Property Inspection Program	Complete inspections of approximately 200 units on an annual basis.	<p>The City inspected nearly 1400 multi-family rental units (average 280 units per year) during the planning period. Program accomplishments are reported to the City Council annually.</p> <p>Continued Appropriateness: This program is continued in the 2013-2021 Housing Element.</p>
	Provide City Council with annual program status report.	
4. Crime Free Rental Housing	Research existing crime-free rental program and organizations to establish a base methodology and program incentives, goals and objectives.	<p>Budget and staffing limitations prevented implementation of this program during the planning period.</p> <p>Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's RDA in 2012.</p>
	Initiate program in 2009.	
5. Lead Based Paint Awareness	Continue to remediate lead cases through the City's residential	The City continued public outreach to applicants for additions/rehabs regarding lead based paint

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
	<p>rehabilitation programs.</p> <p>Coordinate with LA County and Pacoima Beautiful regarding existing funding and programs.</p> <p>Conduct annual lead based paint seminar in City beginning in 2009.</p>	<p>abatement requirements and met with LA County representatives. Lead based paint seminars occurred in 2009 and 2010; however, the State's elimination of the RDA rendered further seminars infeasible. The City also continues to work with Pacoima Beautiful and the County of Los Angeles to identify funding for outreach and abatement.</p> <p>Continued Appropriateness: This program is continued but modified in the 2013-2021 Housing Element to account for recent programmatic changes resulting from the loss of RDA funding in 2012.</p>
6. Affordable Housing Development Assistance	<p>Complete a master EIR for downtown parking lot sites to expedite future processing of entitlements.</p> <p>Move forward with a Development Agreement on City-owned property for development of 100 affordable senior rental units.</p> <p>Issue a RFP for a mix of family rental and ownership units on other City-owned properties, and initiate discussions with adjacent property owners.</p>	<p>The City completed a draft EIR for the downtown parking lot sites in 2008; however, the Exclusive Negotiating Agreement (ENA) with the developer was allowed to expire so the EIR was never certified and the project did not move forward.</p> <p>Although the City did not issue a RFP for a mix of family rental and ownership units on other City-owned properties or initiate discussions with adjacent property owners, one City-owned parcel (1422 San Fernando Rd.) was leased to an affordable housing developer resulting in 20 new affordable units during the planning period.</p> <p>The City also received a 2013 Transit-Oriented Development (TOD) Planning Grant for a TOD overlay zone around the metro station. The planning effort will identify opportunities for affordable housing development within the TOD overlay.</p> <p>Continued Appropriateness: Elements of this program are incorporated into an overall program in the 2013-2021 Housing Element to facilitate and encourage affordable housing development during the planning period (see Program 5).</p>
7. Senior Housing	Conserve 112 units of affordable senior housing in the Park Vista, Las Palmas and Park Avenue senior projects.	<p>The City continues to monitor existing deed restricted senior units on an annual basis. All 112 units at Park Vista, Las Palmas and Park Avenue remain affordable and available to seniors.</p> <p>Continued Appropriateness: This program will be renamed and modified for the 2013-2021 Housing Element to include all existing and future deed-restricted affordable housing units.</p>
8. Accessory Second Dwelling Unit	Through implementation of the City's accessory second unit ordinance, provide additional sites	The City has issued ten second accessory dwelling unit building permits since 2008. As part of CAPP, code enforcement and planning personnel

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
Program	<p>for the provision of rental housing, and seek to achieve 50 accessory second units during the planning period.</p> <p>Promote the development of accessory second units by incorporating information on the City's website, and through code enforcement referrals to address overcrowding.</p>	<p>worked with prospective applicants who were eligible for accessory second-dwelling units. The City's website is in the process of redesign. The new website will include information about the accessory second-dwelling unit ordinance.</p> <p>Continued Appropriateness: This program is not continued in the 2013-2021 Housing Element as a separate program; however primary program components are included in the CAPP program (see Program 2).</p>
9. Affordable Housing Density Bonus	<p>Adopt a local density bonus ordinance by 2009 to implement State requirements as a means of enhancing the economic feasibility of affordable housing developments.</p> <p>Advertise on the City's website, and promote in conjunction with discussions with development applicants.</p>	<p>The City adopted a density bonus ordinance that complies with Government Code Sections 65915-65918 in 2013. The City will advertise the availability of the new ordinance on the website and promote the program while discussing applications with developers.</p> <p>Continued Appropriateness: This program has been implemented and is not included in the 2013-2021 Housing Element. Density bonuses and waiver/modifications of development standards will be included as incentives in an overall program to facilitate and encourage affordable housing development during the planning period (see Program 5).</p>
10. Inclusionary Zoning	<p>Adopt a local inclusionary housing ordinance applicable to San Fernando's Redevelopment Project Areas by 2009.</p> <p>Evaluate the alternative options for fulfillment of inclusionary requirements, such as provision of affordable units off-site or payment of an in-lieu fee.</p> <p>Advertise on the City's website, along with incentives available through the density bonus ordinance for on-site provision of affordable units.</p>	<p>Inclusionary regulations included as part of 2011 Redevelopment Plan Amendment of the Consolidated Redevelopment Project Areas prior to state dissolution of the City's Redevelopment Agency.</p> <p>Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's RDA in 2012.</p>
11. Zoning Ordinance Revisions	<p>Amend the zoning ordinance by December 2009 to make explicit provisions for manufactured housing, community care facilities, SROs, transitional and supportive housing, and emergency shelters.</p> <p>Develop objective standards to regulate emergency shelters as provided for under SB 2.</p>	<p>The City amended the Zoning Code in March 2013 to make provisions for manufactured housing, community care facilities, SROs, transitional housing, supportive housing, and emergency shelters. As part of the same Zoning Code amendment, the City added objective standards to regulate emergency shelters pursuant to SB 2.</p> <p>Continued Appropriateness: This program has been modified for the 2013-2021 Housing Element (see Program 8).</p>

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
12. Pursue Outside Funding Sources	Actively pursue County, State, Federal and private funding sources as a means of leveraging local funds and maximizing assistance, with a goal of securing at least three new funding sources.	The City worked with prospective developers to identify outside funding sources such as Federal HOME and CDBG funds as well as TCAC tax equity credits to provide gap financing to leverage local funds during the planning period.
	Complete the development agreement on the downtown senior mixed-use project by early 2009 to enable dispersal of Prop 1C grant funds to the project.	In 2008, the City assisted a developer in obtaining \$3.56M Infill Infrastructure Grant for a 100 senior-unit / mixed-use project on an Agency-owned parcel. In 2009, HCD denied the City's request to allow the grant on an alternate location. As a result, the project did not proceed.
	Support housing grant applications both through regulatory relief offered through density bonuses, and through City Council endorsement/support of funding applications.	<p>In 2010, the City assisted two developers with HOME applications that would produce 95 affordable units and 22 market rate units. The projects included density bonus and/or variance applications. The projects were entitled and funded, but the developers chose not to move forward.</p> <p>In 2011, the City assisted two developers with HOME applications that would produce at total of 62 very low to low income rental units on a City-owned lot and 20 market rate units. The projects, located at 1422 San Fernando Rd. and 131 Park Ave., were occupied in 2013.</p> <p>In 2012, the City assisted one developer with HOME applications that will produce at total of 29 very low income rental units and 84 moderate income units. Project entitlements included a rezone from industrial to R-3 and a density bonus. The units are anticipated to be occupied after January 1, 2014.</p> <p>Continued Appropriateness: Elements of this program are incorporated into an overall program in the 2013-2021 Housing Element to facilitate and encourage affordable housing development during the planning period (see Program 5).</p>
13. Green Building	Develop educational materials on green building and provide to homeowners and builders in San Fernando.	The City required LEED Certification or comparable building design for agency-assisted housing projects during the planning period. For example, the construction drawings for the 20-unit affordable housing development at 1422 San Fernando Rd. achieved LEED Silver. Funding and staffing limitations made development of educational materials infeasible.
	Implement requirements for green building design in agency-assisted new construction projects.	
		Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
		RDA in 2012.
14. Section 8 Rental Assistance Program	Encourage landlords to register units with the Housing Authority.	<p>The City coordinated with landlords and encouraged participation in the Housing Choice Voucher Program (formerly Section 8) during the planning period. The City also prepared bilingual HaCOLA information and made it available to landlords and tenants who may be interested in the program.</p> <p>An average of 34 San Fernando residents received Housing Choice Vouchers annually between 2008 and 2012. As of July 2013, 79 San Fernando residents were on the Housing Choice Voucher program waiting list.</p> <p>Continued Appropriateness: This program is included, but renamed in the 2013-2021 Housing Element.</p>
	Prepare bilingual HaCOLA program contact information for prospective Section 8 landlords and tenants.	
15. Fair Housing Program	Continue to contract with the FHC to promote open housing practices for residents, and to facilitate communication between tenants and landlords.	<p>Bi-lingual fair housing brochures have been available at public counters since 2009, including at recreation and senior centers. The City made its facilities available to host FHC workshops in 2010.</p> <p>Continued Appropriateness: This program is continued in the 2013-2021 Housing Element.</p>
	Beginning in 2009, disseminate bi-lingual fair housing brochures in a variety of public locations, including City Hall, Cesar E. Chavez Park community center, Las Palmas Park community center, and the local library, and provide enhanced outreach through coordination of FHC's fair housing education with existing community events.	
16. Local Housing Mediation Service	Research existing housing mediation programs, including programs in the cities of Glendale and Burbank.	<p>Budget and staffing limitations prevented implementation of this program during the planning period.</p> <p>Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's RDA in 2012.</p>
	Establish parameters for a local Mediation Program, and initiate by late 2009.	
17. Housing Services Directory	Prepare and maintain a current housing service directory, and disseminate to the public.	<p>The City prepared a housing services directory in 2008. Initial distribution took place in 2009 and continued throughout the planning period.</p> <p>Continued Appropriateness: This program is not continued in the 2013-2021 Housing Element; however, the program objective is carried forward as a policy.</p>

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
18. Housemate Matching Program	Coordinate with Alternative Living for the Aging, as well as City sponsored programs, to develop the parameters for establishing and funding a local Housemate Matching Program.	Budget and staffing limitations prevented implementation of this program during the planning period. Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's RDA in 2012.
	Initiate the Program by 2009, and advertise throughout the community.	
19. City First-Time Homebuyer Program	Evaluate providing homeownership assistance to moderate income purchasers in agency-assisted projects on a case-by-case basis.	Budget and staffing limitations prevented implementation of this program during the planning period. Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to the State's dissolution of San Fernando's RDA in 2012.
20. Housing Economic Recovery Ownership (HERO) Program	Actively promote the HERO Program and similar programs to expand homeownership, including preparation of a bi-lingual program flyer and distribution to every household and commercial business in San Fernando.	City continued outreach to San Fernando residents during the planning period notifying them of the opportunity for eligible low/moderate income first time homebuyers to purchase foreclosed, vacant and abandoned properties in the City. The program was discontinued by the State during the planning period. The City also held a workshop in 2011 that was widely attended by local realtors, mortgage brokers, and lending institution representatives. Continued Appropriateness: This program is not included in the 2013-2021 Housing Element due to discontinuation by the State.
	Conduct targeted outreach to realtors, mortgage brokers and lending institutions to advise them of this homebuyer assistance program.	
21. County Homeownership Program (HOP)	Advertise the availability of the HOP Program at the public counter and on the City's website, along with the schedule of the county's bilingual first-time homebuyer seminars.	The City advertised HOP program availability by providing information at public counters. The City's website is undergoing a major redesign. The new website will include HOP program information. Continued Appropriateness: This program is included in a new program in the 2013-2021 Housing Element that promotes homeownership opportunities during the planning period (see Program 11).

Table B-2: Evaluation of Program Accomplishments under 2008-2014 Housing Element

Housing Program	Program Objectives	Program Accomplishments
22. Mortgage Credit Certificate (MCC) Program	Advertise the availability of the MCC Program at the public counter and on the City's website, along with a listing of participating MCC lenders.	<p>The City advertised HOP program availability by providing information at public counters. The City's website is undergoing a major redesign. The new website will include HOP program information.</p> <p>Continued Appropriateness: This program is included in a new program in the 2013-2021 Housing Element that promotes homeownership opportunities during the planning period (see Program 11).</p>
23. Foreclosure Prevention Resources	Promote available foreclosure resources through the City's website and informational handouts at the Community Development Department public counter.	<p>The City advertised foreclosure prevention resources to residents via the City's website and through the distribution of flyers during the planning period.</p> <p>Continued Appropriateness: This program is not continued in the 2013-2021 Housing Element; however, the program objective is carried forward as a policy.</p>

V. CIRCULATION ELEMENT

INTRODUCTION AND BACKGROUND

General Plan Legislation

The Circulation Element has been a required element of the General Plan since 1955. Section 65302 (b) of the California Government Code requires:

A Circulation Element consisting of the general location and extent of existing and proposed major thoroughfares, transportation routes, terminals, and other local public utilities and facilities, all correlated with the land use element of the plan.

During the past 20 years, transportation technology has advanced rapidly. This enhanced technology has resulted in an increased emphasis on the provision of a balanced multi-model transportation system to meet the needs of residents and businesses.

Purpose and Function

The purpose of the Circulation Element is to provide a safe and efficient transportation system for existing and proposed land uses within the community. The Element provides a basis for the orderly pattern of development while minimizing the impact of transportation services on residential neighborhoods and the environmental quality of the community.

Relationship to Other General Plan Elements

The manner in which people and goods move within and through the City is an important factor in maintaining a quality living environment. The Circulation Element must be closely coordinated with the Land Use and Housing Elements to fully utilize the resources of the community. In addition, the Circulation Element is also closely related to the Noise Element. For instance, the Circulation Element prescribes traffic volumes for the various roadways comprising the transportation network, while the Noise Element describes the baseline noise levels which are necessary to achieve noise compatible land uses. In an urban environment, aesthetic elements of a circulation system such as medians, street trees and attractive pedestrian walkways can provide passive open space and positive visual resources.

This topic is considered in the Open Space/Conservation/Parks-Recreation Elements. This 1987 Circulation Element revises and updates, as appropriate, the Element which was adopted in 1973. That Circulation Element incorporated many of the freeway improvements which were achieved during the past decade. The achievement of those improvements has resulted in less traffic through the core area of the City.

[In January 2005, the City Council adopted the Corridors Specific Plan \(SP-4\), which put in place policies and strategies to transform Truman Street, San Fernando Road, Maclay Avenue, and First Street into attractive, livable, and economically vital districts. Key components of the plan included a\) creating a more comfortable environment for pedestrians by introducing streetscape improvements and implementing roadway designs to tame the current flow of traffic, and b\) introducing a policy](#)

[framework with associated design standards and guidelines to focus activity and investment along these corridors.](#)

[In December 2017, the City amended the Corridors Specific Plan to accommodate the East San Fernando Valley Transit Corridor \(ESFVTC\) transit improvements, a proposal by the Los Angeles Metropolitan Transit Authority \(Metro\) to introduce either Bus Rapid Transit \(BRT\) along Truman Street, Low Floor Light Rail Transit \(LRT\) along San Fernando Road, or High Floor LRT within the existing railroad right-of-way, as well as to implement pedestrian, bicycle, and vehicular access to the San Fernando/Sylmar Metrolink Station and the transit stops associated with Metro's proposed ESFVTC initiative.](#)

FINDINGS AND CONCLUSIONS

This section summarizes the major findings and conclusions regarding circulation services and transportation facilities available within the City of San Fernando. The principal sources of background data for preparation of this section are listed below:

- The 1973 Circulation Element of the General Plan.
- CalTrans, "1985 Traffic Volumes on California Highways."
- Traffic impact analysis studies for public and private projects.
- Citywide speed zone surveys.
- Traffic studies on coordination of railroad operations with vehicular traffic movements.
- [Service data from the Southern California Rapid Transit District.](#)
- [Traffic impact analysis from the 2017 Corridors Specific Plan Environmental Impact Report \(EIR\)](#)

Findings

1. The local street system is comprised of major highways, secondary highways, collector and local access streets. A description of the existing arterial system is provided in Section B of the Technical Appendix. The functions of these streets are briefly described below: The major function of major and secondary highways is to move large volumes of traffic from one part of the City to another. On-street parking and access points along these highways to adjacent land uses are carefully considered in relationship to traffic volumes.

Collector streets provide both land access and traffic circulation within residential neighborhoods and commercial and industrial areas. This system differs from the arterial system in that facilities on the collector system may penetrate residential neighborhoods, distributing trips from the arterials, through the area, to the ultimate destination. The collector street system moves traffic to local shopping centers, schools, parks and between adjacent neighborhoods.

The function of the access street is to provide direct access to individual parcels. The access street is not designed for through traffic.

2. As the City of San Fernando is surrounded by freeways there is excellent freeway access. The City is served by the Golden State Freeway (1-5), the San Diego Freeway (1-405), the Foothill

Freeway (1-210), and the Simi-San Fernando Freeway (State Route 118). These routes are shown in Exhibit V-1.

3. San Fernando is served by two major inter-regional bus lines, the Greyhound Bus Lines and the Continental Trailways Bus System. Both offer service from San Fernando to points lying outside the Southern California Rapid Transit District and vice versa. Both lines use the Golden State Freeway in making connections to either Los Angeles or points to the north. There is no east-west service emanating in San Fernando.
4. ~~The City of San Fernando lies near the northern boundary of the Southern California Rapid Transit District. Through the numerous lines that make up this system, access is provided to points as far south as Newport Beach and as far east as Yucaipa. San Fernando is served by 10 lines that make connections with other sections of the San Fernando Valley and downtown Los Angeles. These lines are described in Section B of the Technical Appendix.~~
5. San Fernando is served by eight Los Angeles County Metropolitan Transportation Authority (Metro) bus routes which provide service to other sections of the San Fernando Valley and downtown Los Angeles.
4. San Fernando is served by the Antelope Valley line of the Metrolink commuter rail service. The Sylmar/San Fernando Metrolink Station is located just west of the Planning Area, near the intersection of Hubbard Avenue and First Street. Both the station and its park-and-ride lot sit just outside San Fernando's city limits. Trains on the line make 15 round trips on weekdays, and six round trips on both weekend days.
6. The San Fernando trolley offers daily service, stopping at 28 locations throughout the City,
- 5.7. Although the San Fernando Airport is no longer in operation, there are adequate regional facilities to provide air passenger and freight services.
- 6.8. Approximately five freight trains, ~~almost exclusively freight,~~ pass through San Fernando each day. ~~A 'Team Track' allows local dealers to ship or receive merchandise directly from parked freight cars.~~
- 7.9. The City is bounded by mountain ranges to the east and north and Van Norman Dam is located to the west. These areas do not generate traffic and, as a result, most through traffic from the west, north and east is generated in a relatively small area lying between the City of San Fernando and the above-mentioned geographic features.

Conclusions

1. The City of San Fernando is fully developed with well-established traffic patterns. The City's circulation system is classified according to the character of service which the street is intended to provide.
2. Street standards utilized by the City of Los Angeles are not appropriate for the City of San Fernando. Due to the geographic location of the City, arterials perform a somewhat different function in the circulation network.
3. Topographic barriers limit the area of traffic generation and there is more than average freeway access. Because of these factors, much of the traffic usually carried by arterials will use the freeway system instead. The City of San Fernando street classifications are described in Section B of the Technical Appendix.
4. There are adequate regional transportation facilities to serve the needs of the community.

ISSUES AND OPPORTUNITIES

This section outlines circulation related issues and opportunities in the City of San Fernando. The identification of these issues and opportunities serves to highlight areas of interest that should be considered in circulation planning and decisions on traffic improvements.

Issues

Traffic blockages, due to railroad switching movements, can impede through traffic circulation along major arterials and can restrict the movement of emergency vehicles.

Opportunities

1. Traffic studies have been completed that provide information useful to the coordination of railroad operations with vehicular traffic movements and to improve traffic flow through the City.
2. The construction of the Foothill and Simi Valley Freeways altered traffic patterns, resulting in less through traffic within the core area of the City. Concurrently, access to other destinations of regional interest was improved for San Fernando residents and businesses.

GOALS AND OBJECTIVES

This section identifies the continuing and long-range goals and objectives of the Circulation Element. As is the case for the other General Plan Elements, the goals and objectives are based on the information summarized in the previous sections, the data contained in the Technical Appendix and the 1973 Circulation Element.

Goals

1. To provide an efficient street system which allows maximum accessibility, while providing maximum safety and economy of movement.
2. To provide a street system that links San Fernando to other communities and regional facilities, while providing the residents of those communities with easily accessible routes to various facilities within the City of San Fernando.
3. To recognize problem areas and to implement programs aimed at solving those problems.
- ~~3.4.~~ To generate a pedestrian- and transit-oriented network of complete streets within the Corridors Specific Plan area that provides high quality connections to the Metrolink Station for all travel modes, while balancing the needs of automobile access with the safety and comfort of pedestrians and bicyclists.

Objectives

1. Conflicts between vehicular traffic and railway operations will be minimized to the maximum extent possible.
2. The Central Business District will be enhanced as a commercial area through the establishment of efficient circulation patterns.
3. Traffic Improvements will be implemented, as needed, to respond to changes in regional traffic patterns that affect local circulation.

4. Within the San Fernando Corridors Specific Plan (SP-5) area:

- a. Facilitate the transition of the Maclay Avenue, Truman Street, San Fernando Road, and First Street corridors into multi-modal streets that complement the land uses and development pattern planned for the corridors through implementation of the specific plan.
- b. Maintain and improve vehicular traffic circulation within the specific plan area and the adjacent community in order to safely and efficiently move both local and through traffic to its destination, while accommodating future demand for circulation by all modes of transportation.
- c. Implement traffic calming techniques to improve traffic and pedestrian safety.
- d. Create attractive urban streetscapes with design and amenities that are visually compatible with and enhance planned private development pursuant to this specific plan in general, and that support pedestrian use and outdoor activities in particular.
- e. Ensure Metro's East San Fernando Valley Transit Corridor project preserves on-street parking, does not compromise pedestrian accessibility and comfort, or negatively impact adjacent businesses.

IMPLEMENTATION MEASURES

This section presents the policies and action programs adopted by the Planning Commission and City Council in order to achieve a circulation and transportation system that meets the needs of the community. The implementation measures are, in effect, a statement of the policies and actions which have been endorsed by the City over the recent years.

Policies

1. The street classification system should separate those streets that are designed for through traffic from those that are local in nature. The intended function should be obvious to the driver.
2. San Fernando's circulation system should be coordinated with that of the City of Los Angeles, whose standards are different. This will insure the elimination of abrupt changes in the roadway that cause confusion and congestion for the motorist.
3. The circulation system should provide continuity of movement throughout the City and should facilitate safe, efficient emergency access.

Action Programs

1. Circulation Element Map implementation -- Exhibit V-1 is the official Circulation Element Map that indicates the street classifications for roadways in San Fernando and the regional freeway system. The goals and objectives of this map will be continually implemented and any changes will be processed as an amendment to the General Plan.
2. Inter-governmental Coordination — On the basis of City Council policy, there is coordination with the City of Los Angeles on a continuing basis regarding roadway transition areas between San Fernando and Los Angeles. Although each City maintains different street standards, the

Intergovernmental coordination serves to maximize the provision for a continuity of movement throughout the City without confusion or congestion.

3. Traffic Signalization — The City utilizes a traffic signal interconnect system which sequences traffic lights at different intersections to provide progression of traffic movement throughout the City. New state-of-the-art computerized controllers are being phased in to upgrade the system.
4. Traffic Impact Studies — To avoid adverse Impacts on businesses and residential neighborhoods, the City frequently authorizes special studies on the traffic impacts stemming from the development of public and private projects. In addition, the Traffic Commission continually surveys intersections at high accident locations for purposes of recommending traffic improvements.

INTERNAL CONSISTENCY

Street classifications are appropriate to serve the land use categories designated in the Land Use Element. The landscaping along major arterials provides passive open space, enhancing the visual image of the community to the motorist.

ATTACHMENT "B"**ORDINANCE NO. 1621****AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF
SAN FERNANDO, CALIFORNIA, APPROVING THE SAN
FERNANDO CORRIDORS SPECIFIC PLAN SP-5 AND
REPEALING IN ITS ENTIRETY THE FORMER CORRIDORS
SPECIFIC PLAN SP-4**

WHEREAS, on February 28, 2013, Metro awarded a \$282,392 planning grant to the City of San Fernando for the completion of a comprehensive update to the existing SP-4 San Fernando Corridors Specific Plan, to make the necessary amendments to the General Plan text, Zoning Ordinance text, and related maps and to prepare an Environmental Impact Report (EIR) pursuant to CEQA. The City agreed to contribute a match of \$13,306 (\$6,306 in-kind, such as staff-time and \$7,000 funds); and

WHEREAS, on May 19, 2014, the firm of Sargent Town Planning (Consultant) was selected to complete the project from a general solicitation to qualified consultants; and

WHEREAS, throughout this process, several public meetings, and workshops were conducted with residents, merchants, and local stakeholders. The City conducted several individual and small group meetings with local stakeholders in September 2014 and October 2014 and again in September 2015;

WHEREAS, the Development Advisory Committee (DAC) conducted four public meetings on September 30, 2014, November 12, 2014, January 14, 2015, and June 17, 2015;

WHEREAS, the City and Sargent Town Planning conducted one environmental scoping meeting on January 7, 2016 and four community workshops on November 19, 2014, January 21, 2015, August 28, 2017, and September 15, 2017;

WHEREAS, the Planning and Preservation Commission also conducted discussions at two of its regularly scheduled meetings on August 4, 2015, October 6, 2015;

WHEREAS, Metro's deadline to complete the project has been extended from the original date of June 30, 2016 to a new deadline date of March 31, 2018;

WHEREAS, a Final EIR was prepared by the Consultant pursuant to CEQA assessing the potential environmental impacts that may result from the approval of the proposed amendments to the General Plan and Zoning Ordinance and adoption of the proposed San Fernando Corridors Specific Plan SP-5;

WHEREAS, on November 7, 2017, the Planning and Preservation Commission certified Resolution No. 2017-008, approving and recommending that the City Council approve and adopt the Final Environmental Impact Report SCH NO. 2015121088 (EIR), amendments to the General Plan text and map, amendments to the Zoning Ordinance text and maps, and the proposed San Fernando Corridors Specific Plan SP-5;

WHEREAS, on November 10, 2017, a Public Notice was published in the *Los Angeles Daily News*, advertising the public hearing by the San Fernando City Council concerning the consideration for approval of the San Fernando Corridors Specific Plan SP-5, and related amendments to the General Plan text and map, to the Zoning Ordinance text and map, and the Final EIR;

WHEREAS, on November 20, 2017, the San Fernando City Council conducted a duly noticed Public Hearing on the proposed San Fernando Corridors Specific Plan SP-5, and related amendments to the General Plan text and map, to the Zoning Ordinance text and map, and the Final EIR;

WHEREAS, on November 20, 2017, the San Fernando City Council deliberated and considered evidence and comments on the Final Environmental Impact Report SCH NO. 2015121088 (EIR), amendments to the General Plan text and map, amendments to the Zoning Ordinance text and map, and the proposed San Fernando Corridors Specific Plan SP-5; and

WHEREAS, on November 20, 2017, the San Fernando City Council deliberated and considered evidence and comments on the proposed San Fernando Corridors Specific Plan SP-5 and discussed replacing the current San Fernando Corridors Specific Plan SP-4, in its entirety, with the final adoption of the proposed San Fernando Corridors Specific Plan SP-5, upon its second reading.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY ORDAIN AS FOLLOWS:

SECTION 1. FINDINGS

A. The facts set forth in the recitals above are true and correct and incorporated herein by reference.

B. On November 20, 2017, the City Council adopted Resolution No. 7826, which certifies the Final EIR SCH No. 2015121088 for the proposed San Fernando Corridors Specific Plan SP-5, and amendments to the General Plan text and maps; and

C. The City Council approval of the Final EIR SCH No. 2015121088 was contingent upon the final adoption of the San Fernando Corridors Specific Plan SP-5.

D. On November 20, 2017, the City Council introduced for first reading, Ordinance No. 1621, to approve the proposed San Fernando Corridors Specific Plan SP-5 (Exhibit "A") and Zoning Ordinance text and map amendments (Exhibit "B").

SECTION 2. APPROVAL OF SPECIFIC PLAN AND DOCUMENTS. The City Council hereby introduces and approves the first reading of Ordinance No.1621, approving the proposed San Fernando Corridors Specific Plan SP-5, and amendments to the Zoning Ordinance text and map.

SECTION 3. CUSTODIAN OF RECORDS. The documents and materials that constitute the record of proceedings on which these findings are based are located in the City Clerk's Office of the City of San Fernando, located at 117 Macneil Street, San Fernando, CA 91340. The Custodian of records is the City Clerk.

SECTION 5. CERTIFICATION. The City Clerk shall certify to the passage and adoption of this resolution and enter it into the book of original Resolutions.

PASSED, APPROVED, AND ADOPTED by the City Council of the City of San Fernando at its regular meeting on this 20th day of November, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

APPROVED AS TO FORM:

Richard Padilla, Assistant City Attorney

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) ss
CITY OF SAN FERNANDO)

I, ELENA G. CHAVEZ, City Clerk of the City of San Fernando, do hereby certify that the foregoing Ordinance No.1621 was introduced at the regular meeting of the City Council held on the 20th day of November 2017, and carried by the following roll call vote:

AYES:

NOES:

ABSENT:

Elena G. Chávez, City Clerk

EXHIBIT “A”

CORRIDORS SPECIFIC PLAN SP-5

REFER TO EXHIBIT “D” OF
ATTACHMENT “A”

EXHIBIT "B"**DIVISION 12. - SP SPECIFIC PLAN ZONES****Sec. 106-666. - Intent and purpose.**

The SP specific plan zones are established to acknowledge and make reference to specific plans that have been adopted by the City of San Fernando pursuant to Government Code Section 65450 et seq. The specific plan zones provide for the flexible, creative and detailed planning and design of portions of the city which require a more comprehensive and coordinated approach than can be achieved through the conventional application of zoning regulations.

(Ord. No. 1562, § 10, 1-3-2005)

Sec. 106-667. - Municipal code and zoning map amendments.

- (a) The adoption of a specific plan shall be accompanied by an amendment to the San Fernando Municipal Code to acknowledge the approved specific plan and to establish a specific plan zone for the approved specific plan. A specific plan zone shall include the designation "SP," a reference number that corresponds to the particular specific plan, and the name of the specific plan.
- (b) The adoption of a specific plan shall be accompanied by an amendment to the City of San Fernando Zoning Map to change the zoning designation of the area covered by the specific plan to the designated specific plan zone. The specific plan zone shall be designated on the zoning map with the designation "SP," followed by a reference number that corresponds to the particular specific plan.

(Ord. No. 1562, § 10, 1-3-2005)

Sec. 106-668. - Specific plan zones and zoning map designations.

The following specific plans have been approved and are established as specific plan zones to be designated on the city zoning map as set forth herein.

- (1) Reserved.
- (2) Reserved.
- (3) Reserved.
- (4) Reserved.

(5) *San Fernando Corridors Specific Plan.* The San Fernando Corridors Specific Plan (SP-5), a copy of which is on file in the office of the city clerk, has been prepared to implement development strategies for the revitalization of the City's primary commercial corridors, namely Truman Street, San Fernando Road, Maclay Avenue, and First Street. The San Fernando Corridors Specific Plan (SP-5) covers approximately 150 acres commercial, and industrial zoned properties generally located along both sides of North/South Maclay Avenue, from Pico Street north of Truman Street to the northerly terminus of the city, along both sides of San Fernando Road and Truman Street from the city's

westerly boundary line at Hubbard Street to the city's easterly boundary line along Fox Street (including properties on the south side of Celis Street from Kalisher Street to Fox Street), and along both sides of First Street and the south side of Second Street between Hubbard and Hagar Streets. The area covered by the San Fernando Corridors Specific Plan shall be established as the ~~SP-4~~SP-5 San Fernando Corridors Specific Plan zone, and shall be designated on the zoning map by the designation "~~SP-4~~SP-5."

The purpose of the San Fernando Corridors Specific Plan (SP-5) is to put in place regulations and strategies to transform the City's downtown and adjacent supporting districts into attractive, livable, and economically vital places, while preserving existing, surrounding residential neighborhoods. The Plan sets forth a policy framework, development standards, design guidelines, and an implementation program that includes capital improvements. The development standards are mandatory, while the design guidelines are recommendations that provide potential applicants and the City with a basis for proposing and reviewing development applications.

The provisions of the San Fernando Corridors Specific Plan (SP-5) are applicable to all properties within the Specific Plan area. The regulations, development standards, and design guidelines as contained in the San Fernando Corridors Specific Plan shall apply in their entirety to the review of development proposals. All other provisions of the Municipal Code continue to apply within the Specific Plan area except as expressly provided to the contrary in the San Fernando Specific Plan. Where San Fernando Corridors Specific Plan development standards and design guidelines do not provide adequate direction, the City of San Fernando Municipal Code shall prevail.

(Ord. No. 1562, § 10, 1-3-2005)

Sec. 106-669. - Amendments to specific plans.

Unless otherwise indicated in the applicable specific plan, all amendments to an adopted specific plan shall require planning commission review and recommendation and city council review and approval in accordance with the procedures specified by law.

(Ord. No. 1562, § 10, 1-3-2005)

Sec. 106-670. - Consistency with specific plan.

No application for a discretionary approval, including, but not limited to, a conditional use permit, site plan review, tentative map or parcel map, may be approved, adopted or amended within an area covered by a specific plan, unless found to be consistent with the adopted specific plan.

(Ord. No. 1562, § 10, 1-3-2005)

Sec. 106-671. - Relationship of specific plan to San Fernando Zoning Ordinance.

The provisions of any adopted specific plan shall control over duplicative and conflicting provisions of the San Fernando Zoning Ordinance. In the event the adopted specific plan is silent as to a development standard or procedure, the provisions of the San Fernando Zoning Ordinance shall control.

(Ord. No. 1562, § 10, 1-3-2005)

Secs. 106-672—106-695. - Reserved.**Division 13. - Wireless Telecommunications Facilities****Subdivision II. - Development Requirements and Standards****Sec. 106-1352. - Location standards.**

- (a) Wireless telecommunications facilities shall be prohibited on all properties adjacent to residentially zoned properties and/or within 250 feet of any property line of a residentially zoned parcel, whichever provides the greater distance from residentially zoned properties.
- (b) Subject to the requirements for conditional use permits per section 106-1348, wireless telecommunications facilities are permissible only in the following areas:
 - (1) Properties within the ~~support commercial subdistrict~~ Workplace Flex District of the ~~SP-4~~ SP-5 (San Fernando Corridors Specific Plan) zone.
 - (2) Properties within the M-1 (Limited Industrial) zone.
 - (3) Properties within the M-2 (Light Industrial) zone.
 - (4) Properties owned and utilized by the City of San Fernando for municipal purposes.

(Ord. No. 1569, § 3, 12-5-2005)

CITY OF SAN FERNANDO ZONING MAP

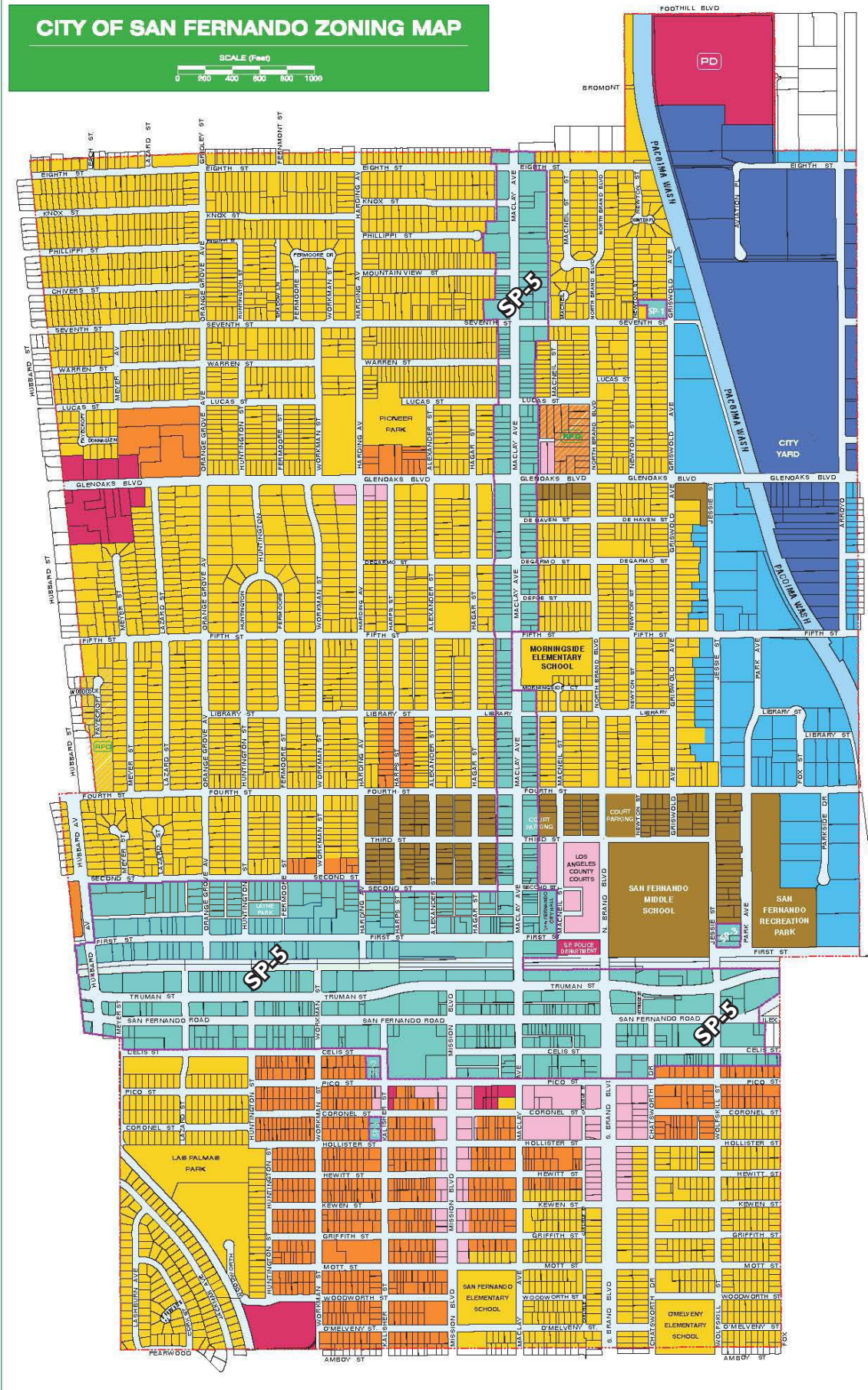
SCALE (Feet)
0 200 400 600 800 1000

San Fernando
HISTORIC & VISIONARY

COMMUNITY
DEVELOPMENT
DEPARTMENT

- R-1 Single Family Residential
- R-2 Multiple Family Dwelling
- R-3 Multiple Family
- C-1 Limited Commercial
- C-2 Commercial
- SC Service Commercial
- M-1 Limited Industrial
- M-2 Light Industrial
- SP-5 Corridors Specific Plan
- RPD Residential Planned Development
- PD Precise Development Overlay

City Boundary



RESOLUTION NO. 2017-008

RESOLUTION OF THE PLANNING & PRESERVATION COMMISSION OF THE CITY OF SAN FERNANDO RECOMMENDING THAT THE CITY COUNCIL ADOPT THE SAN FERNANDO CORRIDORS SPECIFIC PLAN IN CONJUNCTION WITH THE AMENDMENT OF THE CITY OF SAN FERNANDO GENERAL PLAN TEXT, GENERAL PLAN MAP, AND THE AMENDMENT OF THE SAN FERNANDO MUNICIPAL CODE AND ZONING MAP. IN ADDITION, THE PLANNING & PRESERVATION COMMISSION APPROVES AND RECOMMENDS CERTIFICATION OF THE ENVIRONMENTAL IMPACT REPORT AND ASSOCIATED MITIGATION MONITORING PROGRAM.

WHEREAS, on February 28, 2013, Metro awarded a \$282,392 planning grant to the City of San Fernando for the completion of a comprehensive update to the existing SP-4 San Fernando Corridors Specific Plan, to make the necessary amendments to the General Plan text, Zoning Ordinance text, and related maps and to prepare an Environmental Impact Report (EIR). The City is to contribute an additional match of \$13,306 (\$6,306 in-kind, such as staff-time and \$7,000 funds).

WHEREAS, on May 6, 2013, the City Council approved and entered into an agreement with Metro accepting a \$282,392 grant to prepare the San Fernando TOD Overlay Zone project.

WHEREAS, on May 19, 2014, the City Council approved a Professional Services Agreement with Sargent Town Planning (City Contract No. 1745) to complete the project from a general solicitation to qualified consultants.

WHEREAS, Metro's grant deadline to complete the project has been extended from the original date of June 30, 2016 to a new deadline date of March 31, 2018.

WHEREAS, throughout this process, several public meetings, and workshops have been conducted with residents, merchants, and local stakeholders. The Consultant has conducted several individual and small group meetings with local stakeholders in September and October 2014 and again in September 2015. The proposed specific plan amendment was also discussed by:

- The Development Advisory Committee (DAC) conducted four public meetings on September 30, 2014, November 12, 2014, January 14, 2015, and June 17, 2015.
- The City and Consultant conducted one environmental scoping meeting on January 7, 2016 and four community workshops on November 19, 2014, January 21, 2015, August 28, 2017, and September 15, 2017.
- The Planning and Preservation Commission also conducted discussions at two of its regularly scheduled meetings on August 4, 2015, October 6, 2015.

WHEREAS, the Planning & Preservation Commission conducted a public hearing held on the proposed San Fernando Corridors Specific Plan, General Plan Text and General Plan Map Amendments, and Zoning Code and Map Amendments and Draft Environmental Impact Report SCH No. 2015121088 on November 7, 2017 at 6:30 p.m., and proper public notice was duly given as shown in Exhibit "A"; and,

City of San Fernando Planning and Preservation Commission
Resolution No. 2017-008
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WHEREAS, the Planning & Preservation Commission's findings and recommendations for approval are memorialized in writing in the form of Planning & Preservation Commission Resolution 2017-008 on November 7, 2017.

WHEREAS, the City of San Fernando (c/o Community Development Department), hereinafter referred to as "Applicant," has submitted a proposal to approve the San Fernando Corridors Specific Plan in conjunction with amendments to the General Plan Text and General Plan Map, and amendments of the City of San Fernando Municipal Code and Zoning Map in order to implement development strategies for the revitalization of approximately 150 acres of property along Maclay Avenue, San Fernando Road, Truman Street, and First Street; properties fronting segments of Second Street between Hubbard Avenue and Macneil Street; and properties fronting Celis Street and Pico Street between Kalisher Street and Fox Street as shown in Exhibit "B": San Fernando Corridors Land Use Map, henceforth referred to as the "Project".

WHEREAS, the Project contains the goals, objectives, and policies to guide the community's vision for the revitalization of the North Maclay Avenue, San Fernando Road, Truman Street, and First Street corridors. The Specific Plan document includes a detailed statement of the community's vision for the future of the corridor areas as well as the revitalization strategy and urban design principles to be used in achieving that vision. It provides specific land use regulations, development standards and design guidelines that would apply to new development in the corridor areas, as well as a program of public improvements to the street and sidewalks.

WHEREAS, the Project would include modifying and renaming the District and Sub-District boundaries of the existing San Fernando Corridors Specific Plan (SP-4) and the General Plan and Land Use Designations of the expanded Project Area from the existing Industrial and High Density Residential Land Use Designation with District land use designations provided as part of the San Fernando Corridors Specific Plan (SP-5) in a manner consistent with the goals and objectives of the General Plan Land Use Element, Housing Element, and Circulation Element.

WHEREAS, the San Fernando Corridors Specific Plan and Zoning Map Amendment would establish the San Fernando Corridors Specific Plan (SP-5) as the primary zoning regulations for the Project Area, amending the current City Zoning Ordinance and Zoning Map. The properties within the Project Area of the Specific Plan would establish six primary districts: the Maclay, Downtown, Mixed-Use Corridor, Auto Commercial, Workplace Flex, and General Neighborhood districts.

WHEREAS, following the conclusion of a November 7, 2017 duly noticed public hearing on the matter, the Planning & Preservation Commission finds the Project to be in conformance with the General Plan, and recommends that the City Council approve the San Fernando Corridors Specific Plan in conjunction with the corresponding General Plan Text and General Plan Map Amendments, and the required Zoning Code and Zoning Map Amendments and Environmental Impact Report SCH No. 2015121088.

WHEREAS, pursuant to the California Environmental Quality Act (CEQA) and the City of San Fernando CEQA Guidelines, the City of San Fernando as the Lead Agency overseeing the environmental review for the proposed specific plan has prepared a Draft Environmental Impact Report SCH No. 2105121088 to determine the nature and extent of the environmental impacts associated with the

City of San Fernando Planning and Preservation Commission

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proposed redevelopment of the North Maclay Avenue, San Fernando Road, Truman Street, and First Street corridors.

WHEREAS, the Draft Environmental Impact Report SCH No. 2015121088 prepared for the project has identified impacts associated with future development allowed by the project and that all these impacts can, pursuant to CEQA, be reduced to “less than significant” levels with implementation of the appropriate mitigation measures administered through the associated Mitigation Monitoring Program.

WHEREAS, the Draft Environmental Impact Report SCH No. 2015121088, dated August 2017 that was circulated for public comment is hereby entered into the records along with the attached “Final” Environmental Impact Report SCH No. 2015121088, dated October 2017, which addresses the comments and concerns received. Any and all references to the Environmental Impact Report shall be in reference to the combined two documents, both of which are available for public review on the City’s website and at City Hall.

WHEREAS, the proposed amendment to the San Fernando Municipal Code is consistent with the General Plan of the City of San Fernando by providing for the following: Amendment of the San Fernando Municipal Code and Zoning Map to provide the San Fernando Corridors Specific Plan’s development standards and design guidelines as the regulatory framework to guide public and private actions in the Project Area (i.e., guiding site development and building architecture for new construction and/or rehabilitation) that are designed to support future development of the Project Area within the City of San Fernando, in a manner consistent with the General Plan and without adversely impacting the public health, safety, convenience, and welfare.

NOW, THEREFORE, the Planning & Preservation Commission of the City of San Fernando hereby resolves as follows:

SECTION 1. That the Planning & Preservation Commission determined that the proposed text revisions to the San Fernando Municipal Code are based the findings of fact as discussed below:

- **The proposed amendment is consistent with the objectives, policies, general land uses and programs of the City’s General Plan.**

It is the Planning & Preservation Commission’s assessment that the amendment of the San Fernando Zoning Code and Zoning Map to establish the Specific Plan’s development strategies (i.e., development standards and design guidelines) that facilitate public and private development within the San Fernando Corridors Specific Plan Project Area is consistent with the objectives, policies, and general land use and programs of the San Fernando General Plan. The Project is consistent with the General Plan’s goals that seek to retain the character of the community, promote the economic viability of the commercial areas, attract new commercial activities within the downtown area, while conserving the surrounding residential neighborhoods that help add to the community’s identity.

- **The adoption of the proposed amendment would not be detrimental to the public interest, health, safety, convenience or welfare.**

City of San Fernando Planning and Preservation Commission

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The proposed San Fernando Zoning Code and Zoning Map Amendments associated with the Project will help improve the public interest, health, safety, convenience, and welfare by improving the streetscape along all six Districts, improving access to transit, and increasing commercial and residential opportunities within the Project Area. These amendments will implement the San Fernando Corridors Specific Plan so as to guide public and private physical improvements in a manner that encourages a higher quality of design that is consistent with the scale and character of the Districts, and improves pedestrian connectivity and safety through the clustering of commercial, residential, and service oriented uses, and reduces amount and speed of vehicle traffic through new street and landscaping improvements within the public right-of-ways.

SECTION 2. The Planning & Preservation Commission finds that all of the facts set forth in this Resolution are true and correct.

SECTION 3. On November 7, 2017, the Planning & Preservation Commission held a duly noticed public hearing to consider the proposed application for the Project filed by the Applicant and the findings and recommendations made by the Planning & Preservation Commission. Evidence, both written and oral, was presented at said hearing, including but not limited to all written reports of City staff and the City's environmental consultant, the verbal testimony of City staff at the public hearing and the verbal and written comments submitted by interested members of the public.

A. The public hearing afforded opportunities for public testimony and comments on the Project.

B. Notice of the hearing was given pursuant to San Fernando Municipal Code Section 78-69 and Section 106-72, and in compliance with Government Code Sections 65090 and 65091, a notice of public hearing for the San Fernando Corridors Specific Plan, Zone Code and Zoning Map Amendments, and General Plan Text and General Plan Map Amendments was advertised in the San Fernando Valley Sun on October 26, 2017 (a local paper of general circulation), ten (10) days prior to the scheduled public hearing before the Planning & Preservation Commission. In addition, the City mailed notice of the hearing to all properties within a 500 foot radius of the Project Area on October 26, 2017.

SECTION 4. Based upon substantial evidence presented to the Planning & Preservation Commission during the November 7, 2017 public hearing, including public testimony, written materials and written and oral staff reports, with regard to the Project, the Planning & Preservation Commission concurred with the staff's determination that the Project will not have a significant adverse impact on the environment with the identified mitigation measures incorporated as part of the Environmental Impact Report and herein referred to as the "EIR" and subsequently adopted findings to that effect on November 7, 2017.

SECTION 5. Based upon the evidence presented with regard to the proposed San Fernando Corridors Specific Plan, the Planning & Preservation Commission hereby:

A. Finds that the San Fernando Corridors Specific Plan (SP-5) complies with the requirements of the California Government Sections 65451(a) through 65451(b).

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B. Finds that the San Fernando Corridors Specific Plan (SP-5) contains diagrams and text that illustrates the development standards and design guidelines that will guide public and private development within the San Fernando Corridors Specific Plan Project Area including architectural design, densities, floor area ratios, etc., while providing public right-of-way improvements along MacLay Avenue, San Fernando Road, Truman Street, and First Street, and specified segments of San Fernando Mission Boulevard and Hubbard Avenue in compliance with the goals and objectives of the General Plan's Land Use, Housing, and Circulation Elements.

C. Finds that the San Fernando Corridors Specific Plan (SP-5) contains component plans and textual references for the provisions of street, sewer, water, solid waste and other applicable infrastructure improvements to be undertaken by the Applicant to support the proposed Project.

D. Finds that the San Fernando Corridors Specific Plan (SP-5) contains standards and guidelines by which the proposed development will proceed including specific provisions establishing the land use distribution, permitted uses, on-site parking facilities, landscaping requirements, architectural improvements, minimum requirements for the rehabilitation of the existing commercial, industrial, and residential structures in compliance with applicable City of San Fernando building codes and state and federal regulations.

E. Finds that the San Fernando Corridors Specific Plan (SP-5) contains implementation measures, phasing plans, infrastructure plans, and identifies financing mechanisms/opportunities that will enable the City of San Fernando to ensure that the Project will be completed in compliance with the plans submitted for Planning & Preservation Commission review and subsequent review and approval by the City Council.

F. Finds that the San Fernando Corridors Specific Plan (SP-5) contains a detailed discussion of the Project's conformance with the adopted General Plan, including consistency with the goals, policies of the Land Use Element, Housing Element, and Circulation Element.

G. Finds that the San Fernando Corridors Specific Plan (SP-5) allows for the Project's development of higher density residential, commercial, and mixed-use land uses that are consistent with the General Plan within the Specific Plan Project Area that are adequate in size, shape, topography and location to accommodate the proposed mixture of land uses.

H. Finds that the following uses are to be added as "Permitted Uses" within the respective Districts of the San Fernando Corridors Specific Plan SP-5 as, indicated in the table below.

USE	DISTRICTS			
	Downtown	Mixed-Use Corridor	Auto Commercial	Workplace Flex
Maker Space	-	P	P	P
Market Hall/Food Hall	P	P	P	-
Incubator Business	P	P	P	P
Bowling Alley	P	p	P	-

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SECTION 6. Based upon the evidence and all other applicable information presented, the Planning & Preservation Commission finds that the proposed amendment of the General Plan and General Plan Map is appropriate for the following reasons:

A. Changing the General Plan Land Use Designation for the expanded Project Area from “Industrial” and “High density Residential” to the corresponding District Designations as proposed as part of the San Fernando Corridors Specific Plan will permit the types of residential, commercial, and mixed-use development projects that are required for the Project.

B. Approving the General Plan Text Amendment would revise the General Plan land use designation descriptions to provide consistency between the Specific Plan and the General Plan by establishing development strategies that provide for land use densities and development standards that support the mixture of land uses proposed for the Maclay, Downtown, Mixed-Use Corridor, Auto Commercial, Workplace Flex, and General Neighborhood Districts.

C. Changing the land use designation within the San Fernando Corridors Specific Plan Project Area will not adversely impact or be detrimental to the LDR (Low Density Residential, MDR (Medium Density Residential), HDR (High Density Residential), and Commercial land uses adjacent to the Project Area.

D. Amending the General Plan Text and General Plan Map to accommodate the Specific Plan goals, allows for the clustering of commercial, higher density residential, and mixed-used development within the Project Area in a manner that “retains the small town character of San Fernando, promotes the economic viability of commercial areas, and maintains an identity that is distinct from surrounding communities,” in a manner consistent with San Fernando General Plan Land Use Goals I-III, Pg. IV-6. Through the Specific Plan’s development standards and design guidelines, the City seeks to “ensure high quality-development and design”, which promotes façade improvements and new buildings that adhere to the scale and character of the district. These development strategies recommend a “stylistic architectural and landscape ‘language’ that reflects and relates to the architectural histories of the City, however, they do not impose one historic style, but instead, encourage a variety of styles, in keeping with the diverse eclectic character of the City.”

E. Amending the General Plan Text and General Plan Map in conjunction with the San Fernando Corridors Specific Plan that would allow the City to meet its Housing Element policies by increasing the residential development potential attributed to the Specific Plan density requirements that will provide for affordable housing opportunities for all economic segments of the community within the Project Area. The residential development opportunities created by the San Fernando Corridors Specific Plan are consistent with the San Fernando Housing Element goals and objectives that seek to promote the adequate provisions of affordable housing in a range of unit types and sizes to meet the future needs of San Fernando’s residents.

SECTION 7. Based upon the evidence and all other applicable information presented, the Planning & Preservation Commission finds that the Zoning Code Amendment and Zoning Map Amendment, which allows for the existing zoning classification for the existing Specific Plan Area (SP-4) and the Expanded Project Area to change to the SP-5 zoning classification, is appropriate for the following reasons:

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A. Changing the designation of the Project Area to the San Fernando Corridors Specific Plan Districts is consistent with the goals, objectives, policies, general plan land uses and programs of the City of San Fernando General Plan as amended in that the proposed Zoning Code and Zoning Map amendments will be consistent with the Specific Plan for the Project;

B. Changing the designation of the Project Area to the San Fernando Corridors Specific Plan Districts is consistent with the General Plan's goals, which seek to retain the small town character of the community, promote the economic viability of the commercial areas, attract new commercial activities within the downtown area, while conserving the surrounding residential neighborhoods that help add to the community's unique identity. In addition, the zoning code and zoning map amendments will help improve the public interest, health, safety, convenience, and welfare by improving the streetscape along all six Districts, and increasing commercial and residential opportunities within the Planning Area. These amendments will implement the Specific Plan so as to guide public and private physical improvements in a manner that encourages a higher quality of design that is consistent with the scale and character of the Districts, and improves pedestrian connectivity and safety through the clustering of commercial, residential, and service oriented uses, and reduces the amount and speed of vehicle traffic through new street and landscaping improvements within the public rights-of-way.

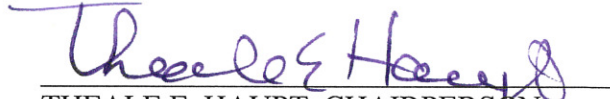
C. The adoption of the proposed zoning map amendment would not be detrimental to the public interest, health, safety, convenience or welfare in that the new zoning classification and corresponding zoning ordinance would include specific development standards and design guidelines as part of the Specific Plan that would ensure that the Project is compatible with residential neighborhoods that surround the Project Area.

SECTION 8. Based on the foregoing, the Planning & Preservation Commission has reviewed and considered the environmental information contained in the Environmental Impact Report SCH No. 2015121088 (EIR) and does hereby recommend that the City Council approve and certify the attached EIR, as shown in Exhibit "C" and to adopt the attached amendments to the General Plan text and map, as shown in Exhibit "D", contingent upon and subject to the approval of the San Fernando Corridors Specific Plan (SP-5), as shown in Exhibit "E", and Zoning Code and Zoning Map Amendments, as shown in Exhibit "F", pursuant to Planning & Preservation Commission Resolution No. 2017-008. The Draft Environmental Impact Report SCH No. 2015121088, dated August 2017 that was circulated for public comment is hereby entered into the records along with the attached "Final" Environmental Impact Report SCH No. 2015121088, dated October 2017, which addresses the comments and concerns received. Any and all references to the Environmental Impact Report shall be in reference to the combined two documents, both of which are available for public review on the City's website and at City Hall.

SECTION 9. The Secretary of the Planning & Preservation Commission shall certify to the passage, approval, and adoption of this resolution, and shall cause this resolution and certification to be entered in the Book of Resolutions of the Planning & Preservation Commission of the City.


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PASSED, APPROVED AND ADOPTED this 7th day of November 2017.



THEALE E. HAUPT, CHAIRPERSON

ATTEST:



SECRETARY TO THE PLANNING &
PRESERVATION COMMISSION

STATE OF CALIFORNIA
COUNTY OF LOS ANGELES
CITY OF SAN FERNANDO)


I, Jack Wong, Secretary to the Planning & Preservation Commission of the City of San Fernando, do hereby certify that the foregoing Resolution No 2017-008 was duly adopted by the Planning & Preservation Commission and signed by the Chairperson of said City at a meeting held on the 7th day of November 2017; and that the same was passed by the following vote, to wit:

AYES: 3 – Y. Mejia, A. Durham, and T. Haupt

NOES: 0 - None

ABSENT: 1 – J. Perez-Helliwell

ABSTAIN: 1 – A. Montes



SECRETARY TO THE PLANNING &
PRESERVATION COMMISSION

AGENDA REPORT

To: Planning and Preservation Commission Chairperson Haupt and Commissioners

From: Jack Wong, Interim Community Development Director

Date: November 7, 2017

Subject: Consideration of a Resolution approving and recommending the San Fernando Corridors Specific Plan SP-5, amendments to the General Plan text and map, amendments to the Zoning Ordinance text and map, and Environmental Impact Report for approval by the San Fernando City Council

RECOMMENDATION:

It is recommended that the Planning and Preservation Commission:

- a. Open the Public Hearing and receive presentation of City staff;
- b. Pose questions to City staff;
- c. Receive Public testimony;
- d. Pose further questions to City staff based on public testimony;
- e. Close the Public Hearing and commence deliberations on the proposed Corridors Specific Plan SP-5 and related EIR and General Plan and Zoning Ordinance text changes and maps;
- f. Approve the attached Planning and Preservation Commission Resolution 2017-008 (Attachment No.1), which recommends that the City Council certify the Environmental Impact Report SCH No. 2015121088 (EIR) pursuant to the California Environmental Quality Act (CEQA) and approve:
 - a. General Plan/Map Amendments
 - b. San Fernando Corridors Specific Plan SP-5
 - c. Zoning Ordinance/Map Amendment

BACKGROUND:

1. On February 28, 2013, Metro awarded a \$282,392 planning grant to the City of San Fernando for the completion of a comprehensive update to the existing SP-4 San Fernando Corridors Specific Plan, to make the necessary amendments to the General Plan text, Zoning

Consideration of San Fernando Corridors Specific Plan (SP-5) and Related General Plan, Zoning Ordinance, and map amendments, and EIR DocumentPage 2 of 9

Ordinance text, and related maps and to prepare an Environmental Impact Report (EIR) pursuant to CEQA. The City agreed to contribute a match of \$13,306 (\$6,306 in-kind, such as staff-time and \$7,000 funds).

2. The firm of Sargent Town Planning (Consultant) was selected to complete the project from a general solicitation to qualified consultants. A Professional Services Agreement (PSA) was signed on May 19, 2014.
3. Metro's deadline to complete the project has been extended from the original date of June 30, 2016 to a new deadline date of March 31, 2018.
4. Throughout this process, several public meetings, and workshops have been conducted with residents, merchants, and local stakeholders. Sargent Town Planning, the consultant who is preparing the specific plan amendment, conducted several individual and small group meetings with local stakeholders in September and October 2014 and again in September 2015. The proposed specific plan amendment was also discussed by the following:
 - a. The Development Advisory Committee (DAC) conducted four public meetings on September 30, 2014, November 12, 2014, January 14, 2015, and June 17, 2015.
 - b. The Planning and Preservation Commission also conducted discussions at two of its regularly scheduled meetings on August 4, 2015, October 6, 2015.
 - c. The City and Sargent Town Planning conducted one environmental scoping meeting on January 7, 2016 and four community workshops on November 19, 2014, January 21, 2015, August 28, 2017, and September 15, 2017.
5. There have been two major phases during the development of the project. The first phase can be characterized as the initial data collection and development of concepts and strategies. The term of the first phase was between June 2014 and May 2015. During this initial phase, the Consultant conducted extensive data collection and solicited community participation, which resulted in the development of preliminary concepts and early place making strategies. These ideas were vetted over the next five months, between June 2015 and October 2015, with City staff and the Planning and Preservation Commission. The term of the second phase was from October 2015 to present day, wherein the Consultant initiated revisions to the Specific Plan in recognition of community input and prepared the Draft Environmental Impact Report.
6. Community input and comments were seriously considered and substantially shaped the final product, in terms of suggested uses and their locations, building intensities, urban aesthetics, and place making strategies.

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7. Public Notice for this public hearing was published in The San Fernando Sun newspaper on October 26, 2017 as shown in Attachment A.

ANALYSIS:

The San Fernando Corridors Specific Plan is a “living” document that is meant to be updated pursuant to changing circumstances. The original San Fernando Corridors Specific Plan was adopted by Ordinance #1562 in January, 2005 as SP-4 (SP-4 Plan). Since then, the voters of Los Angeles County approved Measure M, which provides the necessary funding for one of the planned transit improvements identified in the East San Fernando Valley Transit Corridor Plan, which traverses San Fernando’s downtown area, terminating at the Sylmar/San Fernando Metrolink Station. The current proposed San Fernando Corridors Specific Plan (SP-5 Plan) identifies strategies that recognize Metro’s public transportation project that is projected to start in 2019 and to update the policies and strategies contained in the current SP-4 Plan. As such, the SP-5 Plan will rescind and replace the SP-4 Plan, in its entirety.

The proposed SP-5 Plan establishes planning principles, land use policies, development standards, and design guidelines for public improvements and private development within the specific plan area. Some of the more notable changes proposed in the SP-5 Plan are:

- Making all residential projects, which are currently permitted administratively by right of zone, subject to receiving a Conditional Use Permit from the Planning and Preservation Commission
- Reducing the building heights and number of floors for residential projects within the planning area
- Limiting the eligible parcels for residential projects within the Downtown District
- Reducing the allowable residential density
- Expanding the areas where commercial and retail uses are allowed
- Requiring new multi-family buildings facing Second Street to be neighborhood-friendly in their design and scale along the single family/multi-family zone boundary
- Updating the specific plan to promote economic development by:
 - Changing the permitting requirements of many commercial uses that are conditionally permitted to permitted by right;
 - Changing permitting requirements for bona fide eating establishments with ancillary alcoholic beverages for on-site consumption (up to Type 47 liquor license) from a conditional use permit to a by-right permit.
 - Allowing more commercial and restaurant uses within the Auto Commercial and Workplace Flex districts.

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- Allowing some light industrial uses within the Mixed-Use Corridor District, subject to frontage requirements.
- Making the Development Standards more user-friendly
- Ensuring that Metro's East San Fernando Valley Transit Corridor transportation improvements are introduced in the best interest of the City

To effectuate these changes, State law requires that the City's General Plan, Specific Plan and Zoning Ordinance and maps are internally consistent i.e., no internal contradictions. Therefore, as part of the overall process, the Consultant has prepared the appurtenant amendments to the General Plan and Zoning Ordinance and related maps. These documents are also attached to this staff report for your consideration.

ENVIRONMENTAL REVIEW:

The City of San Fernando is the designated Lead Agency overseeing the environmental review for the proposed SP-5 Plan. As the Lead Agency, the City of San Fernando directed the preparation of an Environmental Impact Report, which identified potential impacts associated with future development allowed by the project. On December 22, 2015, the City circulated a Notice of Preparation (NOP) for review and comment, through January 20, 2016, by the public, responsible agencies, and reviewing agencies. On January 7, 2016, a Scoping Meeting was conducted to receive public input and comment and on August 10, 2017, the Draft Environmental Impact Report SCH No. 2015121088 (EIR) was released, initiating a 45-day public review period. A Notice of Completion (NOC) of the Draft EIR was also provided on August 10, 2017 to the Governor's Office of Planning and Research State Clearinghouse for environmental review documents, along with copies for review by state agencies. A Notice of Availability (NOA) of the Draft EIR for review and copies of the Draft EIR were also sent to County Clerk on August 10, 2017 and to responsible agencies, agencies that had commented on the NOP, and all other interested parties that had requested notice and copies of the Draft EIR.

The City and Consultant conducted an environmental scoping meeting on January 7, 2016 and discussed the Draft EIR at two community meetings on August 28, 2017 and September 15, 2017.

The significant impacts identified were noise during construction, the unearthing of subsurface cultural resources during construction, and decreases in intersection performance due to automobile traffic. All these impacts can be mitigated with the appropriate mitigation measures as follows:

- **Noise during construction.** The Project will allow future development within the Specific Plan Area. Construction within an existing community, particularly where potential development sites are close to existing residences, has the potential to

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generate construction noise levels that exceed community standards. As such, the Project could result in potential significant noise impacts during construction. To address the potential construction noise:

- Prior to the issuance of any demolition, grading or building permits with the Specific Plan area, specifications shall be prepared that identify requirements regarding attenuation of noise from construction vehicles and activities, including notification of surrounding owners and tenants of the proposed construction schedule and activities, posting of project hours and contractor contact information at all construction entrances, submittal of a material haul route plan to the City, staging of noisy equipment away from sensitive uses, and implementation of noise attenuation measures to the extent feasible.
- **Unearthing of subsurface cultural resources during construction.** Due to the history of the area, the potential exists for cultural resources, and specifically Tribal Cultural. As a result, future development associated with the Project that involved site excavation could have an impact on subsurface cultural artifacts within the Specific Plan Area. As the significance of any subsurface artifacts is currently unknown, this impact is considered potentially significant. To address the potential subsurface cultural resource impacts:
 - The project proponent of any proposed development within the Specific Plan area that involves excavation, or similar ground disturbing activity, shall retain a certified Native American Monitor, procured by the Fernandeano Tataviam Band of Mission Indians ("the Tribe"), for the duration of construction-related ground disturbance activities. The Monitor shall complete monitoring logs on a daily basis that document ground disturbing activities, locations, soil, and any cultural materials identified. On-site Native American monitoring shall end when the project site grading and excavation activities are completed, or when the Tribe's Tribal Historic and Cultural Preservation Officer (THCPO) has indicated, in writing, that the project will no longer need to obtain Native American monitoring services.
- **Future development associated with the Project could increase vehicle traffic within the Specific Plan Area.** Traffic analysis of assumed future traffic identified intersections where the level of performance could be reduced to an unacceptable level. As such, the Project could have a significant impact due to increase vehicular traffic. To address potential traffic impacts, the City shall:
 - Create an eastbound right-turn lane on First Street. This improvement may require the removal of one parking space between the commercial driveways on the south side of First Street west of Maclay Avenue. Removing the one parking

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space would allow for a right turn of 150 feet in length. If additional right turn storage is required, then additional parking spaces on the south side of First Street may need to be removed. This improvement will also fit within the existing curbs, not requiring any street widening.

- Implement a signal synchronization program, coordinating traffic signal systems within the Downtown District of the Specific Plan area, specifically along Maclay Avenue, Hubbard Street, Truman Street, and San Fernando Road.

If the following mitigation measures are implemented, potential impacts would be reduced to a less than significant level. The Draft Environmental Impact Report SCH No. 2015121088, dated August 2017 that was circulated for public comment is hereby entered into the records along with the attached "Final" Environmental Impact Report SCH No. 2015121088, dated October 2017, which addresses the comments and concerns received. Any and all references to the Environmental Impact Report shall be in reference to the two documents in combination, both of which are available for public review on the City's website and at City Hall.

RESPONSE TO CORRESPONDENCE FROM MS. MARY MENDOZA RECEIVED ON OCTOBER 25, 2017 (ATTACHMENT NO. 2):**Background**

This project was funded by a grant from Metro to update the zoning in the project area to meet Metro's Transit Oriented Development ("TOD") Planning Grant Program guidelines of promoting development that takes "advantage of proximity and access to public transit through appropriate density, reduced reliance upon private automobiles, and enhanced walkability." Key components of meeting Metro's grant objectives include: 1) amending the existing zoning to allow residential uses near the Metrolink Station where residential use were not allowed, 2) increasing residential densities between the San Fernando Mall and the Metrolink Station to promote residential development near the Metrolink Station and near the proposed stops of Metro's East San Fernando Valley Transit Corridor transit initiative, 3) creating a stronger pedestrian link between Maclay Avenue and the City Center along First Street to the Metrolink Station, permitting residential uses along First Street due to the proximity to the Metrolink Station and Maclay Avenue; and 4) updating the street standards to accommodate Metro's East San Fernando Valley Transit corridor, while continuing to calm traffic in order to create a pedestrian-oriented, multi-modal environment. These strategies resulted in a direction that may increase the area and amount of housing that could be built within the Specific Plan area.

However, as the public process unfolded, it became clear that certain segments of the community believed the above-mentioned approach was not in the best interest of San Fernando. Based upon the community's input at the public workshops, City staff and the consultant team adjusted the approach to meet the communities concerns, while continuing to strive to be consistent with the objectives of Metro's grant.

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Ms. Mendoza presented a letter of concern to the City Clerk on October 25, 2017, which was past the deadline for receipt of written comments on the CEQA document. The letter was signed by 12 community residents. However, staff wishes to acknowledge and address the community concerns below:

a. *Eliminate the Mixed-Use Overlay: No Residential in the Downtown and Historic Core.*

The current SP-4 Plan does not provide a maximum density for residential uses within the San Fernando Mall and City Center Sub-Districts, while capping the maximum density within the Mixed-Use Transition Sub-District at 45 du/acre. In preparing the proposed SP-5 Plan, the locations and amount of housing that can be built within the proposed Downtown District were significantly reduced.

The current SP-4 Plan, with the exception of parcels located within 200 feet of the railroad right-of-way, allows residential uses within the entire area covered by the proposed SP-5 Downtown District: on upper floors throughout the entire San Fernando Mall and City Center Sub-Districts and on both ground floors and upper floors within the portions of the Mixed-Use Transition Sub-District located south of the San Fernando Mall between San Fernando Mission Boulevard and Brand Boulevard.

The proposed SP-5 Downtown District only allows residential uses on upper floors of parcels within the Downtown Residential Overlay, which applies only to select parcels that do not front the San Fernando Mall, excepting the two through-parcels located on the south side of the San Fernando Mall at San Fernando Mission Boulevard. In addition, SP-5 puts in place a maximum residential density of 50 du/acre for all parcels within the proposed Downtown Residential Overlay.

Staff believes it is important to retain residential uses within the Downtown District due to its proximity to the proposed East San Fernando Valley Transit Corridor station, at Maclay and the railroad right-of-way, and also to help create an active, vibrant downtown environment where people do not need to rely on cars to get around. In addition, many of the parcels within the Downtown Residential Overlay are controlled by the City. Furthermore, any proposed residential development that occurs on these and other parcels will be subject to a discretionary review by the Planning and Preservation Commission through the Conditional Use Permit process and any such action would be appealable to the City Council.

b. *Industrial Zoning: Limit Height in the Workplace Flex to 2 floors or 25-30 feet.* For the properties located south of the railroad right-of-way, the current SP-4 Plan allows building heights within the Support Commercial Zone of up to 3 stories/40 feet. For the portions north of the railroad right-of-way, current zoning allows heights of up to 45 feet within the M-1 and M-2 zones, but does not specify the number of stories. In the proposed SP-5 Plan, the SP-4's Support Commercial height standards (3 story/40 foot) for the parcels south of the railroad right-of-way were applied to the parcels along First

Consideration of San Fernando Corridors Specific Plan (SP-5) and Related General Plan, Zoning Ordinance, and map amendments, and EIR DocumentPage 8 of 9

Street. As mentioned above, the original intent was to allow housing along First Street due to its proximity to the existing Sylmar/San Fernando Metrolink Station and proposed Maclay LRT Station.

- c. ***Civic Center and Maclay from 1st to 4th: Prohibit Housing and Limit Building Heights to 2 Stories.*** The parcels along Maclay Street between First Street and Fourth Street are currently subject to SP-4's City Center Sub-District, which allows housing on upper floors of four-story (50 foot tall) buildings with an unlimited density. Per the proposed SP-5 Plan, housing would not be permitted on these parcels. In addition, SP-5 limits the height of buildings on these parcels at 3 stories/40 feet – the same height allowed within the adjacent Maclay District.
- d. ***R-3 Zoning-Neighborhood General: Reduce Density and height for entire site.*** The intention of this Specific Plan update was to mediate the transition between the housing already allowed by the existing R-3 zoning and the R-1 neighborhoods across Second Street, and not to downzone the existing zoning. Reduced height restrictions are also contemplated along Celis Street in the Mixed-Use Corridor District. As mentioned above, an important objective of this Plan is to provide housing within walking distance of transit.
- e. ***Maclay Avenue between 4th and 8th Streets: Lower Building Heights.*** Per the RFP and per Metro's grant, the scope of this Specific Plan update was focused on the portions of the plan area located south of Second Street, near the Metrolink Station and Metro's proposed East San Fernando Valley Transit Corridor stops. Accordingly, the standards of the Maclay District were not studied as part of this specific plan update, and were not discussed during the public process, and consequently are not part of this update effort.
- f. ***Open Space: add a Central Park or Plaza.*** Parks and open spaces are allowed in all the districts of the SP-5 Plan. In addition, Chapter 8 (Implementation) of the proposed SP-5 Plan contains an action item directing the City to look for upcoming site opportunities to create a public plaza space in the Downtown District as a gathering place for community and special events.

CONCLUSION:

The proposed San Fernando Corridors Specific Plan (SP-5) promotes sustainable development, livability, walkability within the downtown area and the community, and easy access to the Sylmar/San Fernando Metrolink Station and Metro's proposed transit stops. New improvements in the Plan Area will be of high architectural quality and scale and will be designed in keeping with the vision and character of the immediate district, establishing a genuine "sense of place" and contributing to the overall branding of the district.

Consideration of San Fernando Corridors Specific Plan (SP-5) and Related General Plan, Zoning Ordinance, and map amendments, and EIR Document

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Staff recommends that prior to approving the amendments to the General Plan, Zoning Ordinance and related maps, and proposed San Fernando Corridors Specific Plan (SP-5), the Planning and Preservation Commission hereby finds that:

- The Draft Environmental Impact Report (EIR) was completed in compliance with California Environmental Quality Act, and
- The Draft EIR was presented to the Planning and Preservation Commission for review and consideration, and the Planning and Preservation Commission has considered the information contained in the Draft EIR prior to considering approval of the proposed amendments to the General Plan, Zoning Ordinance, related maps, and proposed San Fernando Corridors Specific Plan (SP-5).

Staff recommends that the Planning and Preservation Commission approve the attached Resolution 2017-008 (Attachment No. 1), which accomplishes the following:

- Certifies the attached Environmental Impact Report, pursuant to the California Environmental Quality Act, and
- Approves the attached General Plan text and map amendments, and
- Approves the attached San Fernando Corridors Specific Plan (SP-5), and
- Approves the attached Zoning Ordinance text and map amendments, and
- Recommends the City Council to similarly approve and adopt the proposed San Fernando Corridors Specific Plan (SP-5), and related amendments to the General Plan, Zoning Ordinance, and maps and the Environmental Impact Report SCH No. 2015121088.

ATTACHMENTS:

1. Planning and Preservation Commission Resolution No. 2017-008
 - Exhibit "A" Public Hearing Notice
 - Exhibit "B" Project Area Map
 - Exhibit "C" Environmental Impact Report SCH No. 2015121088
 - Exhibit "D" General Plan Amendment and Land Use Map Amendment
 - Exhibit "E" San Fernando Corridors Specific Plan (SP-5)
 - Exhibit "F" Zoning Ordinance Amendment and Zoning Map Amendment
2. Ms. Mary Mendoza Letter received October 25, 2017

[CLICK HERE TO VISIT THE CITY'S WEBSITE TO VIEW ATTACHMENTS THAT WERE PRESENTED AT THE NOVEMBER 7, 2017 PLANNING & PRESERVATION COMMISSION MEETING](#)



**CITY OF SAN FERNANDO
PLANNING AND PRESERVATION COMMISSION**

**DRAFT MINUTES OF THE
November 7, 2017 MEETING
CITY HALL COUNCIL CHAMBER**

THE FOLLOWING MINUTES ARE A SUMMARY OF ACTIONS TAKEN BY THE PLANNING COMMISSION. AUDIO OF THE ACTUAL MEETING ARE AVAILABLE FOR LISTENING AT: www.ci.san-fernando.ca.us/commissionandboardmeetings/#ppc

**City Clerk, Elena Chavez Administered the Oath of Office to Aida Montes before the start of the meeting.*

CALL TO ORDER

The meeting was called to order by Theale Haupt at 6:30 p.m.

ROLL CALL

The following persons were recorded as present:

PRESENT:

Chairperson Theale Haupt, Vice-chair Alvin Durham, Commissioners Yvonne Mejia and Aida Montes

ABSENT:

Commissioner Jennifer Perez-Helliwell

ALSO PRESENT

Interim Community Development Director, City Attorney Richard Padilla, City Consultants David Sargent, Juan Gomez-Novy from Sargent Town Planning (STP), Ned Baldwin from Meridian Consultant, and Community Development Secretary Michelle De Santiago

APPROVAL OF AGENDA

Vice-chair Alvin Durham moved to approve the agenda of November 7, 2017, meeting. Seconded by Commissioner Y. Mejia, the motion carried with the following vote:

AYES:	A. Durham, Y. Mejia, T. Haupt and A. Montes
NOES:	None
ABSENT:	J. Perez-Helliwell
ABSTAIN:	None

PUBLIC STATEMENTS

Ursula Lazo – Selective Real Estate – Ms. Lazo indicated that there is a growing blight issue with the open trash enclosures in the San Fernando Mall area. She indicated that there is a

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reoccurring illegal dumping or overflowing trash that attracts illegal activity. She asked that the City consider installing chain link fencing to prevent unwanted activity.

CONSENT CALENDAR

Commissioner Y. Mejia moved to approve the minutes of the June 6, 2017 and the July 18, 2017, Planning and Preservation Commission Meetings. Seconded by A. Durham, the motion carried with the following vote:

AYES:	Y. Mejia, A. Durham, and T. Haupt
NOES:	None
ABSENT:	J. Perez-Helliwell
ABSTAIN:	A. Montes

CONTINUED BUSINESS

Land Use Determination 2017-001 – 1026 Griswold Avenue #B, San Fernando, CA 91340 – Land Use Determination recommended to be considered to the next regular meeting of December 5, 2017 to allow the applicant additional time to address the various requests made by the Planning and Preservation Commission.

Chairperson T. Haupt recommended that the Commission vote on the project and recommend that the applicant work with staff to provide the outstanding items.

Subsequent to the recommendation, Vice-chair A. Durham moved to approve the Land Use Determination 2017-001 to allow for the operation of a fitness training facility (with ancillary sale of supplements) at 1026 Griswold Avenue, #B, based on the Planning and Preservation Commission's land use determination that "health clubs or centers", which include the proposed fitness training facility are similar to or not more objectionable than any of the permitted uses within the M-1 (Limited Industrial) zone, pursuant to Planning and Preservation Commission Resolution 2017-007 (Attachment No.1) .

AYES:	A. Durham, Y. Mejia, and T. Haupt
NOES:	None
ABSENT:	J. Perez-Helliwell
ABSTAIN:	A. Montes

PUBLIC HEARING

Consideration of a Resolution approving and recommending the San Fernando Corridors Specific Plan SP-5, amendments to the General Plan text and map, amendments to the Zoning Ordinance text and map, and Environmental Impact Report for approval by the San Fernando City Council.

STAFF PRESENTATION

Jack Wong introduced the project recommending that the Planning and Preservation Commission:

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- a. Open the Public hearing and receive presentation of City staff;
- b. Pose questions to City staff;
- c. Receive public testimony;
- d. Pose further questions to City staff based on public testimony;
- e. Close the public Hearing and commence deliberations on the proposed Corridors Specific Plan SP-5 and related EIR and General Plan and Zoning ordinance text changes and maps;
- f. Approve the attached Planning and Preservation Commission Resolution 2017-008 (Attachment No. 1), which recommends that the City Council certify the Environmental Impact Report No. 2015121088 (EIR) pursuant to the California Environmental Quality Act (CEQA) and approve:
 - General Plan/Map Amendments,
 - San Fernando Corridors Specific Plan SP-5,
 - Zoning Ordinance/Map Amendments.

David Sargent from Sargent Town Planning (STP) provided the Commission with a powerpoint presentation of the proposed amendments and comparison charts.

COMMISSION DISCUSSION

A. Durham indicated that it was a great plan that addresses a lot of the community concerns. He indicated that the Planning and Preservation Commission will be kept busy by the plan. He stated that this is a “living document” that can be adjusted and is a great start.

Y. Mejia thanked staff and the consultants for all of their work. She stated that she is very impressed with the EIR and its transparency and easiness to read. Additionally she indicated that this document is a good balance with good opportunities for business development. She expressed concern with First Street becoming live/work because there already seems to be issues with double parking and blocking line of sights that could be addressed with better safety constraints.

D. Sargent stated that those are operational measures the City management may want to enforce or implement, because the plan is only as good as we enforce it. He stated that there may have to be some synchronization of lights and turning lanes.

Y. Mejia stated that she would like to see opportunities for additional flexibility in the zoning to allow for incubator business, pop-ups and opportunities for trial run businesses to allow for some temporary spaces.

J. Wong indicated that the Specific Pan provides a certain degree of flexibility wherein the Community Development Director has the ability to determine compatibility of uses not specifically identified in the plan with the surrounding businesses.

T. Haupt asked if there was a cost to the City other than managing it.

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J. Wong indicated that there has been minimal cost to the City to get the document to this point but for the most part it has been the Metro Grant that has paid for all of the Consultant's work.

T. Haupt asked if additional uses could be included, such as Family entertainment: bowling alley, movie theater, and food hall. Furthermore, several uses were formerly Conditional Use Permit (CUP) uses which are now identified as a permitted use, such as restaurants with ancillary alcohol sales. There are also other uses that are now permitted in expanded geographic areas of the Specific Plan area.

Following discussion, staff was directed to include these items in the Specific Plan subject to the motion.

PUBLIC COMMENT

Public hearing opened at 7:44 p.m.

Brian Fagan – stated that he is a stake holder with 20 different lots in the City of San Fernando. He indicated that he was not aware of the Public Workshops that were held in August or September 2017. He stated he would like to have the opportunity to review the plan and that the presentation very positive.

Tom Ross thanked staff for their work on this plan, he indicated that there are severe limitations placed on housing; he would like to see housing be allowed throughout the City Downtown area.

Rodolfo Salinas stated that it was a very good presentation but that his concern is with the lack of parking on his street. He stated that since it is a cul-de-sac style street and it is very narrow. The Commission informed him that the cul-de-sac street in questions is not within the Specific Plan area. T. Haupt suggested that he speak to Jack Wong after the meeting or come in to City Hall and see him.

Maxine Perez stated that she owns property on Celis Street and her concern is how building occupants on First Street and Second Street are going to affect the traffic and parking along Hagar Street and Alexander Street since the street is too narrow especially when there are cars parked on both sides of the street. She stated that most families have three cars per household and that City already has a parking problem.

Brand Dupree stated that he is concerned about the proposed re-zoning being proposed. He stated that he wants to make sure that he can still conduct business in the city. He too stated that there is a parking problem in the city. He asked how the City will pay for more police force when we increase the housing and population.

Ron Hernandez indicated that he owns both commercial and residential properties in the city. He stated that he understands the need for this document and that it is in hopes of attracting more commuters. He is concerned that the cost of implementing this plan will be passed on to the property owners.

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Brent Bell stated that he owns two businesses along First Street and that he was speaking on behalf of the manufacturing business on First Street. His concern is that the zone will change and he will no longer be able to operate his business or will lose its value if sold.

Brad Kelly owns property and works in San Fernando. His concern is at what end of the City this process will start and how long before it gets to the other end of the City. He agreed that there is a parking issue in the City and he stated he too didn't receive any notices regarding the workshops.

Sue Newton indicated that the manufacturing businesses along First Street are the back bone of the City. Sue indicated that her concern is how this proposed rezoning will or will not affect the function of their business on a piece of property that they own.

Mike Blaka stated that he owns a commercial location which has been vacant for the past 10-12 years because it is currently zoned as Automotive Commercial which restricts the types of uses allowed. He stated that potential tenants are not willing to pay the CUP application process. He indicated he is present at the meeting to see if his property would be included in the rezoning so that it becomes more marketable.

Jessie Avila thanked the consultants and the commissioners for their work on this plan. He stated that he is excited about this plan and that there will be a lot of projects that will be impacted by this document.

Alf Knudsen asked the commission regarding the letter that was received regarding the impact of the four rails being proposed by Metro and if this proposed would be through eminent domain and how will this plan change along First Street.

D. Sargent indicated that the letter that you are referring to is the East San Fernando Valley Transit Corridor which is a separate proposal with three different transit options. He stated that it would be best to forward a copy of the notice to Jack Wong since it would include a case number and contact information.

Mary Mendoza informed the Commission that the Metro's recommendation and decision will be considered for a vote in January. Ms. Mendoza stated that there have been extensive meetings with Walt Davis regarding the different options being considered by Metro. She asked if staff has taken into consideration that traffic will be stopped several times along Maclay with the Train, Metro, and possibly Light Rail Train.

Michelle De Santiago – Community Development Secretary read for the record an email received from ibeecyndi@hotmail.com (see attachment).

Chairperson T. Haupt asked Jack Wong to respond to the questions and comments presented. J. Wong stated that there were several valuable comments made and that he will try to address the common theme of the comments:

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- Noticing for the workshops were done through door hangers, publication in the San Fernando Sun Paper, and electronic message board; and
- Parking is always an issue. City of San Fernando is an older city and with this plan we are embarking on new journey. The Specific Plan identifies the need for a public parking structure as a solution to attract people to the downtown and minimize parking issues; and
- The Light Industrial businesses are indeed the backbone of the city and the Specific Plan introduces other business opportunities in the zone that would compliment the existing uses; and
- Traffic circulation throughout the city will be addressed with a recent \$800,000. Grant received by the City to synchronize traffic signals; and
- The local economy and economic health of businesses are prime objectives of the City and with the adoption of the Specific Plan this will provide the foundation to implement other City policies and programs to help existing and new businesses. helping existing businesses not just attracting new businesses; and
- City cannot dictate when and where private investment will start but when it does, we will embrace it.

Mr. Wong emphasized that this plan is not an end all solution but a “living document” and with everyone’s help, it can be step forward.

Public Hearing closed at 8:40

A. Durham stated that tonight’s meeting has generated new questions that may require more time to address before presenting to the City Council.

Y. Mejia thanked the audience members to coming in to voice their concerns and assured the audience that they concerns will be taken into consideration when it come to this plan.

A. Montes expressed that the city needs more diversity in shopping and dining options.

T. Haupt acknowledge the existing parking issues and indicated that if any new construction would take place within this proposed area the project will have to include parking on-site depending on the intended use.

A. Montes asked about 702 Celis Street and how this plan will help the current property owner.

J. Wong stated that SP-5 increases the types of allowable uses to include offices, retail and restaurants.

A. Durham stated that Metro needs to be more transparent with regards to their proposal along First Street.

T. Haupt asked if Metro had a deadline for approving this proposed plan change.

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J. Wong indicated that if the General Plan Amendments are adopted in 2018 as a result of the proposed SP-5 Plan, several new State legislations that become effective January 1, 2018 will require an additional Environmental Justice Element, among other additional work items, as a condition for approving the Specific Plan.

T. Haupt asked if staff can consider adding self-storage as one of the allowable uses so long as it is an ancillary use to retail but not at ground level but may be subject to a CUP in the Downtown or the Work Place Flex.

Following discussion on this suggestion, it was decided to not act further on the suggestion.

Subsequent to discussion Vice-chair A. Durham moved to approve Planning and Preservation Commission Resolution 2017-008, which recommends that the City Council certify the Environmental Impact Report SCH No. 2015121088 (EIR) pursuant to the California Environmental Quality Act (CEQA) and approve the General Plan/Map Amendments, San Fernando Corridors Specific Plan SP-5, and Zoning Ordinance/Map Amendment, including the responses to the questions raised at tonight's meeting.

T. Haupt asked Jack to repeat the additional uses to be included such as; Food Halls in the Downtown area, the bowling alley in the Downtown, and Work/Flex as ancillary use and the items that were brought up were those of noticing procedure, parking, light industrial survival, as well as traffic circulation and signalization.

There was a motion but died for lack of a second.

R. Padilla indicated that if the audience members still have questions the commission can open the Public Hearing to allow for additional questions.

Public Hearing Re-opened at 9:02 p.m.

Brandon Dupree stated that his concerns were with the zoning and the possible re-zoning. He stated that he doesn't want to lose his right to operate his business if and when the proposed plan is adopted.

T. Haupt restated that the existing businesses and uses will remain and will continue to be able to operate.

D. Sargent stated that the uses along First Street will be modified but the current uses will remain and additional uses will be incorporated to allow for more diverse uses.

Brent Bell stated that he would be more comfortable if staff or commission would include language to state that the existing business and approvals will not be modified.

D. Sargent stated that only the zoning name is being changed.

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Mike Salas asked about if the existing uses will be able to rebuild

D. Sargent stated that First Street is not being down zoned.

Public Hearing Closed at 9:22 p.m.

Y. Mejia stated that there are several audience members asking the same question. She indicated that it would be useful if staff included a sentence to clarify the issue or maybe include a table of existing uses and additional allowed uses.

Subsequent to discussion, Commissioner Y. Mejia moved to approve Planning and Preservation Commission Resolution 2017-008, which recommends that the City Council certify the Environmental Impact Report SCH No 2015121088 (EIR) pursuant to the California Environmental Quality Act (CEQA) and to approve the General Plan/Map Amendments, San Fernando Corridors Specific Plan SP-5, and Zoning Ordinance/map Amendments including additional language to include food halls, bowling alleys, and theaters as allowable uses in Downtown District, consideration of reverse angle parking and a matrix and language to be included for the existing uses and additional uses being proposed as part of the report. Seconded by Vice-chair A. Durham, the motion carried with the following vote:

AYES:	Y. Mejia, A. Durham, and T. Haupt
NOES:	None
ABSENT:	J. Perez-Helliwell
ABSTAIN:	A. Montes

STAFF COMMUNICATIONS

None

COMMISSION COMMENTS

T. Haupt asked staff to request that the City Attorney draft a stern letter to the property owner/contractor regarding the lack of progress at 650 Glenoaks Boulevard

ADJOURNMENT

Vice-chair A. Durham moved to adjourn to the regular meeting date of December 5, 2017. Second by Chairperson T. Haupt, the motion carried with the following vote:

AYES:	A. Durham, T. Haupt, Y. Mejia, and A. Montes
NOES:	None
ABSENT:	J. Perez-Helliwell
ABSTAIN:	None

9:29 P.M.

Planning Commission Secretary



Notice of Public Hearing and Notice of Intent to Certify an Environmental Impact Report

**SAN FERNANDO CORRIDORS SPECIFIC PLAN (SP-5), RELATED GENERAL PLAN AND ZONING AMENDMENTS,
AND DRAFT ENVIRONMENTAL IMPACT REPORT**

NOTICE IS HEREBY GIVEN that the San Fernando City Council will hold a public hearing to consider the San Fernando Corridors Specific Plan (SP-5), related General Plan Text and Map Amendments, Zoning Code and Map Amendments, and Draft Environmental Impact Report SCH No. 2015121088 (EIR). The hearing will be held on the date and time described below and will be located in the City of San Fernando City Council Chambers, 117 Macneil Street, at which time and place any and all interested persons may appear and be heard thereon. The scheduled public hearing date is as follows:

<u>Date:</u>	<u>Time:</u>	<u>Hearing Topics</u>
November 20, 2017	6:00 PM	City Council review and approval of San Fernando Corridors Specific Plan (SP-5), related General Plan Text and Map Amendments, Zoning Code and Map Amendments, and Draft Environmental Impact Report SCH No. 2015121088 (EIR).

PROJECT TITLE: San Fernando Corridors Specific Plan (SP-5), with related General Plan Text and Map Amendments, Zoning Code and Map Amendments, and Draft Environmental Impact Report

APPLICANT: City of San Fernando, (c/o Community Development Department), 117 Macneil Street, San Fernando, CA 91340

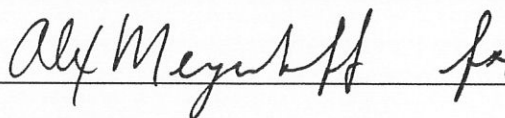
PROJECT LOCATION: The 150.0 acre planning area covered by the San Fernando Corridors Specific Plan (SP-5) encompasses the full lengths of Truman Street, San Fernando Road, and Celis Street within the City of San Fernando, from the eastern boundary with Pacoima to the western boundary with Sylmar. The project boundaries include the entire public rights-of-way as well as parcels located to the north and south of these roads. The San Fernando Corridors Specific Plan (SP-5) planning area also includes the First Street public right-of-way and properties located along the south side of First Street between Hubbard Avenue and Macneil Street; the properties bounded by First Street and Second Street between Hubbard Avenue and Macneil Street; the entire Maclay Avenue public right-of-way and all fronting properties between San Fernando Road to Eighth Street at the City's northern boundary with Sylmar; and the properties located along the north side of Pico Street between Kalisher Street and Chatsworth Drive.

PROJECT DESCRIPTION: The project proposal is a request to adopt the San Fernando Corridors Specific Plan (SP-5) in conjunction with related General Plan Text and Map Amendments and Zoning Code and Map Amendments and Environmental Impact Report. The specific plan document contains the goals, objectives, and policies to guide the community's vision for the revitalization of the North Maclay Avenue, San Fernando Road, Truman Street, and First Street corridors; to enable the creation of a walkable, mixed-use, multi-modal environment that accommodates housing and offices within walking distance of the Sylmar/San Fernando Metrolink Station, Downtown San Fernando, and the proposed Maclay Avenue Metro Station; and to accommodate the new transit initiatives proposed by the East San Fernando Valley Transit Corridor. The specific plan also provides the revitalization strategy and urban design principles to be used in achieving the community's vision. It provides specific land use regulations, development standards and design guidelines that would apply to new development in the corridor areas, as well as a program of public improvements for the streets and sidewalks. The specific plan will have impacts on real properties in terms of land use designations, density or intensity of land uses and new development allowed on properties, future rezoning, and the applicability of various planning policies on

properties within the planning area.

ENVIRONMENTAL ASSESSMENT: The City of San Fernando is the designated Lead Agency overseeing the environmental review for the proposed San Fernando Corridors Specific Plan (SP-5). As the Lead Agency, the City of San Fernando directed the preparation of an Environmental Impact Report. The EIR identified potential impacts associated with future development allowed by the project. The significant impacts identified were noise during construction, the unearthing of subsurface cultural resources during construction, and decreases in intersection performance due to automobile traffic. All these impacts can be mitigated with the appropriate mitigation measures.

Interested members of the public are encouraged to attend the City Council hearing on this matter to provide public comment and/or to raise any objections to matters being considered for approval. Public comment (including objections) may be given orally on the date of the hearing or may be submitted in writing delivered to the City Council by or before the close of the public comment portion of the public hearing. Persons who may later wish challenge the actions of the City Council in court on this matter may be limited to raising only those issues that were raised either orally at the public hearing or in writing submitted by or before the close of the public hearing.



JACK WONG
Interim Community Development Director

Community Development Department ▪ 117 Macneil Street ▪ San Fernando, CA 91340-2993 ▪ (818) 898-1227 ▪ Fax (818) 365-8090

Note: Copies of the Draft San Fernando Corridors Specific Plan (SP-5), draft General Plan Text and Map and Zoning Ordinance Text and Map Amendments, and draft Environmental Impact Report SCH No. 2015121088 (EIR) documents are available for review at:

- San Fernando City Hall, Community Development Department, 117 Macneil Street, San Fernando, CA.
- Los Angeles County Library, 217 N. Maclay Avenue, San Fernando, CA.
- San Fernando Recreation Park Community Center, 208 Park Avenue, San Fernando, CA.
- Las Palmas Park Community Center, 505 S. Huntington Street, San Fernando, CA.
- City of San Fernando Website: <http://ci.san-fernando.ca.us/community-development/planning/>

ATTACHMENT “F”

From: Cyndi [<mailto:ibeecyndi@hotmail.com>]
Sent: Monday, November 06, 2017 11:19 PM
To: Elena Chavez
Subject: Public comment for Nov 7th meeting

****Please read into the public comments at the Planning Commissions meeting Tuesday November 7th 2017. Thank you.****

The following is a summary of recommendations that were submitted to the City Council by the Residents for a Better San Fernando in regards to the proposed Specific Plan Update.

From PICO STREET TO TRUMAN STREET AND BRAND BLVD TO SAN FERNANDO MISSION BLVD.

- ☐ No Residential of Any Kind in the Downtown District.
- ☐ Zoning for Restaurants, retail, and commercial only.
- ☐ City parking structures and other public property should be retained for parking, commercial and open space not residential.
- ☐ Height Limit: 2-Floors.
- ☐ Signage: On-site signage only.

2. COMMERCIAL DISTRICT: SAN FERNANDO MISSION TO WORKMAN, PICO TO TRUMAN

- ☐ No Residential of Any Kind in the Commercial District. Retain commercial uses.
- ☐ Height Limit: 2-Floors.
- ☐ Architectural Guidelines to Complement Historic Downtown.
- ☐ More streetscape improvements.
- ☐ On site signage.

3. CIVIC CENTER COMMERCIAL: MACLAY AVENUE FROM 1ST STREET TO 4TH STREET

- ☐ Maintain development patterns as is. No Residential.
- ☐ City has done a great job with the streetscape improvements, preserving historic buildings such as the old Salvation Army building (formerly Compania de Café), Library Plaza, and the Paseo connecting City Hall to Maclay.
- ☐ Preserve and Enhance Signage District to encourage consistent high-quality signage. No plastic, neon or banners.
- ☐ Height Limit: 1 Floor.

4. MACLAY AVENUE FROM 4TH TO 8TH STREET

- ☐ Strong support for maintaining streetscape improvements that have been completed along Maclay from 1st Street to 8th Street. Height Limit: 2-Story.
- ☐ Preserve Lower Density Community Commercial along Maclay, such as neighborhood markets, ice cream parlors, local restaurants, professional office, churches, schools, and existing single-family residences.
- ☐ 1075 Maclay Townhomes are a good example of the type of the higher quality architecture that should result from the architectural requirements set in the Corridors Specific Plan. (1101 7th Street is not acceptable).
- ☐ Low and medium density dwelling units per acre with a Conditional Use Permit (C.U.P.)

5. PRESERVE INDUSTRIAL ZONE: 1ST STREET FROM MACLAY TO HUBBARD

- ☐ Preserve all M-1 limited industrial and M-2 light industrial zoning along 1st street.
- ☐ Work-live with Conditional Use Permit (C.U.P.). No majority residential buildings; 25% maximum residential or some other condition to prevent multi-family dwellings of any kind.
- ☐ Height Limit: 2 Floors.
- ☐ Allow for commercial only on small existing commercial zoning on 1st Street, near Maclay.
- ☐ Focus must be on increasing the jobs-to-resident ratio by attracting good middle-class jobs.

6. PROTECT SINGLE-FAMILY ZONES: IMPROVE TRANSITION FROM R-1 TO R-3

- ☐ Height Limit: 2 floors, maximum of 30 feet tall.
- ☐ Lower the Existing Dwelling Units Per Acre: Medium to Low Density to provide a much better transition to the immediately adjacent R-1 single family residential neighborhoods.
- ☐ Architectural and Increased Landscaping Requirements. Architecture will fall in line with what the community knows as “traditional” architecture (i.e. Craftsman style, wood siding, pitched roofs, and the like) (No “modern box-like structures like 124 Harding.)
- ☐ Increase Park and Open Space in the R-3 Area.
- ☐ Upper Floor Setbacks to reduce massing and better complement the predominately single story homes and low-density apartments.
- ☐ Market-rate housing such as townhomes/condos.
- ☐ First-time homebuyer programs for veterans with VA-backed loans, teachers, firefighters and other community members.

7. IMPROVE TRANSITION FROM R-2 TO COMMERCIAL: ALONG CELIS

- ☐ Height Limit: 2 Floors. Not to exceed 30 feet.
- ☐ Architectural and Increased Landscaping Requirements and Review Process.
- ☐ Require courtyards, open plazas and other open space with any increased development.
- ☐ Upper Floor Setbacks to reduce massing and better complement the predominately single story homes and low-density apartments.
- ☐ Plant more trees, increase landscaping and provide a green buffer between commercial and residential.

RECEIVED

October 18, 2017

OCT 25 2017

To: Mayor Sylvia Ballin and San Fernando City Council Members

Re: Proposed Specific Plan EIR

From: Mary Mendoza ^{*MMD*} and Residents**CITY CLERK DEPARTMENT
CITY OF SAN FERNANDO**

We would like additional time to review the proposed Specific Plan Update EIR; however here are some additional recommendations on the proposed Specific Plan Update. Please include all petitions and letters submitted to the City and its appointed or elected officials in opposition throughout the Transit Oriented Development and Specific Plan update process.

Eliminate the Mixed Use Overlay: No Residential in the Downtown and Historic Core

There is no doubt the strongest opposition in the community is with allowing multi-family apartment units in the areas outside of the R-3 zones, so we recommend that the City eliminate any possibility for any multi-family residential units even with a Conditional Use Permit (C.U.P.) or use of the Development Agreement Ordinance in the Downtown and Historic Core from San Fernando Mission Blvd to Chatsworth Drive and Pico Street to Truman Avenue.

Suggested Amendment: Eliminate the Mixed Use Overlay that allows for 4 floors/50 foot tall buildings with 50 dwelling units per acre and a 3.5 Floor Area Ratio or any other possibility for residential from San Fernando Mission to Chatsworth Drive and Pico to Truman Avenue.

This would allow for the complete preservation of commercial zoning from the Downtown to the Auto Commercial Zone and encourage a concentrated, high-quality commercial Historic Downtown with a focus on shopping, retail, restaurants, outdoor seating and other commercial uses that residents want for San Fernando.

In addition to not allowing multi-family in the Downtown and Historic Core we support a lowering the height to 2 floors and tightening up architectural requirements around the areas in close proximity to the Historic Core.

Suggested Amendment: Height Limit of 2 floors for the Downtown, especially adjacent to the Historic Core.

Maintaining a Historic Downtown that complements the historic 1882 Casa de Lopez Adobe, St. Ferdinand's Church, the historic Post Office and other older buildings would help preserve the unique quality and historic character of San Fernando.

Industrial Zoning

We strongly support preserving zoning for good middle-class jobs and appreciate the effort to preserve the M-1 limited industrial and M-2 light industrial zoning along 1st Street and Truman immediately adjacent to the railroad tracks. We would recommend lowering the 40 foot height limit along with the number of floors to better transition to the mostly 2 story and single story residences to the north of 1st Street.

Suggested Amendment: Limit Height in the Workplace Flex area to 2 floors or 25-30feet.

Working to increase our ratio of middle-income jobs per residence requires that the City preserve industrial zoning.

Suggested Amendment: Establish a ratio of industrial square footage to residential square footage ratio to maintain a supermajority of square footage and zoning for M1 and M2 uses over residential uses on all projects.

We believe 1st Street can become one of the most prosperous streets in town if we focus on creative work spaces and businesses that provide middle-class jobs.

Civic Center and Maclay from 1st to 4th Street

We do not allow multi-family residential on Maclay Avenue from 1st Street to 4th Street and recommend lowering the height limit to 20-25 feet to maintain what are essentially one-story buildings with a couple of the existing two-story buildings. The initially proposed height increase to 60 feet was excessive as is the existing 50 feet height limit and unlimited du/acre allowed in the 2005 Corridors Specific Plan.

The City has done a fair job along Maclay from 1st to 4th Street by completing the comprehensive Maclay Streetscape Improvements, connecting the Civic Center/City Hall to Maclay with the paseo, preserving the historic Salvation Army Building (formerly Compania de Café), developing Library Plaza with the outdoor courtyard, maintaining the older potentially historic buildings along the corridor and encouraging higher quality architectural and on-site signage among other actions.

We believe the City just needs to keep going in the direction of attracting good quality restaurants and other local community retail on Maclay from 1st to 4th, and that section of Maclay will continue to be one of the most cherished places for our community.

R-3 Zoning-Neighborhood General

I do believe that amending the R-3 zoning is a good idea if it allows the City the opportunity to lower the du/acre from the 43 du/acre, add the upper floor setback, decrease the maximum height from 45 feet and add other moderating requirements to allow a better transition to the immediately adjacent R-1 lower density, single-family residential neighborhoods.

A similar approach would also be helpful along Celis Street, Pico Street and the Maclay District as residents in those general area don't like the Mid-Celis or 1101 Seventh Street apartment buildings but embraced Casa Garcia on Kalisher Street for its lower density, height and architecture.

An argument can also be made to consider having those density and architectural changes apply to the entire R-3 zone while we have resources to make those changes. General Plan recommendations must be made to provide a much better zoning transition between R-1 and R-3.

Maclay: 4th Street to 8th Street

We also agree with decreasing the overall density through lower height maximums and du/acre in the Maclay District from 4th Street to 8th Street. Changes to Maclay need to be carefully crafted because almost all the properties directly behind either side of Maclay from 4th to 8th Street are R-1 single-story residential neighborhoods.

Open Space

We strongly support adding a Central Park or Plaza and encourage development standards to maximize walkways/paseos, courtyards and other open space with new development. The City is in control of some very valuable land that it can use to help to make sure future development incorporates additional open space. We have made some progress in increasing park/open space with the Cesar Chavez Transit Plaza, Heritage Park and the Mountains Conservancy and Recreation Park, so I encourage us to take advantage of the Specific Plan process to continue to move San Fernando away from being a park-poor city.

Streetscape Improvements and Street Adjustments

We support the Maclay Streetscape Improvements and additional streetscape improvements that make San Fernando more pedestrian-friendly, walkable and greener.

Thank you again for the opportunity to provide input. We ask that you not allow for the dramatic increase in more multi-family dwelling units in the City of San Fernando.

Proposed Specific Plan EIR
October 18, 2017

Benita Ramirez

Name/Nombre

Benita Ramirez

Signature/Firma

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Address/Domicilio

Julie A Cwellar

Name/Nombre

Julie A Cwellar

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GUSTAVO FRANCIS

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MARY MENDOZA

Name/Nombre

Mary Mendoza

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Proposed Specific Plan EIR

October 18, 2017

Enrique Lemus
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Enrique Lemus
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JUAN LOPEZ
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Juan Lopez
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432 N. Brand Blvd SFA 91340
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Proposed Specific Plan EIR
October 16, 2017

CYNDI Lopez Cyndi Lopez
Name/Nombre Signature/Firma

2019 Lucas St. San Fernando, CA 91340
Address/Domicilio

Name/Nombre Signature/Firma

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Name/Nombre Signature/Firma

Address/Domicilio

Name/Nombre Signature/Firma

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Name/Nombre Signature/Firma

Address/Domicilio

Proposed Specific Plan EIR
October 18, 2017

Patty Lopez
Name/Nombre

[Signature]
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432 North Brand Blvd SF, CA 91340
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Diana Lopez
Name/Nombre

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Patricia Lopez
Name/Nombre

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225 Harding Avenue San Fernando, CA 91340
Address/Domicilio

MARIA Cervacio
Name/Nombre

[Signature]
Signature/Firma

12636 Bromont Ave San Fernando CA 91348
Address/Domicilio

Margarita Cervacio
Name/Nombre

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Historic &
Visionary

SPECIFIC PLAN: COMMUNITY RECOMMENDATIONS

1. HISTORIC DOWNTOWN: PICO STREET TO TRUMAN STREET AND BRAND BLVD TO SAN FERNANDO MISSION BLVD.

- ☐ No Residential of Any Kind in the Downtown District.
- ☐ Zoning for Restaurants, retail, and commercial only.
- ☐ Architectural Requirements to Complement Historic Lopez Adobe, St. Ferdinand's Church, the Historic Post Office and other older buildings identified in the Historic Resources Survey.
- ☐ City parking structures and other public property should be retained for parking, commercial and open space not residential. Public property should be used for the larger public good.
- ☐ Sidewalk seating, historic streetscape improvements, more trees, increased landscaping, etc.
- ☐ Height Limit: 2-Floors. Nothing to be taller than St. Ferdinand's Church tower and all architecture to complement Lopez Adobe and other older buildings in the historic downtown.
- ☐ Signage: On-site signage only. Signage Requirements to complement Historic Downtown. Higher quality materials and craftsmanship, i.e. handmade wood signs, no plastic, neon or banners.
- ☐ Architectural Review Process

2. COMMERCIAL: SAN FERNANDO MISSION TO WORKMAN, PICO TO TRUMAN

- ☐ No Residential of Any Kind in the Commercial District. Retain commercial uses, such as the City's main grocery store, local restaurants and other retail.
- ☐ Entertainment District along these blocks would enhance the area and provide a nice transition from the Historic Downtown.
- ☐ Height Limit: 2-Floors.
- ☐ Architectural Guidelines to Complement Historic Downtown and Surrounding Residential Areas.
- ☐ More street trees, increased landscaping, streetscape improvements
- ☐ Signage: On site signage. Higher quality materials and craftsmanship.
- ☐ Parking Structure: Design and Landscaping Requirements.

3. CIVIC CENTER COMMERCIAL: MACLAY AVENUE FROM 1ST STREET TO 4TH STREET

- ☐ Maintain development patterns as is. No Residential.
- ☐ City has done a great job with the streetscape improvements, preserving historic buildings such as the old Salvation Army building (formerly Compania de Café), Library Plaza, and the Paseo connecting City Hall to Maclay.
- ☐ Preserve and Enhance Signage District to encourage consistent high-quality signage. No plastic, neon or banners.
- ☐ Height Limit: 1 Floor. No Building Taller than Old Salvation Army Building.
- ☐ Preserve investments into streetscape improvements that have been made.

4. MACLAY AVENUE FROM 4TH TO 8TH STREET

- ☐ Strong support for maintaining streetscape improvements that have been completed along Maclay from 1st Street to 8th Street. Height Limit: 2-Story.
- ☐ Height Limit: 2-Floors to provide best transition to R-1 single family residential zoning along both sides of Maclay.
- ☐ Preserve Lower Density Community Commercial along Maclay, such as neighborhood markets, ice cream parlors, local restaurants, professional office, churches, schools, and existing single-family residences.
- ☐ 1075 Maclay Townhomes are a good example of the type of the higher quality architecture that should result from the architectural requirements set in the Corridors Specific Plan. (1101 7th Street is not acceptable).
- ☐ Low and medium density dwelling units per acre with a Conditional Use Permit (C.U.P.) to allow public input given the proximity to R-1 single family residential neighborhoods
- ☐ Encourage homeownership such as townhomes and single-family homes.
- ☐ Architectural Review Process.

5. PRESERVE INDUSTRIAL ZONE: 1ST STREET FROM MACLAY TO HUBBARD

- ☐ Preserve all M-1 limited industrial and M-2 light industrial zoning along 1st street.
- ☐ Work-live with Conditional Use Permit (C.U.P.). No majority residential buildings; 25% maximum residential or some other condition to prevent multi-family dwellings of any kind.
- ☐ Height Limit: 2 Floors.
- ☐ More street trees, increased landscaping and streetscape improvements.
- ☐ Allow for commercial only on small existing commercial zoning on 1st Street, near Maclay.
- ☐ Focus must be on increasing the jobs-to-resident ratio by attracting good middle-class jobs.
- ☐ Architectural Review Process.

6. PROTECT SINGLE-FAMILY ZONES: IMPROVE TRANSITION FROM R-1 TO R-3

- ☐ Height Limit: 2 floors, maximum of 30 feet tall.
- ☐ Lower the Existing Dwelling Units Per Acre: Medium to Low Density to provide a much better transition to the immediately adjacent R-1 single family residential neighborhoods.
- ☐ Architectural and Increased Landscaping Requirements. Architecture will fall in line with what the community knows as "traditional" architecture (i.e. Craftsman style, wood siding, pitched roofs, and the like) (No "modern box-like structures like 124 Harding. It is too tall and massive)
- ☐ Increase Park and Open Space in the R-3 Area. Include courtyards, paseos, mature trees and increased landscaping (Layne Park is not enough open public space for a densely populated R-3 neighborhood)
- ☐ Upper Floor Setbacks to reduce massing and better complement the predominately single story homes and low-density apartments.
- ☐ Market-rate housing such as townhomes/condos.
- ☐ First-time homebuyer programs for veterans with VA-backed loans, teachers, firefighters and other community members.
- ☐ Architectural Review Process

7. IMPROVE TRANSITION FROM R-2 TO COMMERCIAL: ALONG CELIS

- ☐ Height Limit: 2 Floors. Not to exceed 30 feet.
- ☐ Low Dwelling Units per Acre to provide a much better transition to the immediately adjacent R-2 residential zone.
- ☐ Architectural and Increased Landscaping Requirements and Review Process. (Mid-Celis Apartment is too tall and massive; however Casa Garcia Senior Apartments a model of what the residents want in that area).
- ☐ Require courtyards, open plazas and other open space with any increased development.
- ☐ Upper Floor Setbacks to reduce massing and better complement the predominately single story homes and low-density apartments.
- ☐ Plant more trees, increase landscaping and provide a green buffer between commercial and residential.
- ☐ Zoning: Housing restricted to Senior Housing, Townhomes and Ownership

Please include this letter in the Specific Plan Proposed Update - EIR

9/18/2015

Mary Mendoza
Residents for a Better San Fernando
San Fernando, CA 91340

City Manager Brian Saeki and Community Development Director Fred Ramirez
Cc: Mayor Joel Fajardo
117 North Macneil Street
San Fernando, CA 91340

City Manager Saeki and Director Ramirez:

Thank you for meeting with me and a small group of residents on Tuesday, September 15, 2015 regarding the proposed staff changes to the Transit Oriented Development (TOD) Overlay Zone since the August 4, 2015 City Planning Commission meeting. **We were disappointed our discussion was left unfinished and that Mr. Saeki stated that staff would be not be making more changes to the proposal. We strongly believe many more revisions need to be made to the TOD proposal and we remain committed to this process; therefore we are requesting additional conversations prior to the October 6, 2015 Planning Commission meeting as the community's voice needs to be adequately reflected in the proposal.** We have devised the attached TOD Community Recommendations that we would like to discuss having amended into the TOD proposal prior to the Planning Commission meeting.

We acknowledge the efforts of Mr. Ramirez and Mr. David Sargent, the TOD Consultant in reducing the number of units in the proposed Amended Corridors Specific Plan area from 1,000 multi-family units by the year 2035 (June 17 proposal) to 996 units (August 4 proposal) to now 759 (September 15 proposal), however the reduction of 241 units from the initial proposal is not reflective of the strong opposition expressed by the community at the August 4, 2015 City Planning Commission, where everyone except 3 people (a young woman from out of town, former Councilmember Jessie Avila and his partner Linda Campanella) spoke against tall out-of-scale buildings and any more apartments period.

We ask you to work with us to maintain the historic, small-town character of the City of San Fernando and allow the voice of the community not the Metropolitan Transit Authority or developers to drive changes to the Corridors Specific Plan. We strongly support making our town more walkable, pedestrian-friendly, architecturally attractive and greener, and feel we can jointly develop an enhanced Corridors Specific Plan that better reflects the vision residents have of our town.

We remain vehemently opposed to the intense overdevelopment and massive increase in multi-family residential units that is being proposed in the TOD Overlay Zone. We strongly support tightening the requirements to protect all the light industrial areas where the City can attract good middle-class jobs, reducing the dwelling units per acre, reducing the height of buildings, developing better architectural and landscaping requirements, increasing open space, not allowing any multi-family residential outside of the R-3 areas, especially in the Historic Downtown (Truman to Pico and Brand to San Fernando Mission Blvd) and the Civic Center Commercial (Maclay from 1st to 4th Street), requiring a Conditional Use Permit for any new residential development in the TOD Overlay Zone and Specific Corridors Plan and requiring Architectural Review for all new development.

Finally, we still remain extremely concerned that the City has not provided a proper evaluation of what the sewer system and other city infrastructure can actually handle. **Once again, we are asking that the presentation of the TOD proposal be postponed until AFTER the City Council receives a detailed presentation of the existing sewer conditions and devises a plan for how to address those critical infrastructure needs.** It would be against our community's best interest and contrary to good long-term planning for the City staff to promote an extreme increase in overdevelopment without a proper evaluation of what the City's infrastructure can actually handle. We demand that you take our life-safety concerns in earnest and promptly provide a detailed evaluation on the sewer system prior to any further development in town and approval action on the TOD Overlay Zone.

We are taking the TOD process very seriously and expect the City staff to act on the community's recommendations. We have attended every Development Advisory Committee meeting, talked to hundreds of local residents, provided input through the formal TOD Stakeholder interviews, written letters, spoken at the City Planning Commission and are continuing to study the existing Specific Plan, the Housing Element, the General Plan and the TOD proposal carefully.

Residents remain very frustrated and disillusioned with the City ignoring the strong opposition to the proposed 101-unit JC Penney apartment project, so we can assure you that the vast majority of residents in San Fernando do not support the proposal to allow for over 600 multifamily residential units in the TOD zone in addition to what is already allowed in other parts of town.

We are hopeful that you will meet with us again next week to discuss how to jointly incorporate our recommended changes into the TOD proposal prior to it being presented to the City Planning Commission.

Sincerely,



Mary Mendoza

Residents for a Better San Fernando

marymendoza1919@yahoo.com

ATTACHMENT "G"

SP-4 / ZONING ORDINANCE LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)	(AC)	(WF)				(GN)	
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Accessory Buildings and Structures															
Cabana Located on the Same Lot as the Principal Residential Use.	P	—	—	—	—	—	—	P	—	—	—	—	—	—	P
Garage	P	—	—	—	—	—	—	P	—	—	—	—	—	—	P
Recreation Room	P	—	—	—	—	—	—	P	—	—	—	—	—	—	P
Storage Shed	P	—	—	—	—	—	—	P	—	—	—	—	—	—	P
Workroom	P	—	—	—	—	—	—	P	—	—	—	—	—	—	P
Automobile and Vehicle Uses															
Automobile Sales	—	—	—	—	P	CUP	P	—	—	P	CUP	CUP	CUP	P	—
Automobile Services	—	—	—	—	P	—	P	—	—	P	CUP	P	P	P	
Automobile Rental Agencies	—	—	—	—	—	—	—	—	—	—	CUP	—	—	—	—
Carwashes	—	—	—	—	—	—	CUP	—	—	—	—	—	—	CUP	
Gasoline Refueling Service Stations	—	—	—	—	—	—	P	—	—	—	CUP	CUP	CUP	P	—
Manufacturing and Light Industrial Uses: All uses permitted and conditionally permitted within the M-1 and M-2 Zones [1], including:															
Agriculture	—	—	—	—	—	—	—	—	—	—	—	P	P	—	—
Assembling	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—
Assembly and Production Facilities	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—
Manufacturing	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—
Repairing	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—
Research and Development	—	—	—	—	—	—	—	—	—	—	P	CUP	CUP	—	—
Testing	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—
Warehousing	—	—	—	—	—	—	—	—	—	—	P	CUP	CUP	—	—
Wholesaling	—	—	—	—	—	—	—	—	—	—	P	P	P	—	—

KEY

- P = use permitted by right
- CUP = use requires approval of conditional use permit
- = use not permitted

SP-5 DISTRICTS

- M = Maclay District
- D = Downtown District
- MUC = Mixed-Use Corridor District
- AC = Auto Commercial District
- WF = Workplace Flex District
- GN = General Neighborhood District

SP-4 DISTRICTS AND SUB-DISTRICTS

- M = Maclay District
- CC = City Center Sub-District
- SFM = San Fernando Mall Sub-District
- MUT = Mixed-Use Transition Sub- District
- WC = Workplace Commercial Sub-District
- AC = Auto Commercial Sub-District
- WF = Workplace Flex District

SAN FERNANDO ZONING ORDINANCE ZONES

- R-3 = Multiple Family Zone
- C-1 = Limited Commercial Zone
- C-2 = Commercial Zone
- M-1 = Limited Industrial Zone
- M-2 = Light Industrial Zone

NOTES

- [1] See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
- [2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
- [3] Nonresidential off-street parking lot
- [4] Use permitted only on upper floors.
- [5] Use permitted only within Downtown Residential Overlay and only on upper floors.
- [6] Use not permitted within 200 feet of railroad right-of-way.

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Accessory Buildings and Structures						
Cabana Located on the Same Lot as the Principal Residential Use.	P	–	–	–	–	P
Garage	P	–	–	–	–	P
Recreation Room	P	–	–	–	–	P
Storage Shed	P	–	–	–	–	P
Workroom	P	–	–	–	–	P
Automobile and Vehicle Uses						
Automobile Sales	–	–	–	P	CUP	–
Automobile Services	–	–	–	P	CUP	–
Automobile Rental Agencies	–	–	–	P	CUP	–
Carwashes	–	–	–	–	CUP	–
Gasoline Refueling Service Stations	CUP	–	CUP	P	CUP	–
Manufacturing and Light Industrial Uses: All uses permitted and conditionally permitted within the M-1 and M-2 Zones [1,2], including:						
Agriculture	–	–	P	–	P	–
Assembling	–	–	P	–	P	–
Assembly and Production Facilities	–	–	P	–	P	–
Maker Space	–	–	P	P	P	–
Manufacturing	–	–	P	–	P	–
Repairing	–	–	P	–	P	–
Research and Development	–	–	P	–	P	–
Testing	–	–	P	–	P	–
Warehousing	–	–	P	–	P	–
Wholesaling	–	–	P	–	P	–

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)	(AC)	(WF)				(GN)	
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Parking Structures and Facilities															
Privately-Owned	–	CUP	–	P	P	–	–	CUP	P	P	P	–	–	–	CUP[3]
Publicly-Owned	–	P	–	P	P	–	–	CUP	P	P	P	–	–	–	CUP[3]
Public Uses															
Civic and Cultural Facilities															
Libraries	–	P	P	–	–	–	–	–	–	–	–	–	–	–	CUP
Public Recreation Facilities	–	P	P	–	–	–	–	–	–	–	–	–	–	–	–
Museums	–	P	P	–	–	CUP	CUP	–	–	–	–	–	–	CUP	CUP
Art Galleries	–	P	P	–	–	CUP	CUP	–	–	–	–	–	–	CUP	–
Public and Institutional Uses															
Hospitals or Sanitariums	–	–	–	–	–	–	P	–	–	–	–	–	–	P	CUP
Mortuaries	–	–	–	–	–	–	P	–	–	–	–	–	–	P	
Open Spaces, including Parks and Playgrounds	P	–	–	–	–	P	P	P	–	–	–	–	–	P	P
Public Health Services and Facilities	P	–	–	–	–	–	–	–	–	–	–	–	–	–	–
Public Assembly Uses															
Banquet Halls	–	CUP	CUP	CUP	–	–	–	CUP	CUP	–	–	–	–	–	–
Churches, Temples, or Other Places of Worship	–	–	–	–	–	–	–	–	–	–	–	–	–	–	CUP
Conference Facilities	–	CUP	CUP	CUP	–	–	–	CUP	CUP	–	–	–	–	–	–
Community Recreational Centers	CUP	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Meeting Facilities	CUP	CUP	CUP	CUP	–	–	–	CUP	CUP	–	–	–	–	–	–
Movie Theaters	–	CUP	CUP	CUP	–	–	CUP	CUP	CUP	–	–	–	–	CUP	–
Public Clubs, Lodges, and Halls	CUP	P[4]	P[4]	–	–	CUP	CUP	–	–	–	–	–	–	CUP	–
Venues/Auditoriums for the Performing Arts	–	CUP	CUP	–	–	–	CUP	–	–	–	–	–	–	CUP	–

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES
P = use permitted by right	M = Maclay District	M = Maclay District	R-3 = Multiple Family Zone
CUP = use requires approval of conditional use permit	D = Downtown District	CC = City Center Sub-District	C-1 = Limited Commercial Zone
– = use not permitted	MUC = Mixed-Use Corridor District	SFM = San Fernando Mall Sub-District	C-2 = Commercial Zone
	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone
	GN = General Neighborhood District	AC = Auto Commercial Sub-District	
		WF = Workplace Flex District	

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Parking Structures and Facilities						
Privately-Owned	–	P	P	P	P	–
Publicly-Owned	–	P	P	P	P	–
Public Uses						
Civic and Cultural Facilities						
Libraries	–	P	–	–	–	–
Public Recreation Facilities	–	P	–	–	–	–
Museums	–	P	P	–	–	–
Art Galleries	P	P	P	–	–	–
Public and Institutional Uses						
Hospitals or Sanitariums	–	–	–	–	–	–
Mortuaries	–	–	–	–	–	–
Open Spaces, including Parks and Playgrounds	P	P	P	P	P	P
Public Health Services and Facilities	P	–	–	–	–	–
Public Assembly Uses						
Banquet Halls	–	CUP	CUP	–	–	–
Churches, Temples, or Other Places of Worship	P	–	P	P	P	CUP
Conference Facilities	–	CUP	CUP	–	–	–
Community Recreational Centers	CUP	–	P	–	–	P
Meeting Facilities	CUP	CUP	CUP	–	–	–
Movie Theaters	P	P	P	P	–	–
Public Clubs, Lodges, and Halls	CUP	P	–	–	–	–
Venues/Auditoriums for the Performing Arts	P	P	P	P	–	–

NOTES
[1] See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
[2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
[3] Nonresidential off-street parking lot
[4] Use permitted only on upper floors.
[5] Use permitted only within Downtown Residential Overlay and only on upper floors.
[6] Use not permitted within 200 feet of railroad right-of-way.

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)	(AC)	(WF)				(GN)	
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Residential Uses															
Accessory dwelling unit	–	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Apartments	P	P[5]	P[5]	–	–	–	–	P	–	–	–	–	–	–	CUP
Community Care Facilities/Small	P	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Community Care Facilities/Large	P	–	–	–	–	–	–	–	–	–	–	–	–	–	CUP
Condominiums	P	P[5]	P[5]	–	–	–	–	P	–	–	–	–	–	–	CUP
Duplexes	–	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Home Occupations in accordance with Division 9 of Article VI of the <i>San Fernando City Code</i>	P	P[5]	P[5]	–	–	–	–	P	–	–	–	–	–	–	P
Live-Work	P	P[5]	P[5]	–	–	–	–	P	–	–	–	–	–	–	–
Manufactured home as defined in <i>Health and Safety Code</i> Section 18007	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
Mobile Home Park	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
Primary single-family dwelling units, one per lot, in a permanent location	–	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Boarding Houses	–	–	–	–	–	CUP	CUP	–	–	–	–	–	–	CUP	CUP
Supportive Housing	–	–	–	–	–	–	–	–	–	–	–	–	–	–	P
Townhouses	–	P[5]	P[5]	–	–	–	–	P	–	–	–	–	–	–	CUP
Transitional Housing	–	–	–	–	–	–	–	–	–	–	–	CUP	CUP	–	P

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Residential Uses						
Accessory dwelling unit	–	–	–	–	–	–
Apartments	CUP	CUP[4]	CUP	–	–	CUP
Community Care Facilities/Small	P	P[4]	P	–	–	P
Community Care Facilities/Large	CUP	CUP[4]	CUP	–	–	CUP
Condominiums	CUP	CUP[4]	CUP	–	–	CUP
Duplexes	–	–	–	–	–	P
Home Occupations in accordance with Division 9 of Article VI of the <i>San Fernando City Code</i>	P	P[4]	P	–	CUP	P
Live-Work	CUP	CUP[4]	CUP	–	CUP	–
Manufactured home as defined in <i>Health and Safety Code</i> Section 18007	–	–	–	–	–	–
Mobile Home Park	–	–	–	–	–	–
Primary single-family dwelling units, one per lot, in a permanent location	–	–	–	–	–	P
Single Room Occupancy	–	–	–	–	–	–
Supportive Housing	CUP	CUP[4]	CUP	–	–	CUP
Townhouses	CUP	–	CUP	–	–	CUP
Transitional Housing	CUP	CUP[4]	CUP	–	–	CUP

KEY

P

=

use permitted by right

CUP

=

use requires approval of conditional use permit

–

=

use not permitted

SP-5 DISTRICTS

M

=

Maclay District

D

=

Downtown District

MUC

=

Mixed-Use Corridor District

AC

=

Auto Commercial District

WF

=

Workplace Flex District

GN

=

General Neighborhood District

SP-4 DISTRICTS AND SUB-DISTRICTS

M

=

Maclay District

CC

=

City Center Sub-District

SFM

=

San Fernando Mall Sub-District

MUT

=

Mixed-Use Transition Sub- District

WC

=

Workplace Commercial Sub-District

AC

=

Auto Commercial Sub-District

WF

=

Workplace Flex District

SAN FERNANDO ZONING ORDINANCE ZONES

R-3

=

Multiple Family Zone

C-1

=

Limited Commercial Zone

C-2

=

Commercial Zone

M-1

=

Limited Industrial Zone

M-2

=

Light Industrial Zone

- NOTES
- [1]

See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
- [2]

M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
- [3]

Nonresidential off-street parking lot
- [4]

Use permitted only on upper floors.
- [5]

Use permitted only within Downtown Residential Overlay and only on upper floors.
- [6]

Use not permitted within 200 feet of railroad right-of-way.

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Retail, Service, Entertainment, Lodging and Office Uses															
Business and Personal Service Shops															
Adult Businesses: Merchandise Sales	–	CUP	CUP	P	–	–	CUP	–	P	–	–	–	–	CUP	–
Banks, Credit Unions, Loan Companies, Title Companies	–	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Barber & Beauty Shops	–	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Dry Cleaning	P[1]	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Interior Decorating Studios	–	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Laundromat	P[1]	–	–	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Nail Salons	–	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Photocopy Shops	–	P	P	P	CUP	P	P	P	P	CUP	–	CUP	CUP	P	–
Repair Shops	–	–	–	P	CUP	–	P	P	P	CUP	–	–	–	P	–
Shoe Repair	–	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Video Rental & Sales	P[1]	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Entertainments Uses															
Billiards/Pool Parlors	–	CUP	CUP	P	–	–	CUP	P	P	–	–	–	–	CUP	–
Bowling Alleys	–	–	–	P	–	–	CUP	P	P	–	–	–	–	CUP	–
Nightclubs	–	CUP	CUP	–	–	–	–	–	–	–	–	–	–	–	–
Skating/Skateboard Venues	–	–	–	P	–	–	–	P	P	–	–	–	–	–	–
Health and Exercise Clubs	P	P[2]	P[2]	–	–	–	–	P	–	–	–	–	–	–	–
Lodging															
Bed-and-Breakfasts	CUP	CUP[2]	CUP[2]	–	–	–	–	CUP	–	–	–	–	–	–	–
Hotels, Motels	–	CUP[2]	CUP[2]	–	–	CUP	CUP	CUP	–	–	–	–	–	CUP	–
Boardinghouses	–	–	–	–	–	–	–	CUP	–	–	–	–	–	–	–

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES
<i>P</i> = use permitted by right	M = Maclay District	M = Maclay District	R-3 = Multiple Family Zone
<i>CUP</i> = use requires approval of conditional use permit	D = Downtown District	CC = City Center Sub-District	C-1 = Limited Commercial Zone
<i>–</i> = use not permitted	MUC = Mixed-Use Corridor District	SFM = San Fernando Mall Sub-District	C-2 = Commercial Zone
	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone
	GN = General Neighborhood District	AC = Auto Commercial Sub-District	
		WF = Workplace Flex District	

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Retail, Service, Entertainment, Lodging and Office Uses						
Business and Personal Service Shops						
Adult Businesses: Merchandise Sales	–	–	–	–	–	–
Banks, Credit Unions, Loan Companies, Title Companies	–	P	P	P	–	–
Barber & Beauty Shops	–	P	P	P	–	–
Dry Cleaning	P	P	P	P	–	–
Interior Decorating Studios	–	P	P	P	–	–
Laundromat	P	P	P	P	–	–
Market Hall, Food Hall	–	P	P	P	–	–
Nail Salons	–	P	P	P	–	–
Photocopy Shops	–	P	P	P	–	–
Repair Shops	–	–	P	P	–	–
Shoe Repair	–	P	P	P	–	–
Video Rental & Sales	P	P	P	P	–	–
Entertainments Uses						
Billiards/Pool Parlors/Arcades	–	P	P	–	–	–
Bowling Alleys	–	P	P	P	–	–
Nightclubs	–	P	–	–	–	–
Skating/Skateboard Venues	–	–	P	–	–	–
Health and Exercise Clubs	P	P	P	–	–	–
Lodging						
Bed-and-Breakfasts	CUP	CUP[1]	CUP	–	–	–
Hotels, Motels	P	CUP[1]	CUP	P	–	–
Boardinghouses	–	–	–	–	–	–

NOTES
[1] See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
[2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
[3] Nonresidential off-street parking lot
[4] Use permitted only on upper floors.
[5] Use permitted only within Downtown Residential Overlay and only on upper floors.
[6] Use not permitted within 200 feet of railroad right-of-way.

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Offices															
Administrative	P[1]	–	–	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Business	P[1]	P[2]	P[2]	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Design	P[1]	P[2]	P[2]	P	–	–	–	–	–	–	–	–	–	–	–
Government	P[1]	P[2]	P[2]	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Medical and Dental	P	P[2]	P[2]	–	–	–	–	P	–	–	–	–	–	–	–
Professional	P[1]	P[2]	P[2]	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Social, Cultural or Public Services, including Public and Non-Profit Organizations.	–	CUP	CUP	–	–	–	–	–	–	–	–	–	–	–	–
Restaurants															
Alcoholic Beverages for on-site consumption in conjunction with operation of a restaurant (up to Type 47 liquor license)	CUP[1]	CUP	CUP	–	P	–	–	–	–	P	–	–	–	–	–
Bar, tavern, cocktail lounge, (Type 48 liquor license)	–	CUP	CUP	–	CUP	–	–	–	–	CUP	–	–	–	–	–
Drive-up and/or Drive-in Restaurants, including Restaurants in a Single Free-Standing Building	–	–	–	P	CUP	–	–	CUP	P	CUP	P	–	–	–	–
Full-Service Sit-Down Restaurants, including Walk-in Food Takeout Establishments	P	P	P	P	–	–	–	CUP	P	–	CUP	–	–	–	–
Outdoor Dining, subject to encroachment permit	P	P	P	–	P	–	–	–	–	P	–	–	–	–	–

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES
<i>P</i> = use permitted by right	M = Maclay District	M = Maclay District	R-3 = Multiple Family Zone
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	GN = General Neighborhood District	AC = Auto Commercial Sub-District	
		WF = Workplace Flex District	

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Offices						
Administrative	P	P	P	P	–	–
Business	P	P	P	P	–	–
Design	P	P	P	P	–	–
Incubator Business	–	P	P	P	P	–
Government	P	P	P	P	–	–
Medical and Dental	P	P	P	P	–	–
Professional	P	P	P	P	–	–
Social, Cultural or Public Services, including Public and Non-Profit Organizations.	–	P	–	–	–	–
Restaurants						
Alcoholic Beverages for on-site consumption in conjunction with operation of a restaurant (up to Type 47 liquor license)	P	P	P	P	P	–
Bar, tavern, cocktail lounge, (Type 48 liquor license)	CUP	CUP	CUP	CUP	–	–
Drive-up and/or Drive-in Restaurants, including Restaurants in a Single Free-Standing Building	–	–	–	CUP	CUP	–
Full-Service Sit-Down Restaurants, including Walk-in Food Takeout Establishments	P	P	P	–	P	–
Outdoor Dining, subject to encroachment permit	P	P	P	P	P	–

NOTES
[1] See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
[2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
[3] Nonresidential off-street parking lot
[4] Use permitted only on upper floors.
[5] Use permitted only within Downtown Residential Overlay and only on upper floors.
[6] Use not permitted within 200 feet of railroad right-of-way.

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Retail Stores															
Antique Shops	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Clothing Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Drug Stores and Pharmacies	P[1]	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Electronic Equipment Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Florist Shops	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Gift Shops	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Grocery Stores	P[1]	P	P	P	CUP	P	P	P	P	CUP	–	–	–	P	–
Hardware Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Jewelry Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Music Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Pet Supply Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Photographic Equipment and Supply Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	P	P	P	–
Shoe Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Specialty Foods	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Sporting Goods Stores	–	P	P	P	CUP	P	P	–	P	CUP	–	–	–	P	–
Service Commercial															
Contractor Supply and Home Improvement Stores	–	–	–	–	–	–	–	–	–	–	P	P	P	–	–
Electrical Supply	–	–	–	–	–	–	–	–	–	–	P	CUP	CUP	–	–
Equipment Sales and Rentals	–	–	–	–	–	–	–	–	–	–	P	–	P	–	–
Film Laboratories	–	–	–	–	–	–	–	–	–	–	P	–	–	–	–
Home Furnishings, Hardware and Appliance Sales and Repair	–	–	–	–	–	–	–	–	–	–	P	P	P	–	–
Plumbing, Heating, Air Conditioning Equipment Supply and Repair	–	–	–	–	–	–	–	–	–	–	P	–	–	–	–

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES
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	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone
	GN = General Neighborhood District	AC = Auto Commercial Sub-District	
		WF = Workplace Flex District	

SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Retail Stores						
Antique Shops	–	P	P	P	–	–
Clothing Stores	–	P	P	P	–	–
Drug Stores and Pharmacies	P	P	P	P	–	–
Electronic Equipment Stores	–	P	P	P	–	–
Florist Shops	–	P	P	P	–	–
Gift Shops	–	P	P	P	–	–
Grocery Stores	P	P	P	P	–	–
Hardware Stores	–	P	P	P	–	–
Jewelry Stores	–	P	P	P	–	–
Music Stores	–	P	P	P	–	–
Pet Supply Stores	–	P	P	P	–	–
Photographic Equipment and Supply Stores	–	P	P	P	–	–
Shoe Stores	–	P	P	P	–	–
Specialty Foods	–	P	P	P	–	–
Sporting Goods Stores	–	P	P	P	–	–
Service Commercial						
Contractor Supply and Home Improvement Stores	P	–	P	P	P	–
Electrical Supply	–	–	P	P	P	–
Equipment Sales and Rentals	–	–	–	–	P	–
Film Laboratories	–	–	P	P	P	–
Home Furnishings, Hardware and Appliance Sales and Repair	P	P	P	P	P	–
Plumbing, Heating, Air Conditioning Equipment Supply and Repair	–	–	–	–	P	–

- NOTES**
- [1]

See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
- [2]

M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
- [3]

Nonresidential off-street parking lot
- [4]

Use permitted only on upper floors.
- [5]

Use permitted only within Downtown Residential Overlay and only on upper floors.
- [6]

Use not permitted within 200 feet of railroad right-of-way.

SP-4 LAND USE AND PERMIT REQUIREMENTS

SP-4 Land Use	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
Plumbing Shops	–	–	–	–	–	–	–	–	–	–	P	–	–	–	–
Tool Sales and Rentals	–	–	–	–	–	–	–	–	–	–	P	P	P	–	–
Studios: Physical Activity and Instruction, including exercise and physical therapy, dance, martial arts, and similar activities.	–	P[2]	P[1]	P	–	–	–	P	P	–	–	–	–	–	–
Studios: Art and Design															
Artist Studio - all media	–	P[2]	P[2]	P	–	–	–	P	P	–	P	CUP	CUP	–	–
Design Professional Studio	–	P[2]	P[2]	–	–	–	–	P	–	–	P	CUP	CUP	–	–
Photography Studio	–	P[2]	P[2]	P	–	–	–	P	P	–	P	CUP	CUP	–	–
Studios: Movie and Television															
Post-Production Studios	–	–	–	P	–	–	–	P	–	–	P	P	P	–	–
Recording Studio	–	–	–	P	–	–	–	P	–	–	P	P	P	–	–
Television, Movie, and Media Arts Production Studio	–	–	–	P	–	–	–	P	–	–	P	P	P	–	–
Schools															
Business and Professional Practice	–	P[2]	P[2]	P	–	–	–	–	P	–	P	–	–	–	–
Nursery School/Day Care Facilities developed accordance with <i>San Fernando City Code</i> , Chapter 106 (Zoning), Article VI, Division 10 (Section 106-1271 <u>et seq.</u>	P	–	–	–	–	–	–	–	–	–	–	–	–	–	CUP
Performing and Fine Arts	–	P[2]	P[2]	P	–	–	–	–	P	–	P	–	–	–	–
Schools					–	CUP	CUP	–	–	–	–	–	–	CUP	
Vocational Training for Trades	–	P[2]	P[2]	P	–	–	–	–	P	–	P	–	–	–	–

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	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone
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SP-5 LAND USE AND PERMIT REQUIREMENTS

SP-5 Land Use	SP-5 District					
	M	D	MUC	AC	WF	GN
Plumbing Shops	–	–	P	P	P	–
Self Storage	–	–	–	–	–	–
Tool Sales and Rentals	–	–	–	–	P	–
Studios: Physical Activity and Instruction, including exercise and physical therapy, dance, martial arts, and similar activities.	P	P	P	P	P	–
Studios: Art and Design						
Artist Studio - all media	P	P	P	P	P	–
Design Professional Studio	P	P	P	P	P	–
Photography Studio	P	P	P	P	P	–
Studios: Movie and Television						
Post-Production Studios	P	–	P	P	P	–
Recording Studio	P	–	P	P	P	–
Television, Movie, and Media Arts Production Studio	P	–	P	P	P	–
Schools						
Business and Professional Prac-tice	P	P	P	P	P	–
Nursery School/Day Care Facilities developed accordance with <i>San Fernando City Code</i> , Chapter 106 (Zoning), Article VI, Division 10 (Section 106-1271 et seq.	P	–	–	–	–	CUP
Performing and Fine Arts	P	P	P	P	P	–
Schools		–				
Vocational Training for Trades	P	P	P	P	P	–

NOTES
[1] See pages 8-10 of this Land Use Requirements Comparison for full list of uses allowed within the M-1 and M-2 zones.
[2] M-1 uses allowed along both sides of First Street; M-2 uses allowed only along the north side of First Street.
[3] Nonresidential off-street parking lot
[4] Use permitted only on upper floors.
[5] Use permitted only within Downtown Residential Overlay and only on upper floors.
[6] Use not permitted within 200 feet of railroad right-of-way.

M-1

LAND USE AND PERMIT REQUIREMENTS

DIVISION 9. - M-1 LIMITED INDUSTRIAL ZONE^[7]

Sec. 106-581. - Purpose.

The M-1 limited industrial zone is established to provide areas for the location and operation of light manufacturing and related services and uses. This division is designed to promote the effective operation of light manufacturing uses and to increase their compatibility within this district and with adjacent land uses. It is also intended to provide for those uses which are supportive of or provide a direct service to the permitted industrial uses.

(Ord. No. 1270, § 30.400, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-582. - Permitted uses.

In the M-1 limited industrial zone, the following uses are permitted:

- (1)

Agricultural.
- (2)

Manufacturing. Subject to the conditions of this zone, manufacturing, assembling, repairing, testing, processing, packaging, warehousing, wholesaling, research or treatment of products may be conducted (other than those which may be obnoxious or offensive because of emission of odor, dust, smoke, gas, noise, vibration or other similar causes detrimental to the public health, safety or general welfare) including but not limited to the following:

a.

Animal hospitals.

b.

Assembly plants.

c.

Automobile laundry.

d.

Automotive repair major and minor.

e.

Building materials and hardware sales.

f.

Cabinet shops and woodworking.

g.

Computer manufacturing, maintenance and service.

h.

Cosmetics (no soap).

i.

Electric and gas appliances.

j.

Food products (excluding fish meat, sauerkraut, vinegar, yeast, and rendering or refining of fats and oils).

k.

Jewelry.

l.

Laundry, cleaning and dry cleaning plants.

m.

Lumbervard, building materials. contractor storage.

- o.

Motion picture studio or television studio.

p.

Optical equipment.
- q.

Pharmaceutical.
- r.

Photographic products and equipment.
- s.

Product service center.
- t.

Signs.
- u.

Telephone, communication exchange or equipment building.
- v.

Welding shop.
- w.

Wholesale business.
- x.

Other uses. The following accessory uses are permitted only where they are integrated with and clearly incidental to a primary permitted use:

1.

Employee's cafeteria or coffeeshop.

2.

Exhibition of products, produced on the premises or available for wholesale distribution.

3.

Offices.
- y.

Additional uses which in the judgment of the commission, as evidenced by resolution in writing, are similar to or not more objectionable than any of those enumerated in this subsection.
- (3)

Commercial uses which are customarily and incidental to industrial uses permitted in subsection (2) of this section.

(Ord. No. 1270, § 30.401, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-583. - Uses permitted subject to conditional use permit.

In the M-1 limited industrial zone, the following uses are permitted subject to a conditional use permit:

- (1)

Animal shelter.
- (2)

Commercial antennas.
- (3)

Engine manufacture.
- (4)

Helistop.
- (5)

Laboratory: chemical, biological, anatomical.
- (6)

Outdoor storage.
- (7)

Outdoor advertising signs, provided that no outdoor advertising sign shall be located within a 500-foot radius of any other such sign; each outdoor advertising sign shall have, at most, two sign faces, and each sign face shall have a maximum area of 100 square feet; the maximum height shall be 24 feet; and no outdoor advertising sign

M-1 LAND USE AND PERMIT REQUIREMENTS (CONTINUED)

- (8) Paint or related manufacture.
- (9) Research and development.
- (10) Service station.
- (11) Swap meets or flea markets, auctions indoor or outdoor.
- (12) Truck terminal or yard.
- (13) New and used car sales.
- (14) Storage and distribution.
- (15) Warehousing.
- (16) Extermination business.
- (17) Heliports.
- (18) House moving businesses.
- (19) Contractors' storage and fabrication yards.
- (20) Lumberyards and outside storage of building materials.
- (21) Boat building and repair businesses.
- (22) Landscaping and gardening service and supply businesses.
- (23) Machine shops and tool and die making.
- (24) Metal welding and plating business.
- (25) Rental yards, maintenance yards and storage yards for construction and agricultural related equipment, machinery and vehicles.
- (26) Restaurants.
- (27) Roofing businesses.
- (28) Salvage and recycling businesses.
- (29) Recreation vehicle storage yards.
- (30) Electric distribution and transmission substations, gas metering and regulation stations, and other similar public utility structures and uses.
- (31) Research and development facilities for the creation of prototypes.
- (32) Pharmaceutical laboratory.
- (33) Administrative, professional and business offices accessory to use permitted in this district.
- (34) Blueprinting and photostating.
- (35) Assembly of plastic products.
- (36) Manufacture of ceramic products using only previous pulverized clay and using kilns fired only by electricity or gas.
- (37) Parcel service delivery depot.
- (38) Photostating.

- (39) Manufacture of control devices and gauges.
- (40) Glass edging, beveling and silvering.
- (41) Studio or office or quarters for industrial designing, model making, sculpture, architecture, engineering, planning, drafting, editorial and general designing and ceramic arts.
- (42) Recreation area or facility accessory to any use permitted in this district.
- (43) Hazardous waste facility which includes off-site facility, on-site facility, regional facility, transfer facility/station, transportable treatment units, and treatment facility and which is subject to division 12 of article VI of this chapter.
- (44) Transitional housing, in accordance with section 106-311.

(Ord. No. 1270, § 30.402, 9-30-1985; Ord. No. 1305, 6-15-1987; Ord. No. 1486, § 2, 12-15-1997)

Sec. 106-584. - Uses prohibited.

In the M-1 limited industrial zone, all uses are prohibited except those expressly permitted by this division.

(Ord. No. 1270, § 30.403, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-585. - Applicable regulations.

In the M-1 limited industrial zone, all uses shall be subject to the applicable regulations of this division, including the following standards:

- (1) Division 3 of article II of this chapter pertaining to site plan review.
- (2) Division 3 of article V of this chapter pertaining to off-street parking and loading standards.
- (3) Division 5 of article V of this chapter pertaining to sign standards.
- (4) Division 6 of article V of this chapter pertaining to property development standards.

(Ord. No. 1270, § 30.404, 9-30-1985; Ord. No. 1305, 6-15-1987)

Secs. 106-586—106-610. - Reserved.

M-2 LAND USE AND PERMIT REQUIREMENTS

DIVISION 10. - M-2 LIGHT INDUSTRIAL ZONE^[8]

Sec. 106-611. - Purpose.

The M-2 light industrial zone is intended to provide an area for a variety of industrial activities operating under development standards designed to limit impacts on surrounding land uses.

(Ord. No. 1270, § 30.450, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-612. - Uses permitted.

In the M-2 light industrial zone, the following uses are permitted:

- (1) All uses permitted in the M-1 zone.
- (2) Emergency homeless shelters subject to the development standards noted in section 106-972 of this chapter.
- (3) Manufacturing. Subject to the conditions of this zone, manufacturing, assembling, repairing, testing, processing, warehousing, wholesaling, research or treatment of products may be conducted (other than those which may be obnoxious or offensive because of emission of odor, dust, smoke, gas, noise, vibration or other similar causes detrimental to the public health, safety or general welfare) including but not limited to the following:
 - a. Animal shelter.
 - b. Assaying.
 - c. Automobiles, trailers, boats, recreational vehicles.
 - d. Ceramics, pottery, statuary.
 - e. Heavy equipment sales and rental.
 - f. Ink, polish, enamel.
 - g. Pest control contractors.
 - h. Public service facilities.
 - i. Sandblasting.
 - j. Tile (indoor kiln).
 - k. Wallboard, glass (no blast furnace).
 - l. Blast furnaces as an accessory use and not needing EPA or AQMD approvals.

(Ord. No. 1270, § 30.451, 9-30-1985; Ord. No. 1305, 6-15-1987; Ord. No. 1625, § 8, 3-18-2013)

Sec. 106-613. - Uses permitted subject to conditional use permit.

In the M-2 light industrial zone, the following uses are permitted subject to a conditional use permit:

- (1) Uses subject to a conditional use permit in the M-1 zone.
- (2) Automotive impound area.
- (3) Metal engraving, metal fabrications.
- (4) Tire retreading and recapping

(Ord. No. 1270, § 30.452, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-614. - Prohibited uses.

In the M-2 light industrial zone, all uses are prohibited except those expressly permitted by this division.

(Ord. No. 1270, § 30.453, 9-30-1985; Ord. No. 1305, 6-15-1987)

Sec. 106-615. - Applicable regulations.

In the M-2 light industrial zone, all uses shall be subject to the applicable regulations of this division, including the following standards.

- (1) Division 3 of article II of this chapter pertaining to site plan review.
- (2) Division 3 of article V of this chapter pertaining to off-street parking and loading standards.
- (3) Division 5 of article V of this chapter pertaining to sign standards.
- (4) Division 6 of article V of this chapter pertaining to property development standards.

(Ord. No. 1270, § 30.454, 9-30-1985; Ord. No. 1305, 6-15-1987)

Secs. 106-616—106-640. - Reserved.

SP-4 / ZONING ORDINANCE DEVELOPMENT STANDARDS

SP-4 Standard	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
A. Development Intensity															
1. Floor Area Ratio (max.) [1]															
a. Non-residential	1.0	3.0	3.0	2.0	2.0	–	–	2.0		3.0	2.0	–	–	–	–
b. Residential Mixed-use	1.5[2]	3.5	3.5	2.0	2.0	–	–	2.5		–	–	–	–	–	–
2. Residential Density (dwelling units/acre)															
a. Minimum	12					–	–	24		–	–	–	–	–	–
b. Maximum	36	Unlimited				–	–	45		–	–	–	–	–	43
B. Height (see Figure 4.2)															
1. Building height as measured from sidewalk or finished grade to top of flat roof, cornice, or eave line of a peaked roof.															
a. Primary Building															
i. Minimum (stories / ft.)	–	1 / 24	1 / 24			–	–	1 / 24		–	–	–	–	–	–
ii. Maximum (stories / ft.)	3 / 40	4 / 50	4 / 50			45 ft.	45 ft.	4 / 50		3 / 40	3 / 40	45 ft.	45 ft.	45 ft.	45 ft.
iii. Maximum within Downtown Residential Overlay (stories / ft.)	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
iv. Maximum height adjacent to R-1 Single Family Residential Zone within a distance of 15 ft. from the property line (ft.)	No facade wall shall extend more than 10 feet above the height of the adjacent single family facade wall														
v. Maximum height along south side of Second Street between Hubbard Avenue and Maclay Avenue for a distance of 20 ft. behind the Second Street setback line (stories / ft.)	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–

KEY		SP-5 DISTRICTS		SP-4 DISTRICTS AND SUB-DISTRICTS		SAN FERNANDO ZONING ORDINANCE ZONES	
P	= use permitted by right	M	= Maclay District	M	= Maclay District	R-3	= Multiple Family Zone
CUP	= use requires approval of conditional use permit	D	= Downtown District	CC	= City Center Sub-District	C-1	= Limited Commercial Zone
–	= use not permitted	MUC	= Mixed-Use Corridor District	SFM	= San Fernando Mall Sub-District	C-2	= Commercial Zone
		AC	= Auto Commercial District	MUT	= Mixed-Use Transition Sub- District	M-1	= Limited Industrial Zone
		WF	= Workplace Flex District	WC	= Workplace Commercial Sub-District	M-2	= Light Industrial Zone
		GN	= General Neighborhood District	AC	= Auto Commercial Sub-District		
				WF	= Workplace Flex District		

SP-5 DEVELOPMENT STANDARDS

SP-5 Standard	SP-5 District					
	M	D	MUC	AC	WF	GN
A. Development Intensity						
1. Floor Area Ratio (max.) [1]						
a. Non-residential	1.0	3.0	2.0	3.0	2.0	–
b. Residential Mixed-use	1.5[2]	3.5[3]	2.5	–	2.0	–
2. Residential Density (dwelling units/acre)						
a. Minimum	12	24[3]	24	–	–	–
b. Maximum	37	50[3]	37	–	18	43
B. Height (see Figure 4.2)						
1. Building height as measured from sidewalk or finished grade to top of flat roof, cornice, or eave line of a peaked roof.						
a. Primary Building						
i. Minimum (stories / ft.)	–	1 / 24	1 / 24	–	–	–
ii. Maximum (stories / ft.)	3 / 40	3 / 40	3 / 40	3 / 40	3 / 40	4 / 45
iii. Maximum within Downtown Residential Overlay (stories / ft.)	–	4 / 50	–	–	–	–
iv. Maximum height adjacent to R-1 Single Family Residential Zone within a distance of 15 ft. from the property line (ft.)	No facade wall shall extend more than 10 feet above the height of the adjacent single family facade wall					
v. Maximum height along south side of Second Street between Hubbard Avenue and Maclay Avenue for a distance of 20 ft. behind the Second Street setback line (stories / ft.)	–	–	–	–	–	2 / 24[4]

SP-4 / ZONING ORDINANCE DEVELOPMENT STANDARDS

SP-4 Standard	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)	(AC)	(WF)				(GN)	
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
vi. Maximum height along north side of Celis Street between Huntington Street and Kalisher Street for a distance of 10 ft. behind the Celis Street setback line (stories / ft.)	–	–	–	–	–	–	–	2/24[4]	–	–	–	–	–	–	–
b. Accessory Building, including non-dwelling units, such as free-standing individual car garages, service structures and tool sheds (ft.)	12	–	–	–	–	–	–	12	–	–	–	–	–	–	12
2. Ground floor height as measured above grade at building setback line (max.)															
a. Residential	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
b. Non-residential	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
3. Ground story height as measured from floor to floor															
a. Residential (ft.)	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
b. Non-residential (ft.)	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–
4. Special architectural features, such as as uninhabited towers (clock, bell, observation) or entry volumes	10 ft. max. above maximum building height														
5. Rooftop structures, such as elevator and mechanical equipment enclosures or roof deck trellises and gazebos	10 ft. max. above maximum height limit, provided structures are set back a minimum of ten (10) feet from building walls and are screened on all sides by a parpaet or slopin roof that is architecturally integrated within the building design.														

SP-5 DEVELOPMENT STANDARDS

SP-5 Standard	SP-5 District					
	M	D	MUC	AC	WF	GN
vi. Maximum height along north side of Celis Street between Huntington Street and Kalisher Street for a distance of 10 ft. behind the Celis Street setback line (stories / ft.)	–	–	2/24[4]	–	–	–
b. Accessory Building, including non-dwelling units, such as free-standing individual car garages, service structures and tool sheds (ft.)	12	–	12	–	–	12
2. Ground floor height as measured above grade at building setback line (max.)						
a. Residential	4 ft.	4 ft.	4 ft.	4 ft.	4 ft.	4 ft.
b. Non-residential	–	–	–	–	–	–
3. Ground story height as measured from floor to floor						
a. Residential (ft.)	–	–	–	–	–	–
b. Non-residential (ft.)	14 min.	18 min.	18 min.	14 min.	14 min.	10 min.
4. Special architectural features, such as as uninhabited towers (clock, bell, observation) or entry volumes	10 ft. max. above maximum building height					
5. Rooftop structures, such as elevator and mechanical equipment enclosures or roof deck trellises and gazebos	10 ft. max. above maximum height limit, provided structures are set back a minimum of ten (10) feet from building walls and are screened on all sides by a parpaet or slopin roof that is architecturally integrated within the building design.					

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES	NOTES
<i>P</i> = use permitted by right	M = Maclay District	M = Maclay District	R-3 = Multiple Family Zone	[1] Floor Area Ratio (FAR) defined as the floor area of the building divided by the total project site area. FAR calculations do not including parking facilities
<i>CUP</i> = use requires approval of conditional use permit	D = Downtown District	CC = City Center Sub-District	C-1 = Limited Commercial Zone	[2] Only within Neighborhood Services Overlay.
– = use not permitted	MUC = Mixed-Use Corridor District	SFM = San Fernando Mall Sub-District	C-2 = Commercial Zone	[3] Only within Downtown Residential Overlay .
	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone	[4] Attic spaces of one- and two-story buildings with pitched roofs may be occupied and day lit with dormer windows.
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone	
	GN = General Neighborhood District	AC = Auto Commercial Sub-District		
		WF = Workplace Flex District		

SP-4 / ZONING ORDINANCE DEVELOPMENT STANDARDS

SP-4 Standard	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
C. Setbacks															
1. Primary Street Setback															
a. Non-Residential (ft.)	0 [2]	0	0	6 - 15	15 min.	10	10	0	6-15	15 min.	6 min.	10	10	10	–
b. Residential (ft.)	15	15 min.	15 min.	–	–	–	–	10 min.	–	–	10 min.	–	–	–	20
c. At-Grade Parking Lot	15 min.	5 min.	5 min.	6 min.	6 min.	–	–	6 min.		6 min.	6 min.	–	–	–	–
d. Freestanding Parking Structure	15 min.	0	0	6 min.	6 min.	–	–	6 min.		6 min.	6 min.	–	–	–	–
2. Side Street Setback															
a. Non-Residential (ft.)	0 [2]	0	0	6 - 15	15 min.	10	10	0		15 min.	0	10	10	10	–
b. Residential (ft.)	5 min,	15 min.	5 min.	–	–	–	–	5 min.	–	–	5 min,	–	–	–	20
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	6 min.	5 min.	–	–	5 min.		5 min.	5 min.	–	–	–	–
d. Freestanding Parking Structure	5 min.	0	0	6 min.	5 min.	–	–	5 min.		5 min.	5 min.	–	–	–	–
3. Side Yard Setback															
a. Non-Residential (ft.)	5 min,	0	0	5 - 15	5 min./ 15 max.	5	5	0 [6]		5 min./ 15 max.	0	10	10	5	–
b. Residential (ft.)	0 [2]	0	0	–	–	–	–	5 min./ 15 max.	–	–	5 min./ 15 max.	–	–	–	5
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	5 min.	5 min.	–	–	5 min.		5 min.	5 min.	–	–	–	–
d. Freestanding Parking Structure	5 min.	0	0	5 min.	5 min.	–	–	5 min.		5 min.	5 min.	–	–	–	–

SP-5 DEVELOPMENT STANDARDS

SP-5 Standard	SP-5 District					
	M	D	MUC	AC	WF	GN
C. Setbacks						
1. Primary Street Setback						
a. Non-Residential (ft.)	0 [2]	0	0	0 - 15	6 min.	–
b. Residential (ft.)	15	5 min.	10 min.	–	10 min.	20
c. At-Grade Parking Lot	15 min.	5 min.	6 min.	6 min.	6 min.	–
d. Freestanding Parking Structure	15 min.	0	6 min.	6 min.	6 min.	–
2. Side Street Setback						
a. Non-Residential (ft.)	0 [2]	0	0	0 - 15	0	–
b. Residential (ft.)	5 min,	5 min.	5 min.	–	5 min,	20
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	5 min.	5 min.	–
d. Freestanding Parking Structure	5 min.	0	5 min.	5 min.	5 min.	–
3. Side Yard Setback						
a. Non-Residential (ft.)	5 min,	0	0	0 - 15	0	–
b. Residential (ft.)	0 [2]	0	5 min./ 15 max.	–	5 min./ 15 max.	–
c. At-Grade Parking Lot	5 min.	5 min.	5 min.	5 min.	5 min.	–
d. Freestanding Parking Structure	5 min.	0	5 min.	5 min.	5 min.	–

KEY

- P = use permitted by right
- CUP = use requires approval of conditional use permit
- = use not permitted

SP-5 DISTRICTS

- M = Maclay District
- D = Downtown District
- MUC = Mixed-Use Corridor District
- AC = Auto Commercial District
- WF = Workplace Flex District
- GN = General Neighborhood District

SP-4 DISTRICTS AND SUB-DISTRICTS

- M = Maclay District
- CC = City Center Sub-District
- SFM = San Fernando Mall Sub-District
- MUT = Mixed-Use Transition Sub- District
- WC = Workplace Commercial Sub-District
- AC = Auto Commercial Sub-District
- WF = Workplace Flex District

SAN FERNANDO ZONING ORDINANCE ZONES

- R-3 = Multiple Family Zone
- C-1 = Limited Commercial Zone
- C-2 = Commercial Zone
- M-1 = Limited Industrial Zone
- M-2 = Light Industrial Zone

NOTES

- [1] Floor Area Ratio (FAR) defined as the floor area of the building divided by the total project site area. FAR calculations do not including parking facilities
- [2] Only within Neighborhood Services Overlay.
- [3] Only within Downtown Residential Overlay .
- [4] Attic spaces of one- and two-story buildings with pitched roofs may be occupied and day lit with dormer windows.

SP-4 / ZONING ORDINANCE DEVELOPMENT STANDARDS

SP-4 Standard	(SP-5 District) SP-4 District / Sub-District or Zoning Ordinance Zone														
	(M)	(D)						(MUC)		(AC)	(WF)				(GN)
	M	CC	SFM	WC	AC	C-1	C-2	MUT	WC	AC	SC	M-1	M-2	C-2	R-3
4. Rear Setback															
a. With alley, as measured from property line (ft.)	5	0	0	0	0	10	19	0	0	0	0	10	10	19	20
b. Without alley (ft.)	15	0	0	10	0	10	19	0	10	0	10	10	10	19	20
c. At-Grade Parking Lot	6 min.	6 min.	6 min.	6 min.	6 min.	–	–	6	6 min.	6	6	–	–	–	20
d. Freestanding Parking Structure	6 min.	6 min.	6 min.	6 min.	6 min.	–	–	6	6 min.	6	6	–	–	–	–

SP-5 DEVELOPMENT STANDARDS

SP-5 Standard	SP-5 District					
	M	D	MUC	AC	WF	GN
4. Rear Setback						
a. With alley, as measured from property line (ft.)	5	0	0	0	0	10
b. Without alley (ft.)	15	0	0	0	10	20
c. At-Grade Parking Lot	6 min.	6 min.	6 min.	6 min.	6 min.	6 min.
d. Freestanding Parking Structure	6 min.	6 min.	6 min.	6 min.	6 min.	–

KEY	SP-5 DISTRICTS	SP-4 DISTRICTS AND SUB-DISTRICTS	SAN FERNANDO ZONING ORDINANCE ZONES	NOTES
<i>P</i> = use permitted by right	M = Maclay District	M = Maclay District	R-3 = Multiple Family Zone	[1] Floor Area Ratio (FAR) defined as the floor area of the building divided by the total project site area. FAR calculations do not including parking facilities
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– = use not permitted	MUC = Mixed-Use Corridor District	SFM = San Fernando Mall Sub-District	C-2 = Commercial Zone	[3] Only within Downtown Residential Overlay .
	AC = Auto Commercial District	MUT = Mixed-Use Transition Sub- District	M-1 = Limited Industrial Zone	[4] Attic spaces of one- and two-story buildings with pitched roofs may be occupied and day lit with dormer windows.
	WF = Workplace Flex District	WC = Workplace Commercial Sub-District	M-2 = Light Industrial Zone	
	GN = General Neighborhood District	AC = Auto Commercial Sub-District		
		WF = Workplace Flex District		

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Councilmember Joel Fajardo

Date: November 20, 2017

Subject: General Discussion of Development Issues Including, But Not Limited to, Project Labor Agreements, Down Payment Assistance Programs, and Other Topics of Community Concern or Interest

RECOMMENDATION:

I have placed this on the agenda for City Council discussion and consideration.

BUDGET IMPACT:

There is no impact to the budget by discussing this item. Additional future costs, to be determined, is based on City Council direction.

ATTACHMENT:

A. Form – Request to Agendize an Item for City Council Discussion/Consideration

REQUEST TO AGENDIZE AN ITEM FOR CITY COUNCIL DISCUSSION/CONSIDERATION

CITY COUNCILMEMBER INFORMATION

NAME

TITLE

ITEM INFORMATION

SUBJECT *Title of the item you are requesting to be agendized.*

PRIORITIES

Is this included in the current FY priorities?

☐ Yes ☐ No

BUDGET

Is this a budgeted item?

☐ Yes ☐ No

FISCAL IMPACT

Is there a fiscal impact? If yes, indicate amount.

☐ Yes ☐ No \$

BACKGROUND/ANALYSIS *Provide the reason you are requesting this item be agendized.*

ATTACHMENTS *Do you have any attachments to include?*

☐ Yes ☐ No

RECOMMENDATION *Indicate the direction you are recommending.*



Eric Garcetti, Mayor
Rushmore D. Cervantes, General Manager

City of Los Angeles

Moderate Income Purchase Assistance (MIPA) Program

LIMITED FUNDS

The Los Angeles Housing + Community Investment Department (HCIDLA) can help you achieve homeownership. The Moderate Income Purchase Assistance (MIPA) Program helps first-time, moderate-income homebuyers purchase homes in the City of Los Angeles by providing subordinate loans to cover the down payment, closing costs, and acquisition.

The City's homeownership program funds are leveraged with private mortgage financing and other assistance programs to achieve the maximum purchasing power for the homebuyer. The MIPA Program provides purchase assistance of up to \$60,000 for households earning between 81%-120% of Area Median Income (AMI), and up to \$35,000 for households earning between 121%-150% of AMI.

The program features and requirements are detailed below:

Homebuyer Eligibility

- First-time homebuyers who have not had ownership interest in any real property in the past three years
- U.S. Citizens, Lawful Permanent Residents, or other Qualified Aliens
- Household income must be within program limits on table below
- Minimum 1% down payment from Borrower's own funds. Some homebuyers may need to contribute more than the required 1% toward the down payment to complete the purchase
- Homebuyer must occupy home as their Primary Residence
- Completion of an eight-hours (8) of homebuyer education course from a HUD and HCIDLA-approved homebuyer education provider

MIPA Loan Features

- Mod 120 loan up to \$60,000, and Mod 150 loan up to \$35,000 – refer to income table below
- Zero percent (0%) interest rate
- Deferred – requiring no monthly payments
- Shared appreciation provision in which the City will share a certain percentage of the appreciation in the value of the property
- Loan and shared appreciation is due on sale, title transfer, first mortgage repayment, or in 30 years as a balloon payment

Property Eligibility

- Home must be located in the City of Los Angeles
- Home must pass HCIDLA property inspection
- 1-unit, single-family homes including condominiums and townhouses
- No home purchase price limits

2017 Maximum Annual Income Limits

Household Size	1 person	2 people	3 people	4 people	5 people	6 people	7 people	8 people
Mod 120 (81%-120% AMI)	\$50,501 - \$75,700	\$57,701 - \$86,500	\$64,901 - \$97,300	\$72,101 - \$108,100	\$77,901 - \$116,750	\$83,651 - \$125,400	\$89,451 - \$134,050	\$95,201 - \$142,700
Mod 150 (121%-150% AMI)	\$75,701 - \$94,650	\$86,501 - \$108,150	\$97,301 - \$121,650	\$108,101 - \$135,150	\$116,751 - \$146,100	\$125,401 - \$156,900	\$134,051 - \$167,700	\$142,701 - \$178,500

How Does this Program Work?

HCIDLA provides a purchase assistance loan of up to \$60,000. The MIPA loan provides the difference between the purchase price plus closing costs (up to 5% of the purchase price) and the amount of the homebuyer's first mortgage loan, down payment, and other funding sources.

MIPA 120 - EXAMPLE

Purchase Price of Home:	\$500,000
Homebuyer's Closing Costs:	<u>\$20,000</u>
Total Funds Needed:	\$520,000

First Mortgage:	\$455,000
Homebuyer Down Payment (1% of Purchase Price):	\$5,000
City's MIPA Loan	<u>\$60,000</u>
Total Funds Available:	\$520,000

The total City MIPA loan in the above example is \$60,000.

How to Apply?

Please contact one of HCIDLA's Participating Lenders to get pre-qualified for a first mortgage and the City's Moderate Income Purchase Assistance (MIPA) Program. Please visit our website at <http://hcidla.lacity.org/homebuyers> for more information or call HCIDLA's Homeownership Unit General Line (213) 808-8800.

As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability and, upon request, will provide reasonable accommodation to ensure equal access to its programs, services and activities.





Eric Garcetti, Mayor
Rushmore D. Cervantes, General Manager

Low Income Purchase Assistance (LIPA) Program

Shared Appreciation Example

HCIDLA LIPA Loan	\$60,000
Original Home Purchase Price	\$300,000
HCIDLA Percentage Share of Appreciation (HCIDLA Loan ÷ Purchase Price)	20.00%
Future Sales Price or Appraised Value	\$400,000
Less: Original Purchase Price	\$300,000
Appreciation	\$100,000
Less: Transaction Costs to Sell Property	(\$24,000)
Less: Initial Homebuyer Down Payment	(\$4,500)
Less: Eligible Capital Improvements	\$0
Net Appreciation	\$71,500
Share Appreciation due to HCIDLA (Net Appreciation x HCIDLA Percentage Share)	\$14,300
TOTAL DUE HCIDLA:	
HCIDLA LIPA Loan Amount	\$60,000
HCIDLA Share of Appreciation Amount	\$14,300
TOTAL AMOUNT DUE TO HCIDLA	\$74,300

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Fair Housing Lender

Rev: 06.07.17

An Equal Opportunity / Affirmative Action Employer

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AGENDA REPORT

To: Mayor Sylvia Ballin and Councilmembers

From: Councilmember Joel Fajardo

Date: November 20, 2017

Subject: Consideration to Adopt an Ordinance Regarding the Regulation of Behested Payments

RECOMMENDATION:

I have placed this on the agenda for City Council discussion and consideration.

It is recommended that the City Council introduce for first reading, in title only, and waive further reading of Ordinance No. 1670 (Attachment "A") titled, "An Ordinance of the City Council of the City of San Fernando, California, Amending Article VII (Campaign Reform) of Chapter 2 (Administration) by the Addition of Section 2-910 Regarding the Regulation of Behested Payments."

BACKGROUND:

On May 5, 2015, I agendized this item for discussion but no final City Council action was taken. I reported that I would look into ways to improve the ordinance and possibly bring it back to a future meeting.

BUDGET IMPACT:

There is no impact to the budget by introducing this ordinance. Additional future costs, to be determined, is based on City Council direction.

ATTACHMENT:

A. Ordinance No. 1670

ATTACHMENT “A”**ORDINANCE NO. 1670****AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF
SAN FERNANDO, CALIFORNIA, AMENDING ARTICLE VII
(CAMPAIGN REFORM) OF CHAPTER 2 (ADMINISTRATION)
BY THE ADDITION OF SECTION 2-910 REGARDING THE
REGULATION OF BEHESTED PAYMENTS**

WHEREAS, state law sets forth a reporting scheme under Government Code section 82015(b)(2)(B)(iii) for so-called “behested payments” as part of the Political Reform Act (the “Act”) that applies to elected officials of the City of San Fernando (the “City”);

WHEREAS, the Act provides that even if a payment is made “at the behest” of an elected official, it is neither a gift nor a contribution if it is made principally for legislative, governmental or charitable purposes;

WHEREAS, the California Fair Political Practices Commission (“FPPC”) prescribes a specific Form 803 for behested payment reports, which must be filed within thirty (30) days with the official’s agency when the amount of the payment, or aggregate payments for similar purposes made at the behest of the officer by the same source, is \$5,000 or more in a calendar year;

WHEREAS, the public policy behind the reporting of such behested payments is to provide the disclosure of payments to the public in which there may be a potential for influence over a public official; and

WHEREAS, the City Council desires to augment the state law requirements concerning behested payments as set forth below.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SAN FERNANDO DOES HEREBY ORDAIN AS FOLLOWS:

SECTION 1. The facts set forth in the recitals above are true and correct and are incorporated herein by reference.

SECTION 2. Article VII (Campaign Reform) of Chapter 2 (Administration) of the City of San Fernando Municipal Code is amended by the addition of Section 2-910 which shall read as follows:

Sec. 2-910. Behested Payments.

(a) Definitions. The terms “payment” and “official” shall have those meanings as set forth in California Government Code Sections 82044 and 82020, respectively.

(b) Payments made for legislative, governmental, or charitable purposes at the behest of a candidate who is an elected officer shall be reported within thirty (30) calendar days to the City Clerk on a form prescribed by the City Clerk.

(c) Within twenty (20) calendar days of receipt of a report submitted pursuant to Subsection (b), above, the City Clerk shall publicly identify the behested payment identified therein at a regular City Council meeting at the conclusion of the public comment portion of a City Council meeting. Alternatively, the applicable elected official can publicly identify his or her reported behested payment within twenty (20) calendar days of the submission of a report submitted pursuant to Subsection (b), above, during his or her closing comments at a City Council meeting.

SECTION 3. Should any section, subsection, clause or provision of this Ordinance, for any reason, be held to be invalid or unconstitutional, such invalidity or unconstitutionality shall not affect the validity or constitutionality of the remaining portions of this Ordinance. This Ordinance, and each section, subsection, sentence, clause, and phrase hereof would have been prepared, proposed, approved, and ratified irrespective of the fact that any one or more sections, subsections, sentences, clauses, or phrases be declared invalid or unconstitutional.

SECTION 4. The Mayor shall sign and the City Clerk shall attest to the passage of this Ordinance. The City Clerk shall cause the same to be published once in the official newspaper within fifteen (15) days after its adoption. This Ordinance shall become effective thirty (30) days after adoption.

PASSED, APPROVED, AND ADOPTED by the City Council of the City of San Fernando at the regular meeting held on the _____ day of _____, 2017.

Sylvia Ballin, Mayor

ATTEST:

Elena G. Chávez, City Clerk

APPROVED AS TO FORM:

Rick R. Olivarez, City Attorney

STATE OF CALIFORNIA)
COUNTY OF LOS ANGELES) SS
CITY OF SAN FERNANDO)

I, Elena G. Chávez, City Clerk of the City of San Fernando, do hereby certify that the foregoing Ordinance was adopted a regular meeting of the City Council held on the _____ day of _____, 2017 and was carried by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Elena G. Chávez, City Clerk